



DEPARTMENT OF CITY PLANNING

RECOMMENDATION REPORT

East Los Angeles Area Planning Commission

Date: May 13, 2020
Time: After 4:30 P.M.*
Place: In conformity with the Governor's Executive Order N-29-20 (March 17, 2020) and due to concerns over COVID-19, the APC meeting will be conducted entirely telephonically by Zoom [<https://zoom.us/>].

The meeting's telephone number and access code access number will be provided no later than 72 hours before the meeting on the meeting agenda published at <https://planning.lacity.org/about/commissions-boards-hearings> and/or by contacting apceastla@lacity.org

Public Hearing: January 23, 2020

Appeal Status: Zone Change is appealable only by the applicant to City Council if disapproved in whole or in part. Zoning Administrator's Determination and Zoning Administrator's Adjustment is appealable to the City Council by the applicant or any interested person adversely affected.

Expiration Date: May 15, 2020

Multiple Approval: Yes

PROJECT LOCATION: 2520 North Eastern Avenue (2532, 2608, and 2668 N. Eastern Avenue and 2647-2651 N. Lombardy Boulevard)

PROPOSED PROJECT: The proposed Project is for the new construction, use, and maintenance of 42 small lot homes on the approximately 218,270 square-foot lot. The small lot homes will have approximately 2,000 square feet of floor area and a maximum height of 26 feet. The Project proposes to provide two (2) parking spaces per dwelling unit, which will be located in garages attached to each small lot home. In addition to the two (2) required parking spaces, up to 38 of the small lot homes have been designed to accommodate two (2) additional parking spaces within each of the driveways, providing off-site options for residence and their guests. Access to 38 of the small lot homes will be provided through a common access driveway accessible from Eastern Avenue, while the remaining four (4) small lot homes will be accessible from a common access driveway from Lombardy Boulevard. The Project will require the removal of 39 of 102 Protected Trees (Black Walnut trees) located on the site and the grading and export of approximately 78,000 cubic yards of dirt.

Case No.: APCE-2015-2048-ZC-ZAD-ZAA
CEQA No.: ENV-2015-1918-MND
Incidental Cases: VTT-73531-SL
Related Cases: N/A
Council No.: 14 - Huizar
Plan Area: Northeast Los Angeles
Overlay: Northeast Los Angeles Hillside
Certified NC: LA-32
GPLU: Low Residential
Existing Zone: [Q]R1-1D; [Q]RD6-1D
Proposed Zone: [Q]RD5-1D
Applicant: Rob Flitton,
The True Life Companies
Representative: Jim Ries,
Craig Lawson & Co, LLC

REQUESTED ACTIONS:

1. Pursuant to CEQA Guidelines Section 15074(b), consideration of the whole of the administrative record, including the Mitigated Negative Declaration (MND), Case No. ENV-2015-1918-MND, all comments received, the imposition of mitigation measures and the Mitigation Monitoring Program prepared for the Mitigated Negative Declaration;
2. Pursuant to LAMC Section 12.32, a Zone Change from [Q]R1-1D and [Q]RD6-1D to [Q]RD5-1D and modifications to the existing permanent Qualified "Q" Conditions contained within Ordinance No. 180,403;
3. Pursuant to LAMC Section 12.24 X.26, a Zoning Administrator's Determination to permit a maximum of 178 retaining walls varying in height from less than 3 feet to 6 feet with approximately 103 of these walls including an additional six (6) foot high privacy/security fence on top of the retaining walls; and
4. Pursuant to LAMC Section 12.28, a Zoning Administrator's Adjustment to permit retaining walls varying in height from less than 3 feet to 6 feet with an additional maximum of six (6) foot privacy/security fence on top of the retaining walls within the required front, side, and rear yards in lieu of the maximum 42 inches in the front yard and 6 feet in the side and rear yards pursuant to LAMC Section 12.21 C.1(g).

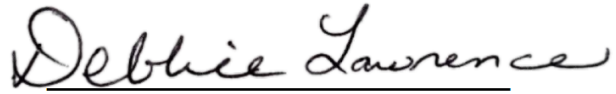
RECOMMENDED ACTIONS:

1. **FIND**, based on the independent judgement of the decision-maker, after consideration of the whole of the administrative record, the project was assessed in the Mitigated Negative Declaration (MND), Case No. ENV-2015-1918-MND, adopted on March 3, 2020; and pursuant to CEQA Guidelines, Sections 15162 and 15164, no subsequent EIR, negative declaration, or addendum is required for approval of the project;
2. **Approve** and **Recommend** that the City Council adopt a **Zone Change** on the Project Site from [Q]R1-1D and [Q]RD6-1D to (T)(Q)RD5-1D;
3. **Approve** the **Zoning Administrator's Determination** to permit a maximum of 178 retaining walls varying in height from less than 3 feet to 6 feet with approximately 103 of these walls including an additional privacy/security fence on top of the retaining walls for a height not to exceed six (6) feet;
4. **Approve** the **Zoning Administrator's Adjustment** to permit retaining walls varying in height from less than 3 feet to 6 feet with an additional privacy/security fence on top of the retaining walls for a maximum height of six (6) feet in the front, side, and rear yards of the site in lieu of the maximum 42 inches in the front yard and six (6) feet in the side and rear yards pursuant to LAMC Section 12.21 C.1(g);
5. **Adopt** the attached Conditions and Findings; and
6. **Advise** the applicant that, pursuant to California State Public Resources Code Section 21081.6, the City shall monitor or require evidence that mitigation conditions are implemented and maintained throughout the life of the project and the City may require any necessary fees to cover the cost of such monitoring.

VINCENT P. BERTONI, AICP
Director of Planning



Jane J. Choi, AICP
Principal City Planner



Debbie Lawrence
Senior City Planner



May Sirinopwongsagon
City Planner
Telephone: 213-978-1372

ADVICE TO PUBLIC: *The exact time this report will be considered during the meeting is uncertain since there may be several other items on the agenda. Requirements for submission of materials can be found on the Department of City Planning website at <https://planning.lacity.org/about/virtual-commission-instructions>. If you challenge these agenda items in court, you may be limited to raising only those issues you or someone else raised at the public hearing agendaized herein, or in written correspondence on these matters delivered to this agency at or prior to the public hearing. As a covered entity under Title II of the Americans with Disabilities Act, the City of Los Angeles does not discriminate on the basis of disability, and upon request, will provide reasonable accommodation to ensure equal access to these programs, services and activities. Sign language interpreters, assistive listening devices, or other auxiliary aids and/or other services may be provided upon request. To ensure availability of services, please make your request not later than seven working days prior to the meeting by calling the Commission Secretariat at (213) 978-1295.

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PROJECT ANALYSIS

Project Summary

The proposed Project is for the new construction, use, and maintenance of 42 small lot homes located on an approximately 218,270 gross square-foot lot located generally at 2520 North Eastern Avenue. The small lot homes will have approximately 2,000 square feet of floor area, an attached two (2) car garage, with a maximum height of 26 feet. Up to 38 of the small lot homes will be able to accommodate two (2) additional parking spaces within each of the driveways.

The Project will require the removal of 39 of 102 Protected Trees (Black Walnut trees) located on the site and the grading and export of approximately 78,000 cubic yards of dirt. In addition to the construction of the small lot homes, the applicant proposes the construction of a total of 178 retaining walls throughout the site, comprised of 129 walls that will be less than 3 feet in height, 18 walls with a height between 3 and 4 feet, and 32 walls with a height between 4 and 6 feet. The applicant has proposed an additional 6-foot high privacy/security fence to be constructed on top of up to 103 of the retaining walls will have.

On March 3, 2020, the Advisory Agency approved Vesting Tentative Tract Map No. 73531-SL under incidental Case No. VTT-73531-SL for a maximum of 42 small lots pursuant to Los Angeles Municipal Code (LAMC) Section 12.22 C.27, Ordinance No. 176,354, as shown on the revised map stamp-dated March 2, 2020. In addition to the approval of the tract map, the Advisory Agency approved a haul route for the export of approximately 78,000 cubic yards of dirt. The decision was not appealed and became effective on March 14, 2020.

Zone Change

The Project application was submitted to the Department of City Planning on June 1, 2015 and includes a request for a Zone Change which results in an increase in density. The application was deemed complete on October 20, 2016. The application was filed in conjunction with incidental Case No. VTT-73531-SL, which was submitted to the Department on May 20, 2015. The application was deemed complete on March 29, 2016. As clarified in the Department of City Planning Memo dated December 13, 2016, the project is not subject to Measure JJJ because the application was deemed complete prior to December 13, 2016.

Small Lot Subdivision

The Project application includes a request for a Small Lot Subdivision pursuant to LAMC Section 12.22 C.27. The application was submitted to the Department on May 20, 2015. The application was deemed complete on March 29, 2016. As the application was deemed complete prior to the April 18, 2018 effective date of the Ordinance No. 185,462 which amended LAMC Section 12.22 C.27, the Project is subject to the regulations of LAMC Section 12.22 C.27 as it was adopted pursuant to Ordinance No. 176,354.

Background

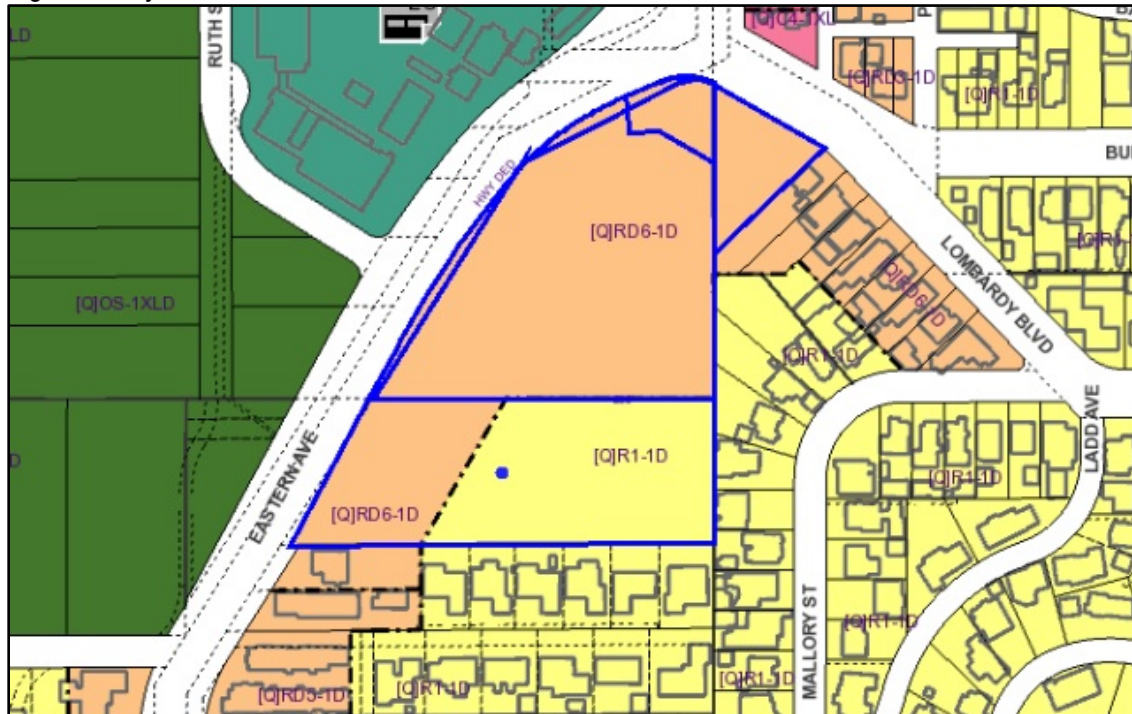
Project Site

The Project Site is a vacant, irregularly-shaped site comprised of multiple lots and approximately 218,270 gross square feet of lot area. The Project Site has approximately 780 feet of frontage along the eastern side of Eastern Avenue and 200 feet of frontage along the southern side of Lombardy Boulevard. The site is currently vacant and contains 102 Protected Trees and 72 non-protected trees. As shown in the survey prepared for the Project, included as

below as Figure 3 and in Exhibit A, the western portion of the site located along Eastern Avenue is an upward sloping hillside with a steep slope. The southern, eastern, and northwestern portion of the site also consists of an upward sloping hillside; however, it is a more gradual slope than the western portion.

The Site is located within the Northeast Los Angeles Community Plan area and has a land use designation of Low Residential. As shown in Figure 1 below, the southeast corner of the site is zoned [Q]R1-1D and the remaining approximately, 162,041 square feet of the site is zoned [Q]RD6-1D.

Figure 1: Project Site



The site is subject to the Qualified “Q” Conditions and Development “D” Limitations contained within Ordinance No. 180,403, also known as the Northeast Los Angeles Hillside Ordinance. The Ordinance contains regulations pertaining to infrastructure, building design, retaining walls, landscaping, and environmental mitigation measures. The applicant has requested to change the zone from [Q]R1-1D and [Q]RD6-1D to [Q]RD5-1D. While the request includes proposed modifications and clarifications to the existing Q Conditions related to retaining walls, hardscape, and grading, the Project has otherwise been designed to comply with existing Northeast Los Angeles Hillside Ordinance.

Project

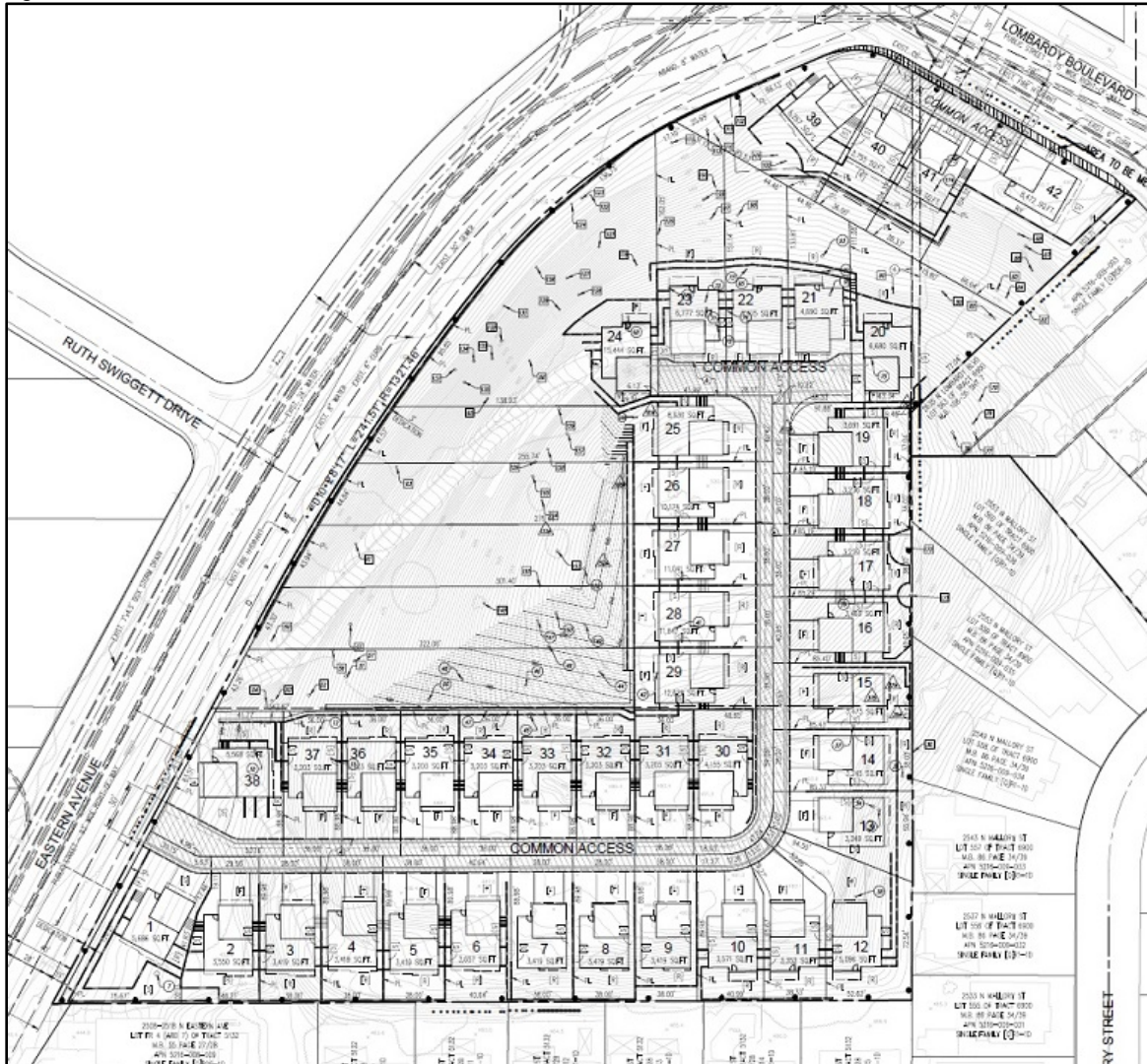
The Project proposes the new construction of 42 small lot homes in conjunction with the merger and resubdivision of the existing lots into 42 small lots as shown on the approved revised Vesting Tentative Tract Map No. 73531-SL pursuant to LAMC Section 12.22 C.27. The small lots would range in size from 3,203 to 15,444 square feet in size. Incidental Case No. 73531-SL was approved by the Deputy Advisory Agency on March 3, 2020, which has been conditioned upon the approval and adoption of the requested Zone Change to [Q]RD5-1D.

The Project includes three (3) different floor plans for the small lot homes, which consists of a two-story dwelling with an optional rooftop deck, a two-story dwelling with a basement, and a three-story dwelling. Each small lot home is proposed to have approximately 2,000 square feet of floor area and a height of 26 feet, as well as two (2) covered parking spaces. Up to 38 of the

42 small lot homes have been designed to accommodate two (2) additional parking spaces within each of the driveways.

As shown in the Figure 2 below, the Project includes two common access driveways which will provide access to the dwelling units. One common access driveway is located at the southwest corner of the site which will provide access to 38 small lot homes from Eastern Avenue. The second common access driveway is located at the northeast corner of the site which will provide access to four (4) small lot homes from Lombardy Avenue.

Figure 2: Site Plan



Protected Trees

The Project Site contains 102 Protected Trees, as shown in an updated Tree Preservation Report dated August 16, 2018 prepared by Arborgate Consulting Inc. The report was submitted to the Urban Forestry Division of the Bureau of Street Services for review on December 6, 2018. The report identifies that all of the Protected Trees on site are Southern California Black Walnuts (Black Walnut). As shown in the Protected Tree Matrix of Health and Condition contained within the report, a majority of the Black Walnut trees have a health grade of C, with some trees having a health grade of B or D. The proposed removal of 39 Protected Trees, as well as non-Protected Trees is further discussed under *Issues – Removal of Protected Trees*.

Grading

The Project would require grading of 82,000 cubic yards of cut and 5,000 cubic yards of fill and the export of approximately 78,000 cubic yards of dirt. As part of the approval of incidental Case No. VTT-73531-SL, the Advisory Agency approved a haul route for the export of 78,000 cubic yards of dirt. As the proposed amount of grading exceeds the maximum permitted under the existing Q Conditions, the applicant has requested to modify the condition as it relates to the development of the Project Site. The requested modification is further discussed under *Issues - Northeast Los Angeles Hillsides Ordinance – Q Conditions and D Limitations*.

Retaining Walls and Privacy/Security Fence

Due to the topography of the site, the Project would include the construction of a total of 178 retaining walls throughout the site comprised of 129 walls that will be less than 3 feet in height, 18 walls with a height between 3 and 4 feet, and 32 walls with a height between 4 and 6 feet. The applicant has proposed an additional 6-foot high privacy/security fence to be constructed on top of up to 103 of the retaining walls will have. The applicant has requested a Zoning Administrator's Determination for the number of retaining walls and a Zoning Administrator's Adjustment to permit retaining walls and privacy/security fences within the required yards with varying heights up to 12 feet. Additionally, the applicant has requested the modification of the existing Q Condition as it relates to the maximum permitted height and length of the retaining walls. The location and height of the retaining walls and privacy/security fence is discussed under *Issues – Northeast Los Angeles Hillsides Ordinance – Q Conditions and D Limitations* and *Issues – Privacy and Over-In-Fences*

Surrounding Properties

The parcels directly adjacent to the site to the east and south have a land use designation of Low Residential and are zoned [Q]RD6-1D and [Q]R1-1D, and are developed primarily with one-story single-family dwellings. Some of the properties located to the south along Eastern Avenue are developed with low density multi-family dwelling units. The parcels to the west and northwest of the site, across Eastern Avenue, have a land use designation of Open Space and Public Facilities and are zoned [Q]OS-1XLD and [Q]PF-1D, respectively. The parcels are developed with the El Sereno Park and Recreation Center and Farmdale Elementary School. To the northeast, along Eastern Avenue, the parcels have a land use designation of Neighborhood Commercial and are zoned [Q]C4-1XL. The parcels are developed with one- to two-story residential uses and one-story commercial uses. The parcels to the northeast, across Lombardy Boulevard, have a land use designation of Low Medium I Residential and Low Residential and are zoned [Q]RD3-1D and [Q]R1-1D and are developed with one- and two-story single- and multi-family dwellings.

The Project Site is located approximately 0.2 miles to the south of the El Sereno Elementary School, located at 2839 North Eastern Avenue, and 0.7 miles to the southwest of the Sierra Park Elementary School, located at 3170 North Budau Avenue.

Streets and Circulation

Eastern Avenue, a designated Avenue II, is dedicated to a width of 80 feet at the site's frontage and is improved with roadway, curb, gutter, sidewalk, and a parkway.

Lombardy Boulevard, a designated Collector, is dedicated to a width of 80 feet at the site's frontage and is improved with roadway, curb, gutter, sidewalk, and a parkway

Relevant Cases

Subject Property:

Case No. CPC-2011-3128-GPA-ZC-HD: On November 26, 2013, the City Planning Commission denied a General Plan Amendment from Low Residential to Neighborhood Commercial and a Zone and Height District Change from [Q]R1-1D and [Q]RD6-1D to RAS3-1 for the development of a 45,388 square-foot charter school, a café, and 20 multi-family residential dwelling units.

Case No. AA-2011-2668-PMEX: On January 1, 2012, the Advisory Agency approved a lot line adjustment in conjunction with the proposed development of the site with a charter school, a café, and 20 multi-family residential dwelling units.

Case No. CPC-2008-1182-ZC and Ordinance No. 180,403: On January 16, 2009, the Ordinance became effective and adopted Qualified “Q” Conditions and Development “D” Limitations for parcels located within the Northeast Los Angeles Community Plan area as shown in the accompanying map related to the development of residential dwelling units.

Surrounding Properties:

Case No. CPC-2008-1182-ZC and Ordinance No. 180,403: On January 16, 2009, the Ordinance became effective and adopted Qualified “Q” Conditions and Development “D” Limitations for parcels located within the Northeast Los Angeles Community Plan area as shown in the accompanying map related to the development of residential dwelling units.

Public Hearing and Communications

A joint public hearing on this matter conducted by the Deputy Advisory Agency and Hearing Officer was held at City Hall on Thursday, January 23, 2020. The public hearing was attended by the applicant, the applicant’s representative, and members from the public. For a summary of the Public Hearing, see Page P-1.

Public comment letters have been included as follows: Exhibit F.1 – Interested Parties (Opposition and Support), and Exhibit F.2 – Applicant Outreach.

Issues

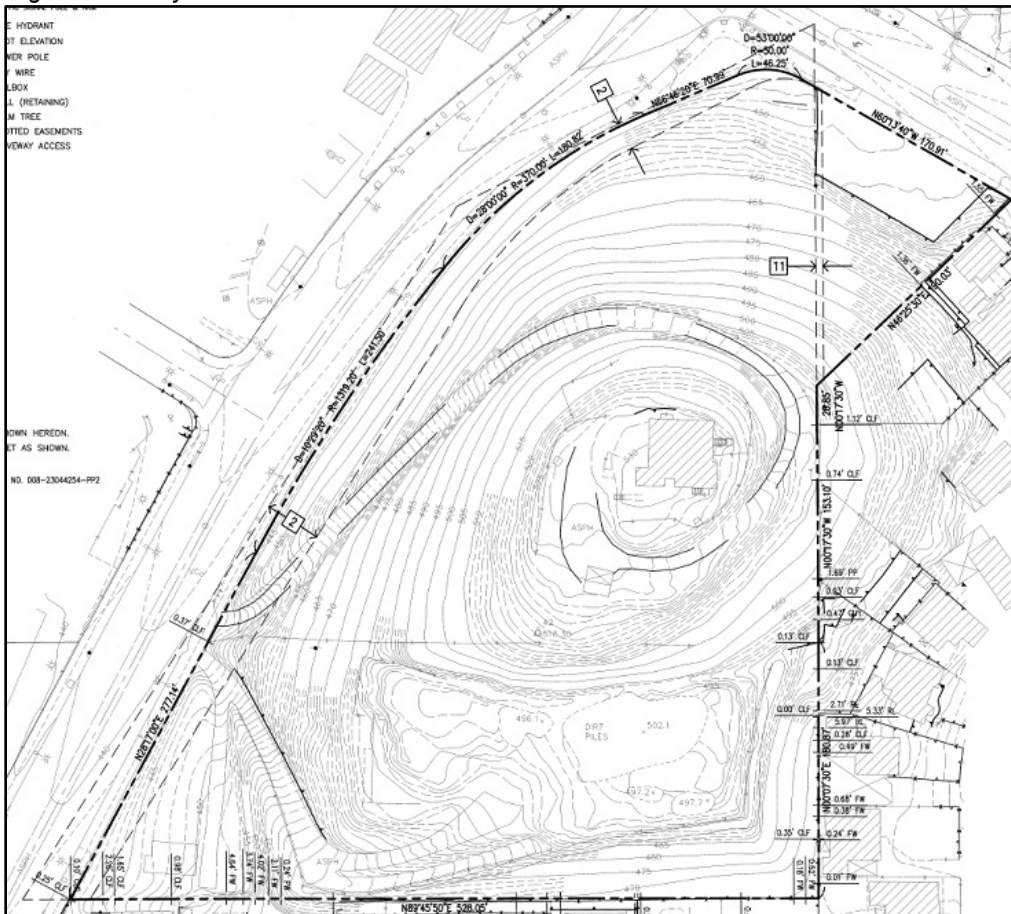
Zoning

The Project Site is located within the Northeast Los Angeles Community Plan area, which designates the site with a land use designation of Low Residential. The Community Plan lists the following zones as corresponding to the Low Residential land use designation: RE9, RS, R1, RU, RD6, and RD5. As previously shown in Figure 1, the southeast corner of the site is zoned [Q]R1-1D and the remaining approximately, 162,041 square feet of the site is zoned [Q]RD6-1D. The applicant has requested to change the zone for the entirety of the site to [Q]RD5-1D to construct a small lot development, which would not be permitted in the [Q]R1-1D zone.

As seen in Figure 3, a survey of the existing topography of the site shows that the southern, eastern, and northeastern portions of the site have a gradual slope. A portion of this area is within the boundaries of the [Q]R1-1D Zone. While small lot subdivisions would be permitted in the portion of the site zoned [Q]RD6-1D, a majority of that site within that zone has a steep slope. The requested Zone Change would allow the clustering of the development along the

portions of the site, which have a relatively less steep slope and would permit the preservation of the existing hillside along Eastern Avenue.

Figure 3: Survey



As recommended, the Project would be subject to the existing D Limitations of the Northeast Los Angeles Hillsides Ordinance. The height of the dwellings would be limited to either 26 or 30 feet depending on the slope of the roof. The maximum permitted Residential Floor Area (RFA) would be calculated utilizing the slope intervals and floor area formula for the applicable zone. In addition to establishing the maximum permitted RFA, the D Limitation establishes a minimum RFA that is permitted for the various zones regardless of the size or slope of the lot. For the recommended RD5 Zone, the D Limitation would permit a dwelling that has a minimum of 2,200 square feet of RFA.

While the applicant has requested the zone change be approved and adopted with a permanent Qualified Classification, the Department has recommended the zone change be adopted with a temporary Tentative and Qualified Classification. Pursuant to LAMC Section 12.32 G.1(h) and G.2(f), the site would remain in the temporary classification for up to six (6) years, excluding moratoriums and time extensions, and would then expire if substantial physical work was not completed in compliance with the Code. The zoning of the site would then revert back to the underlying zone upon the expiration of the subject zone change if it was adopted by City Council. The zone change has been recommended as such due to the unique nature of the site and would allow the Department to review any future project in the event that the subject Project is not constructed.

Density

The R1 Zone is a single-family zone which permits a maximum of one (1) dwelling unit on a lot regardless of the size of the lot. In order to construct more than one (1) dwelling unit on the portion of the lot that is zoned [Q]R1-1D, the lot would have to be subdivided into lots with a minimum lot area of 5,000 square feet. The RD5 and RD6 Zones are low density, multi-family zones which allows for the development of single-family and multi-family dwelling units. Multi-family development could consist of apartments, condominiums, or small lot subdivisions. Table 1 below provides a comparison of the minimum lot area and density provisions of the existing and recommended zone assuming the site is to be subdivided.

TABLE 1: LOT AREA COMPARISON			
	[Q]R1-1D	[Q]RD6-1D	(T)(Q)RD5-1D
Minimum Lot Area	5,000 SF	12,000 SF	10,000 SF
Minimum Lot Area per DU¹	1 per 5,000 SF	1 per 6,000 SF	1 per 5,000 SF
Maximum Density²	11 Lots ³	27 DUs ¹	43 DUs ¹
1. DU = Dwelling Unit 2. Maximum Density calculated based on the lot size of the subject site 3. Maximum Density for [Q]R1-1D calculated would require the subdivision of the lot.			

If the portion of the site that is zoned [Q]R1-1D was to be subdivided, it would permit a maximum of 11 lots which could each be constructed with a single-family dwelling. When taken into consideration with the maximum density of 27 dwelling units permitted in the [Q]RD6-1D Zone, the existing zoning would permit a maximum of 38 dwelling units. The recommended (T)(Q)RD5-1D would permit a maximum of 43 dwelling units, resulting in a net increase of five (5) dwelling units. The applicant proposes to develop the site with a maximum of 42 small lot homes in conjunction with the subdivision of land as shown on the approved vesting tentative tract map.

Northeast Los Angeles Hillside Ordinance – Q Conditions and D Limitations

As previously discussed, the site is subject to the Q Conditions and D Limitations adopted by Ordinance No. 180,403, also known as the Northeast Los Angeles Hillside Ordinance. While the applicant has requested modification and/or clarification of certain regulations contained within the Ordinance, the Project has been designed to comply with a majority of the regulations. Specifically the applicant has requested modifications to Q Condition Nos. 2(f) - Building Design, 3(a) - Retaining Walls, 5(d) and (e) - Environmental.

Q Condition 2(f) and 5(e).

New hardscape areas, not including foundations, shall utilize permeable paving systems including, but not limited to pavers, porous asphalt, porous concrete, grasscrete and/or similar materials as approved by LADBS.

Discussion:

The applicant has requested to clarify applicability of this condition as it relates to the common access driveways and private driveways. The common access driveway will serve to provide vehicular access for emergency vehicles such as fire trucks and service vehicles such as sanitation vehicles. Due to the slope and length of the common access driveway, the Fire Department may require it to be designated as a fire lane. As such, the common access driveway would have to be constructed with a material that is determined to be suitable by the Fire Department. As it relates to the private driveways, it is not anticipated that private driveways will have to serve emergency or service vehicles or that there would be difficulties constructing the private driveway in a similar manner as other private driveways. As such,

Planning Staff recommends providing a clarification as it relates to the common access driveway only.

Recommended language, new language shown in underline:

New hardscape areas, not including foundations and the common access driveways, shall utilize permeable paving systems including, but not limited to pavers, porous asphalt, porous concrete, grasscrete and/or similar materials as approved by LADBS.

Q Condition 3(a).

The maximum total height of all freestanding retaining walls shall not exceed 12 feet, with no individual wall measuring higher than 6 feet on private property. Each freestanding retaining wall shall not exceed 75 feet in linear length, nor extend beyond one lot. Walls shall be separated by a minimum horizontal distance equal to the height of the highest wall. Freestanding garden walls 36" in height or less shall not be considered retaining walls for the purposes of this regulation.

Discussion:

As implemented, the Q Condition does not specifically limit the number of retaining walls that are permitted, but rather limits the height and length of the walls. While this takes into consideration hillside properties which may require multiple retaining walls, the number of retaining walls is limited by LAMC Section 12.21 C.8. As such, projects may also require the approval of a Zoning Administrator's Determination (ZAD) pursuant to LAMC Section 12.24 X.26 despite complying with the height and length limitations of the Q Condition.

In conjunction with the requested modification to the Q Condition, the applicant has also requested a ZAD pursuant to LAMC Section 12.24 X.26 due to the proposed number of retaining walls. LAMC Section 12.21 C.8 establishes regulations which universally apply to all lots Citywide. As the Code cannot universally apply to all lots Citywide, the regulations included an established procedure, Section 12.24 X.26, to allow for consideration of additional walls and increased heights depending on the nature of the lot. As LAMC Section 12.21 C.8 regulates both height and number of the retaining walls and the decision maker has the authority to consider both aspects of retaining walls under LAMC Section 12.24 X.26, Planning Staff has recommended that the Q Condition be modified to require compliance with requirements of LAMC Section 12.21 C.8 and that any deviations from the regulations be considered by the ZAD process. As it relates to the freestanding garden walls, per LADBS Information Bulletin Document No. P/BC 2020-002, walls which are four (4) feet high or less and comply with the specifications contained within the bulletin are not considered retaining walls and are not subject to LAMC Section 12.21 C.8.

Recommended language, deleted language shown in ~~strikeout~~ and new language shown in underline:

~~The maximum total height of all freestanding retaining walls shall not exceed 12 feet, with no individual wall measuring higher than 6 feet on private property. Each freestanding retaining wall shall not exceed 75 feet in linear length, nor extend beyond one lot. Walls shall be separated by a minimum horizontal distance equal to the height of the highest wall. Freestanding garden walls 36" in height or less shall not be considered retaining walls for the purposes of this regulations.~~ Retaining walls, as defined under LAMC Section 12.21 C.8, shall be subject to the regulations contained in LAMC Section 12.21 C.8. Deviations from the regulations shall require the approval of a Zoning Administrator's Determination under LAMC Section 12.24 X.26.

Q Condition No. 5(d).

Grading shall be limited to a maximum of 500 cubic yards + numeric value equal to 5 percent of the total lot size, up to a maximum of 1,000 cubic yards total. Any deviations beyond these limits shall require a Zoning Administrator's approval under LAMC Section 12.27.

Discussion:

As it relates to grading, the Project proposes approximately 82,000 cubic yards of cut, 2,000 cubic yards of fill, 8,000 cubic yards of removal and replacement, and the export of up to 78,000 cubic yards of dirt. The proposed amount of grading would include the grading required to construct the common access driveway, preparation of building sites for each of the 42 small lot homes, and the removal of uncertified fill. As stated by the applicant, the site contains a substantial amount of uncertified fill from previous illegal dumping that has occurred at the site. The uncertified fill is required to be removed as required by the conditions of the Geology and Soils Report Approval Letter Log No. 109928 dated October 4, 2019 issued by the Grading Division of the Department of Building and Safety.

The Northeast Los Angeles Hillsides Ordinance universally applies to all lots located within the boundaries identified by the Ordinance. The Ordinance established a formula for calculating the maximum amount of grading that is permitted based on the lot size, but also limits the maximum total to 1,000 cubic yards. As the lots located within the boundaries of the Ordinance are all unique and may consist of varying slopes, the Q Condition cannot universally apply to all lots within the boundaries of the Ordinance. Anticipating this, the regulation was adopted with an established procedure, LAMC Section 12.27, to allow for consideration of additional grading. Due to the extensive amount of grading that is proposed for the Project, the applicant has requested the modification of the condition in lieu of requesting a Variance.

Recommended language, deleted language shown in ~~strikeout~~ and new language shown in underline:

~~Grading shall be limited to a maximum of 500 cubic yards + numeric value equal to 5 percent of the total lot size, up to a maximum of 1,000 cubic yards total. Any deviations beyond these limits shall require a Zoning Administrator's approval under LAMC Section 12.27.~~
soils cut of up to 82,000 cubic yards, fill of up to 2,000 cubic yards, removal and replacement of up to 8,000 cubic yards, and soils export of up to 79,000 cubic yards.

Removal of Protected Trees

The Conceptual Landscape Plan, included as part Exhibit A, identifies the location of the existing Protected and non-Protected Trees to remain and those which are proposed to be removed. While the Black Walnut trees are located throughout the site, they are primarily located along the steepest portion of the site along Eastern Avenue. When the Project was originally submitted to the Department, the dwelling units were proposed to be located throughout the site, which would have required the removal of 68 of the 102 Black Walnuts. Members of the surrounding community, as well as the Santa Monica Conservancy and the Community Forest Advisory Committee cited concerns regarding the loss of native trees.

A revised site plan was submitted to the Department on November 1, 2016 which consolidated the location of the dwelling units along the southern, eastern, and northeastern portions of the site. The clustering of the dwelling units along the edge of the site resulted in a reduction in the number of proposed Black Walnuts to be removed from 68 to 39 Protected Trees. In a letter dated May 22, 2017, the Santa Monica Mountains Conservancy submitted a letter reaffirming its opposition to the redesign of the Project stating that a project should avoid the removal of protected California black walnut trees. In a letter dated July 7, 2016, the Community Forest Advisory Committee submitted a letter stating their concerns regarding the proposed removal of

68 Protected Trees and provided comments related to the proposed mitigation measures, including suggesting that an Environmental Impact Report should be required.

Prior to the public hearing, a member of the public inquired about Black Walnut trees located at the corner of Eastern Avenue and Lombardy Boulevard. The inquiry was related to whether the trees had grown since the original preparation of the Tree Inventory Report and if they now qualified as Protected Trees. At the joint public hearing, the Hearing Officer requested that the applicant obtain a tree expert to determine if the status of the trees had changed. In a letter dated January 31, 2020 prepared by Greg Applegate, Registered Consulting Arborist #365 of Arborgate Consulting, Inc., the arborist stated that the trees had not grown to a size which would qualify them as Protected Trees.

The removal of the trees was analyzed as part of the MND, which identified mitigation measures to reduce impacts to a less than significant level. The mitigation measures include requirements such as the replacement of a removed Protected Tree with a minimum of four (4) Protected Trees with a minimum size of 15 gallons and the replacement of any non-Protected Tree with a diameter of eight (8) inches or greater at a one to one ratio. In addition to compliance with identified mitigation measures, the Project would also be required to obtain approval from the Board of Public Works for the removal of any Protected or street tree and would be subject to the conditions of that approval.

Open Space

Members of the surrounding community provided comments regarding the lack of open space within the area and stated that the site is an ideal location for a park or other amenities for the community. In a letter dated May 22, 2017, the Santa Monica Mountains Conservancy reaffirmed its opposition to the redesign of the Project, stating that the Project is receiving a significant up-zoning and is not providing public or protected natural open space. Additionally, the Conservancy stated that the approval of the project should require substantial, onsite fee simple open space dedication and a permanent revenue stream from the homeowner's association to maintain it.

The Project Site is a privately owned, vacant lot which is designated by the Northeast Los Angeles Community Plan for Low Residential land uses and was anticipated for the development of single- and multi-family dwellings. The requested RD5 Zone is consistent with the land use designation and would permit a net increase of five (5) dwelling units than the maximum permitted by the existing zones. As conditioned in Case No. VTT-73531-SL, Condition No. 15, and as recommended herein under the Tentative "T" Classification, the applicant would be required to pay Quimby Fees to the Department of Recreation and Parks. As the small lot subdivision creates individual, fee simple lots, the establishment of a Homeowner's Association is not required; however, due to the shared access and easements associated with small lot developments, Condition No. 18(h) of Case No. VTT-73531-SL requires the establishment of a Community Maintenance Agreement for maintenance of common areas, including on-site trees and landscaping. The Zone Change would not reduce existing open space in the area, nor would it reduce future anticipated open space in the area as the site has not been designated or zoned for open space.

The Project Site is located across the street from the El Sereno Park and Recreation Center which includes facilities for various indoor and outdoor sports. Additionally, the site is located approximately 1.1 miles east of Ascot Hills Park, which includes a small outdoor amphitheater and hiking trails.

Privacy and Over-In-Height Fences

The Project proposes to locate the small lot homes primarily along the southern and eastern property lines, which abut lots that are developed with single-family dwellings. Comments that were received from some of the adjacent property owners included concerns related to the loss of privacy due to the close proximity of the dwelling units to the property line and height of the buildings. While the City does not regulate privacy, the Zoning Code does contain yard requirements for residentially zoned lots. The yard regulations require that buildings observe minimum setbacks from the property lines for purposes of maintaining access to light and air. Inherently, the setbacks provide a sense of privacy.

Due to the irregular configuration of the site, it is anticipated that the southern property line would be considered the Side Lot Line and the eastern property line would be considered the Rear Lot Line. These lot lines directly adjoin lots which are zoned [Q]R1-1D and are developed with single-family dwellings. As the Northeast Los Angeles Hillside Ordinance does not regulate yards, a by-right single-family development of the site would be subject to the yard requirements in LAMC Section 12.21 C.10 (*Baseline Hillside Ordinance*), Section 12.21 A.17 (*One-Family Dwellings, Accessory Buildings and Additions. Hillside Regulations*), and Section 12.09.1 B.2 (*RD Zone – Rear Yards*). However, as a small lot development, the Project would be exempt from providing the yards required of the zone and applicable hillside regulations and could provide the minimum five (5) permitted in LAMC Section 12.22 C.27 as adopted by Ordinance No. 176,354.

The maximum height of the small lot homes is proposed at 26 feet. Table 2, below, provides a comparison of the yard and setback requirements along the southern and eastern property lines as required by the various zones for the proposed height of the buildings, the Small Lot Subdivision Ordinance, and as proposed by the Project.

TABLE 2: SIDE AND REAR YARD COMPARISON					
	[Q]R1-1D (12.21 C.10)	[Q]RD6-1D (12.21 A.17 and 12.09.1 (B)(3))	[Q]R5-1D (12.21 A.17 and 12.09.1 (B)(3))	12.22 C.27 (Small Lot Subdivision)	Proposed
Southern P/L¹ (Side Yard) ²	6 Feet	6 Feet	6 Feet	5 Feet	12 – 23 Feet
Eastern P/L¹ (Rear Yard) ²	15 Feet	25 Feet	25 Feet	5 Feet	13 to 18 Feet
1. P/L = Property Line 2. Designation of Yards based on boundary of the site and not the individual small lots					

As shown in the approved tract map and setback matrix, the small lot homes located along the southern property line would vary between 12 feet to 23 feet and those located along the eastern property line would vary between 13 to 18 feet. The setbacks that have been provided would allow the use of that area to function as private open space for each of the small lot homes, similar to the yards of adjacent single-family dwellings. In addition to the retaining walls and privacy/security fences proposed by the applicant, during the Joint Public Hearing, the applicant was amenable to the Hearing Officer's proposal to provide additional landscaping along the southern and eastern property lines to address the concerns of the adjoining neighbors.

As previously discussed, the Project proposes to construct retaining walls with privacy/security fences on top of the retaining walls. A portion of the retaining walls and privacy/security fences will be located within the required Front, Side, and Rear Yards of the site and would be subject to the fence height limitations pursuant to LAMC Section 12.22 C.20(f). While retaining walls would vary in height, the applicant has requested that the privacy/security fences having a height of up to six (6) feet be permitted to be constructed on top of up to 103 retaining walls.

This would result in a potential maximum height of 12 feet. In order to maintain visibility of the site, connectivity with the public right-of-way, and consistency with the existing Q Condition, Planning Staff has recommended that the retaining walls and privacy/security fences constructed on top of the retaining walls be limited to a total combined height of six (6) feet.

Conclusion

Based on the information submitted, the surrounding uses, and good planning and zoning practices, the Project would redevelop the undeveloped and underutilized site with 42 new fee simple, residential dwelling units. As proposed, the Project would be consistent with a number of goals, objectives, and policies of the Northeast Los Angeles Community Plan Area. Staff recommends that the Area Planning Commission find that the Project was adequately analyzed in the Mitigated Negative Declaration prepared for the Project and approve and recommend adoption of the Zone Change and the approval of the Zoning Administrator's Determination and Zoning Administrator's Adjustment.

CONDITIONS FOR EFFECTUATING (T) TENTATIVE CLASSIFICATION REMOVAL

Pursuant to Section 12.32 G of the Municipal Code, the (T) or [T] Tentative Classification shall be removed by the recordation of a final parcel or tract map or by posting of guarantees through the B-permit process of the City Engineer to secure the following without expense to the City of Los Angeles, with copies of any approval or guarantees provided to the Department of City Planning for attachment to the subject planning case file.

1. Dedications and Improvements. Prior to the issuance of any building permits, public improvements and dedications for streets and other rights-of-way adjoining the subject property shall be guaranteed to the satisfaction of the Bureau of Engineering, Department of Transportation, Fire Department (and other responsible City, regional, and Federal government agencies as may be necessary).

A. Responsibilities/Guarantees.

- (1) As part of early consultation, plan review, and/or project permit review, the applicant/developer shall contact the responsible agencies to ensure that any necessary dedications and improvements are specifically acknowledged by the applicant/developer.
- (2) Prior to the issuance of sign-offs for final site plan approval and/or project permits by the Department of City Planning, the applicant/developer shall provide written verification to the Department of City Planning from the responsible agency acknowledging the agency's consultation with the applicant/developer. The required dedications and improvements may necessitate redesign of the project. Any changes to the project design required by a public agency shall be documented in writing and submitted for review by the Department of City Planning.

- B. Dedication Required. That a 3-foot wide strip of land be dedicated along Eastern Avenue adjoining the tract to complete a 43-foot wide right-of-way in accordance with Avenue II Standards of LA Mobility Plan including a 20-foot radius property line return or a 15-foot by 15-foot property cut corner at the intersection with Lombardy Boulevard.

C. Improvement Required.

- (1) Improve Eastern Avenue being dedicated and adjoining the subdivision by the construction of a new concrete curb 2-foot wide concrete gutter and a full-width concrete sidewalks with tree wells including any necessary removal and reconstruction of existing improvements.
 - (2) Improve Lombardy Boulevard by reconstruction of the existing concrete curb and gutter and reconstruct the existing 5-foot sidewalk and landscaping of the parkway necessary removal and reconstruction of existing improvements.
 - (3) Construct the necessary on-site mainline sewers satisfactory to the City Engineer.
 - (4) Construct any necessary retaining walls satisfactory to the City Engineer.
2. Department of Transportation. Suitable arrangements shall be made with the Department of Transportation to assure that a parking area and driveway plan be submitted to the Citywide Planning Coordination Section of the Department of Transportation for approval prior to

submittal of building permit plans for plan check by the Department of Building and Safety. Transportation approvals are conducted at 201 N. Figueroa Street Suite 400, Station 3. For an appointment, call (213) 482-7024.

3. Street Lighting: No street lighting improvements if no street widening per BOE improvement conditions. Otherwise relocate and upgrade street lights; nine (9) on Eastern Avenue and one (1) on Lombardy Boulevard

Notes:

The quantity of street lights identified may be modified slightly during the plan check process based on illumination calculations and equipment selection.

Conditions set: 1) in compliance with a Specific Plan, 2) by LADOT, or 3) by other legal instrument excluding the Bureau of Engineering condition S-3 (i), requiring an improvement that will change the geometrics of the public roadway or driveway apron may require additional or the reconstruction of street lighting improvements as part of that condition.

4. Urban Forestry – Street Trees:

- A. The applicant shall submit a tree report and a landscape plan prepared by a Tree Expert, as required by LAMC Ordinance No. 177,404, for approval by the City Planning Department and the Urban Forestry Division, Bureau of Street Services. The Tree Report shall contain the Tree Expert's recommendation for the preservation of as many desirable (eight inches diameter or greater) trees as possible and shall provide species, health, and condition of all trees with tree locations plotted on a site survey. Any on-site 1:1 tree replacement shall be required for the unavoidable loss of any desirable on-site trees.
- B. Plant street trees and remove any existing trees within dedicated streets or proposed dedicated streets as required by the Urban Forestry Division of the Bureau of Street Services. Parkway tree removals shall be replanted at a 2:1 ratio. All street tree plantings shall be brought up to current standards. When the City has previously been paid for tree planting, the subdivider or contractor shall notify the Urban Forestry Division (213-847-3077) upon completion of construction to expedite tree planting.

Note:

Removal of Protected trees requires the approval of the Board of Public Works.

Removal or planting of any tree in the public right-of-way required approval of the Board of Public Works.

Contact Urban Forestry Division at (213)847-3077 for permit information. CEQA documents must address removal of protected trees and parkway trees.

5. Department of Recreation and Parks. That the Quimby fee be based on the RD5 Zone.

Notice: If conditions dictate, connections to the public sewer system may be postponed until adequate capacity is available.

Notice: Certificates of Occupancy for the subject property will not be issued by the City until the construction of all the public improvements (streets, sewers, storm drains, etc.) as required herein, are completed to the satisfaction of the City Engineer.

(Q) QUALIFIED CONDITIONS

Pursuant to Section 12.32 G of the Municipal Code, the following limitations are hereby imposed upon the use of the subject property, subject to the "Q" Qualified classification.

1. **Site Plan.** The use and development of the subject property shall be in substantial conformance with the site plan labeled Exhibit "A" dated July 23, 2019. Prior to the issuance of building permits, detailed development plans including a site plan illustrating elevations, facades, and architectural treatment, and a landscape/irrigation plan shall be submitted for review and approval by the Central Project Planning Bureau of the Department of City Planning. The plans shall comply with provisions of the Municipal Code, the subject conditions, and the intent of the subject permit authorization.
2. **Building Design.** New hardscape areas, not including foundations and the common access driveways, shall utilize permeable paving systems including, but not limited to pavers, porous asphalt, porous concrete, grasscrete and/or similar materials as approved by LADBS.
3. **Retaining Walls.** Retaining walls, as defined under LAMC Section 12.21 C.8, shall be subject to the regulations contained in LAMC Section 12.21 C.8. Deviations from the regulations shall require the approval of a Zoning Administrator's Determination under LAMC Section 12.24 X.26.
4. **Grading.** Grading shall be limited to a maximum soils cut of up to 82,000 cubic yards, fill of up to 2,000 cubic yards, removal and replacement of up to 8,000 cubic yards, and soils export of up to 79,000 cubic yards.
5. In addition to those Q Conditions contained in this Ordinance, the [Q] Conditions and D Limitations pursuant to Ordinance No. 180,403 shall be retained, except for Conditions Nos. 2(f), 3(a), 5(d) and (e).

CONDITIONS OF APPROVAL

Pursuant to Section 12.24 and 12.28 of the Los Angeles Municipal Code, the following conditions are hereby imposed upon the use of the subject property:

A. Development Conditions

1. **Site Development.** The use and development of the subject property shall be in substantial conformance with the plans labeled Exhibit "A" dated August 23, 2019. Prior to the issuance of building permits, detailed development plans including a site plan illustrating elevations, facades, architectural treatment, retaining walls, privacy/security fence, and a landscape/irrigation plan shall be submitted for review and approval by the Department of City Planning. The plans shall comply with provisions of the Municipal Code, the subject conditions, and the intent of the subject permit authorization.
2. **Use.** Development of the site shall be limited to a maximum of 42 residential dwelling units.
3. **Retaining Walls.** Approved herein is the construction of up to 178 retaining walls to be located throughout the site subject to the following conditions:
 - a. The location and height of the retaining walls shall be in substantial conformance with the Retaining Wall Plan stamped and labeled Exhibit "A" dated August 23, 2019. Each retaining wall permit shall include the following information, prepared by the applicant, to the satisfaction of the Department of City Planning:
 - i. a copy of the stamped approved Retaining Wall Plan;
 - ii. a site plan which shall clearly label and identify the location of the retaining wall(s), including all freestanding retaining or slough walls of four (4) feet or less;
 - iii. if a privacy/security fence is proposed to be installed on top of the retaining wall, then this should be clearly identified on the plans and permit description;
 - iv. an updated inventory of issued and pending permit applications for the construction of the retaining walls; and
 - v. an inventory of all freestanding retaining or slough walls of four (4) feet or less which may not require a permit by the Department of Building and Safety.
 - b. The 178 retaining walls shall include freestanding retaining walls as defined in LAMC Section 12.21 C.8 and those walls which are determined to be freestanding retaining or slough walls of four (4) feet or less.
 - c. The retaining walls may have a maximum height of six (6) feet and shall be in substantial conformance with the heights identified in the Retaining Wall Plan stamped and labeled Exhibit "A" dated August 23, 2019.
 - d. The retaining walls shall maintain a minimum horizontal distance of three (3) feet.

- e. A privacy/security fence may be constructed on top of the retaining walls. The total combined height of the retaining wall and privacy/security fence shall not exceed six (6) feet.
 - f. All of the retaining walls with a height of four (4) feet or higher shall be designed with materials and colors matching the design of the dwelling units.
 - g. All of the retaining walls with a height of four (4) feet shall be screened with landscaping in accordance with LAMC Section 12.21 C.8 and Condition No. 5.
4. **Retaining Walls and Privacy/Security Fences.** Approved herein is the construction of retaining walls and privacy/security fences on top of retaining walls which are located within the required front, side, and rear yards as measured from the boundary of the subdivision. This does not apply to the front, side, and rear yards of the individual small lots. The privacy/security fences shall be subject to the following conditions:
- a. Freestanding retaining walls or retaining walls that have a privacy/security fence constructed on top of the retaining walls, as shown on the stamped approved Retaining Wall Plan, may have a maximum height of six (6) feet.
 - b. No privacy/security fence shall be permitted to be constructed on top of Retaining Walls Nos. 1 and 175.
 - c. No freestanding privacy/security fence higher than 42 inches may be permitted within the front yard, along Eastern Avenue and Lombardy Boulevard, as measured from the boundary of the subdivision.
 - d. All of the privacy/security fences shall be designed with materials and colors matching the design of the dwelling units. If the privacy/security fences are constructed of slumpstone, masonry, or similar material as the retaining wall, then it shall be landscaped in the same manner as retaining wall it is located on top of in compliance with Condition No. 5.
5. **Landscaping.** Prior to the issuance of a building permit, a landscape plan, prepared by a licensed landscape architect, shall be submitted to and approved by the Department of City Planning. The landscape plan shall be in substantial conformance with the stamped approved Conceptual Landscape Plan.
- a. **Protected Trees.**
 - i. The removal of any Protected Tree shall require the approval of the Board of Public Works. A copy of the Board of Public Works action related to a request to remove a Protected Tree shall be submitted to the Department of City Planning to be included in the administrative record.
 - ii. No more than 39 Protected Trees shall be permitted to be removed by the Board of Public Works.
 - iii. Replacement of Protected Trees shall be completed on-site in substantial conformance with the Conceptual Landscape Plan stamped and labeled Exhibit "A" dated August 23, 2019 or as modified by the Board of Public Works. At a minimum, the removal of a Protected Tree shall comply with the replacement requirements of Condition No. 9 or as required by the Board of Public Works.

- iv. Maintenance of the Protected Trees, regardless of location within the subdivision, shall be included as part of the Community Maintenance Agreement required for the small lot development.
 - b. Removal of non-Protected Trees shall comply with the replacement requirements identified in Condition No. 9.
 - c. Retaining walls with a height of four (4) feet or higher shall be screened with landscaping in compliance with LAMC Section 12.21 C.8(b). The landscaping shall screen all visible portions of the retaining wall, including any privacy/security fence that is constructed of the same or similar material as the retaining wall.
 - d. Trees shall be planted along the southern and eastern boundary of the site, abutting the [Q]R1-1D and [Q]RD6-1D zones. The trees shall be planted at a minimum distance of three (3) feet apart.
6. **Parking.** Vehicular and bicycle parking shall be provided in accordance with the LAMC.
7. **Mechanical Equipment.** All mechanical equipment on the roof shall be screened from view.
8. **Lighting.** Outdoor lighting shall be designed and installed with shielding, such that the light source cannot be seen from adjacent residential properties, the public right-of-way, nor from above.

B. Environmental Conditions

9. Aesthetics.
- a. Non-Protected Trees.
 - i. Prior to the issuance of any permit related to development of the Project, a plot plan shall be prepared for the Project, indicating the location, size, type, and general condition of all existing trees on the Project Site and within the adjacent public right(s)-of-way.
 - ii. All significant (8-inch or greater trunk diameter, or cumulative trunk diameter if multi-trunked, as measured 54 inches above the ground) non-protected trees on the Project Site proposed for removal shall be replaced at a 1:1 ratio with a minimum 24-inch box tree. Net new trees located within the parkway of the adjacent public right(s)-of-way may be counted toward replacement tree requirements.
 - iii. Removal of planting of any tree in the public right-of-way shall require approval of the Board of Public Works. All trees in the public right-of-way shall be provided in the current standards of the Urban Forestry Division of the Department of Public Works, Bureau of Street Services.
 - b. Protected Trees.
 - i. All protected tree removals shall require approval from the Board of Public Works.

- ii. A Tree Report shall be submitted to the Urban Forestry Division of the Bureau of Street Services, Department of Public Works, for review and approval prior to implementation of the Report's recommended measures.
 - iii. According to the City's Protected Tree Ordinance, a minimum of four protected trees (a minimum of 15 gallons in size) shall be planted for each protected tree that is removed. The size of each replacement tree shall measure at least one inch or more in diameter at a point one foot above the base, and not less than seven feet in height, measured from the base.
 - iv. In consultation with the Division of Urban Forestry, twenty five-percent of the Protected trees removed shall be replaced with 15 gallon Juglans Californica.
 - v. The location of trees planted for the purpose of replacing a removed protected tree shall be clearly indicated on the required landscape plan, which shall also indicate the replacement tree species and further contain the phrase "Replacement Tree" in its description.
- c. Previously Removed Trees. The 8 qualifying removed protected trees shall be replaced at a ratio of one Juglans Californica and three Quercus for each of the 8 trees, at a minimum of 15 gallons in size. The specific size and species of the trees to be planted as replacement for the protected trees being removed shall be determined by the Urban Forestry Division.
- d. All Trees.
- i. Protected Barrier: A protection barrier shall be installed around the construction area as shown on the map included in the Tree Preservation Report (refer to Appendix A). The barrier shall be 6-foot-high chain link fencing. Twelve-inch-high slit fence shall be attached to the base of the fence with the bottom edge buried 1-2 inches. The barrier may be placed on the line shown on the map or closer to the construction, but not further. The fencing shall not be removed, relocated, or encroached upon without permission of the arborist involved.
 - ii. Storage of materials: There shall be no storage of materials or supplies of any kind inside the area of the protection fencing. Concrete and cement materials, block, sand and soil shall not be placed within the drip-line of any tree to remain.
 - iii. Fuel Storage: Fuel Storage shall not be permitted within 150 feet of any tree to be preserved. Refueling servicing and maintenance of equipment and machinery shall not be permitted within 150 feet of protected trees.
 - iv. Debris and Waste Materials: Debris and waste from construction of other activities shall not be permitted outside the construction area. Wash down of concrete or cement handling equipment, in particular, shall not be permitted within 150 feet of protected trees.
 - v. Planting near Trees Designated for Protection: Any digging within designated protection zones shall done using supersonic air directly as the digging medium, by means of a nozzle, whose nominal rated input pressure (available from manufacturer's literature) must not exceed 130 psig (pounds per square inch at gage) unless otherwise approved. Nozzles designed for input above 130 psig

can damage fine roots. Air compressors rated between 100 to 125 psig recommended.

- vi. Grade Changes: Any grade changes within the protection radius listed should be approved by a Registered Consulting Arborist before construction begins, and precautions taken to mitigate potential injuries. Grade changes can be particularly damaging to trees. Even as little as two inches of fill can cause the death of a tree. Lowering the grade can destroy major portions of a root system.
- vii. Damages: Any tree damages or injuries should be reported to the project arborist as soon as possible. Severed roots shall be cut cleanly to healthy tissue, using proper pruning tools. Broken branches or limbs shall be pruned according to International Society of Arboriculture Pruning Guidelines and ANSI A-300 Pruning Standards.
- viii. Preventing Measures: Pruning of tree canopies and branches should be done at the direction of the project arborist to remove any dead or broken branches, and to provide any necessary clearances for the construction work or equipment.

10. Air Quality.

- a. All off-road construction equipment greater than 50 hp shall meet U.S. EPA Tier 4 emission standards, where available, to reduce NO_x, PM₁₀, PM_{2.5} emissions at the Project Site. In addition, all construction equipment shall be outfitted with Best Available Control Technology devices certified by CARB. Any emissions control device used by the contractor shall achieve emissions reductions that are no less than what could be achieved by a Level 3 diesel emissions control strategy for a similarly sized engine as defined by CARB regulations.
- b. Require the use of 2010 and newer diesel haul trucks (e.g., material delivery trucks and soil import/export) and if the Lead Agency determines that 2010 model year or newer diesel trucks cannot be obtained, the Lead Agency shall require trucks that meet U.S. EPA 2007 model year NO_x emissions requirements.
- c. At the time of mobilization of each applicable unit of equipment, a copy of each unit's certified tier specification, BACT documentation, and CARB or SCAQMD operating permit shall be provided.
- d. Encourage construction contractors to apply for SCAQMD "SOON" funds. Incentives could be provided for those construction contractors who apply for SCAQMD "SOON" funds. The "SOON" program provides funds to accelerate clean up of off-road diesel vehicles, such as heavy duty construction equipment. More information on this program can be found at: <http://www.aqmd.gov/home/programs/business/business-detail?title=off-road-dieselengines&parent=vehicle-engine-upgrades>
- e. Construction activities shall comply with SCAQMD Rule 403, including the following measures:
 - i. Apply water to disturbed areas of the site three times a day.
 - ii. Require the use of a gravel apron or other equivalent methods to reduce mud and dirt trackout onto truck exit routes.

- iii. Appoint a construction relations officer to act as a community liaison concerning on-site construction activity including resolution of issues related to PM generation.
- iv. Limit soil disturbance to the amounts analyzed in the Final MND.
- v. All materials transported off-site shall be securely covered.
- vi. Apply non-toxic soil stabilizers according to manufacturers' specifications to all inactive construction areas (previously graded areas inactive for ten days or more).
- vii. Traffic speeds on all unpaved roads to be reduced to 15 mph or less.

11. Biological Resources.

- a. To avoid potential significant impacts to roosting bats, one of the following shall be implemented by the Project Applicant:
 - i. Tree removal shall occur between March 1st and July 31st or
 - ii. If tree removal would occur between August 1st and February 28th/29th, the Project Applicant shall retain a qualified bat biologist to conduct a roosting bat survey. If evidence of bats is present, then removal of occupied roost trees shall not occur until the biologist determines that the roost is no longer in use through repeated nocturnal surveys. The results of the survey(s) shall be provided to the Department of Public Works prior to removal of any protected trees.
- b. To avoid potential significant impacts to nesting birds, including migratory birds and raptors, one of the following shall be implemented by the Project Applicant:
 - i. Conduct vegetation removal associated with construction from September 1st through January 31st, when birds are not nesting. Initiate grading activities prior to the breeding season (which is generally February 1st through August 31st) and keep disturbance activities constant throughout the breeding season to prevent birds from establishing nests in surrounding habitat (in order to avoid possible nest abandonment); if there is a lapse in activities of more than five days, pre-construction surveys shall be necessary as described in the bullet below, or
 - ii. Conduct pre-construction surveys for nesting birds if vegetation removal or grading is initiated during the nesting season. A qualified wildlife biologist shall conduct weekly pre-construction bird surveys no more than 30 days prior to initiation of grading to provide confirmation on the presence or absence of active nests in the vicinity (at least 300 to 500 feet around the individual construction site, as access allows). The last survey should be conducted no more than three days prior to the initiation of clearance/construction work. If active nests are encountered, clearing and construction in the vicinity of the nests shall be deferred until the young birds have fledged and there is no evidence of a second attempt at nesting. A minimum buffer of 300 feet (500 feet for raptor nests) or as determined by a qualified biologist shall be maintained during construction depending on the species and location. The perimeter of the nest-setback zone shall be fenced or adequately demarcated with staked flagging at 20-foot

intervals, and construction personnel and activities restricted from the area. Construction personnel should be instructed on the sensitivity of the area. A survey report by the qualified biologist documenting and verifying compliance with the mitigation and with applicable state and federal regulations protecting birds shall be submitted to the City and County, depending on within which jurisdiction the construction activity is occurring. The qualified biologist shall serve as a construction monitor during those periods when construction activities would occur near active nest areas to ensure that no inadvertent impacts on these nests would occur.

12. Noise.

- a. The Project shall comply with the City of Los Angeles Building Regulations Ordinance No. 178048, which requires a construction site notice to be provided that includes the following information: job site address, permit number, name and phone number of the contractor and owner or owner's agent, hours of construction allowed by code or any discretionary approval for the site, and City telephone numbers where violations can be reported. The notice shall be posted and maintained at the construction site prior to the start of construction and displayed in a location that is readily visible to the public.
- b. Two weeks prior to commencement of construction, notification shall be provided to the off-site residential and school uses within 500 feet of the Project site that discloses the construction schedule, including the types of activities and equipment that would be used throughout the duration of the construction period.
- c. Temporary sound barriers, capable of achieving a sound attenuation of at least 10 dBA (e.g., construction sound wall with sound blankets), and capable of blocking the line-of-sight to the adjacent residences shall be installed as feasible.
- d. Noise-generating construction equipment operated at the Project Site shall be equipped with effective state-of-the-art noise control devices, i.e., mufflers, lagging, solar power or electric plug-in on-site power generators and/or motor enclosures or other shielding equipment. All equipment shall be properly maintained to assure that no additional noise, due to worn or improperly maintained parts, would be generated.
- e. All construction areas for staging and warming-up equipment shall be located as far as possible from adjacent residences.
- f. Portable noise sheds for smaller, noisy equipment, such as air compressors, dewatering pumps, and generators shall be provided where feasible.
- g. A haul route for exporting cut materials from the site to a nearby landfill that access the San Bernardino and/or Long Beach Freeways should minimize travel on residential streets with sensitive receptors.

13. Transportation/Traffic.

- a. Hillside Construction Staging and Parking Plan. Prior to the issuance of a grading or building permit, the applicant shall submit a Construction Staging and Parking Plan to the Department of Building and Safety and the Fire Department for review and approval. The plan shall identify where all construction materials, equipment, and vehicles will be stored through the construction phase of the project, as well as

where contractor, subcontractor, and laborers will park their vehicles so as to prevent blockage of two-way traffic on streets in the vicinity of the construction site. The Construction Staging and Parking Plan shall include, but not be limited to, the following:

- i. No construction equipment or material shall be permitted to be stored within the public right-of-way.
 - ii. If the property fronts on a designated Red Flag Street, on noticed "Red Flag" days, all the workers shall be shuttled from an off-site area, located on a non-Red Flag Street, to and from the site in order to keep roads open on Red Flag days.
 - iii. During the Excavation and Grading phases, all haul trucks shall be staged on the Project site. The drivers shall be required to follow the designated travel plan or approved Haul Route.
 - iv. Truck traffic directed to the project site for the purpose of delivering materials, construction-machinery, or removal of graded soil shall be limited to off-peak traffic hours, Monday through Friday only. No truck deliveries shall be permitted on Saturdays or Sundays.
 - v. All deliveries during construction shall be coordinated so that all vendor/delivery vehicles will stage and make deliveries on the project site, and that a construction supervisor is present at such time.
 - vi. A radio operator shall be on-site to coordinate the movement of material and personnel, in order to keep the roads open for emergency vehicles, their apparatus, and neighbors.
 - vii. During all phases of construction, all construction vehicle parking and queuing related to the project shall be as required to the satisfaction of the Department of Building and Safety, and in substantial compliance with the Construction Staging and Parking Plan, except as may be modified by the Department of Building and Safety or the Fire Department.
- b. Construction Activity Near Schools.
- i. The Project developer and contractors shall maintain ongoing contact with administrators of the Farmdale Elementary School and the El Sereno Middle School. The administrative offices shall be contacted when demolition, grading and construction activity begin on the Project site so that students and their parents will know when such activities are to occur. The developer shall obtain school walk and bus routes to the schools from either the administrators or from the LAUSD's Transportation Branch (323) 342-1400

and guarantee that safe and convenient pedestrian and bus routes to the school be maintained.

- ii. The developer shall install appropriate traffic signs around the site to ensure pedestrian and vehicle safety.
 - iii. There shall be no staging or parking of construction vehicles, including vehicles to transport workers on any of the streets adjacent to the school.
 - iv. Due to noise impacts on the schools, no construction vehicles or haul trucks shall be staged or idled on these streets during school hours.
- c. Schools affected by Haul Route.
- i. LADBS shall assign specific haul route hours of operation based upon Farmdale Elementary School and El Sereno Middle School hours of operation.
 - ii. Haul route scheduling shall be sequenced to minimize conflicts with pedestrians, school buses and cars at the arrival and dismissal times of the school day. Haul route trucks shall not be routed past the school during periods when school is in session especially when students are arriving or departing from the campus.
- d. Good Neighbor Construction Practices.
- i. Whenever possible, construction vehicles should be parked on site to prevent congestion on streets with limited parking.
 - ii. When temporarily blocking portions of streets for deliveries of construction materials, a flag person shall be provided to assist with pedestrian and vehicular traffic.
 - iii. Street closures shall not take place during peak traffic hours. Any street, sidewalk, or other improvement work shall be conducted in conformance with the latest Manual on Work Area Traffic Control.
 - iv. Care shall be taken not to overfill concrete trucks during deliveries. If spills occur, it shall be the responsibility of the concrete company to immediately provide clean up.
 - v. Construction noise shall be kept to a minimum with consideration of the surrounding neighbors. Unnecessary noise such as music shall be kept below legal levels.

- vi. Streets and sidewalks adjacent to construction sites shall be swept free of construction debris at all times.
 - vii. Care shall be taken to not interfere with trash pick-up by the Bureau of Sanitation. Construction and delivery vehicles shall be subject to trash pick-up parking restrictions.
 - viii. If building materials are to be stored in public right of ways, it shall be by permit from the Department of Public Works, Bureau of Street Services, Investigations and Enforcement Division and shall conform with all applicable rules.
 - ix. All construction/demolition activities shall comply with the construction hours in Section 41.40 of the LAMC.
- e. The Project Applicant shall plan construction and construction staging as to maintain pedestrian access to adjacent active land uses throughout all construction phases. This requires the Applicant to maintain adequate and safe pedestrian protection, including physical separation from workspace and vehicular traffic and overhead protection, due to sidewalk closure or blockage, at all times. Barriers, such as K-Rails, scaffolding, etc., shall be maintained at a height of 8 feet.

C. Administrative Conditions

14. **Approvals, Verification and Submittals.** Copies of any approvals, guarantees or verification of consultations, reviews or approval, plans, etc, as may be required by the subject conditions, shall be provided to the Department of City Planning for placement in the subject file.
15. **Code Compliance.** All area, height and use regulations of the zone classification of the subject property shall be complied with, except wherein these conditions explicitly allow otherwise.
16. **Covenant.** Prior to the issuance of any permits relative to this matter, an agreement concerning all the information contained in these conditions shall be recorded in the County Recorder's Office. The agreement shall run with the land and shall be binding on any subsequent property owners, heirs or assign. The agreement must be submitted to the Department of City Planning for approval before being recorded. After recordation, a copy bearing the Recorder's number and date shall be provided to the Department of City Planning for attachment to the file.
17. **Definition.** Any agencies, public officials or legislation referenced in these conditions shall mean those agencies, public offices, legislation or their successors, designees or amendment to any legislation.
18. **Enforcement.** Compliance with these conditions and the intent of these conditions shall be to the satisfaction of the Department of City Planning and any designated agency, or the agency's successor and in accordance with any stated laws or regulations, or any amendments thereto.

19. **Building Plans.** A copy of the first page of this grant and all Conditions and/or any subsequent appeal of this grant and its resultant Conditions and/or letters of clarification shall be printed on the building plans submitted to the Development Services Center and the Department of Building and Safety for purposes of having a building permit issued.
20. **Corrective Conditions.** The authorized use shall be conducted at all times with due regard for the character of the surrounding district, and the right is reserved to the City Planning Commission, or the Director pursuant to Section 12.27.1 of the Municipal Code, to impose additional corrective conditions, if, in the Commission's or Director's opinion, such conditions are proven necessary for the protection of persons in the neighborhood or occupants of adjacent property.

21. INDEMNIFICATION AND REIMBURSEMENT OF LITIGATION COSTS.

Applicant shall do all of the following:

- a. Defend, indemnify and hold harmless the City from any and all actions against the City relating to or arising out of the City's processing and approval of this entitlement, including but not limited to, an action to attack, challenge, set aside, void, or otherwise modify or annul the approval of the entitlement, the environmental review of the entitlement, or the approval of subsequent permit decisions, or to claim personal property damage, including from inverse condemnation or any other constitutional claim.
- b. Reimburse the City for any and all costs incurred in defense of an action related to or arising out of the City's processing and approval of the entitlement, including but not limited to payment of all court costs and attorney's fees, costs of any judgments or awards against the City (including an award of attorney's fees), damages, and/or settlement costs.
- c. Submit an initial deposit for the City's litigation costs to the City within 10 days' notice of the City tendering defense to the Applicant and requesting a deposit. The initial deposit shall be in an amount set by the City Attorney's Office, in its sole discretion, based on the nature and scope of action, but in no event shall the initial deposit be less than \$50,000. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
- d. Submit supplemental deposits upon notice by the City. Supplemental deposits may be required in an increased amount from the initial deposit if found necessary by the City to protect the City's interests. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
- e. If the City determines it necessary to protect the City's interest, execute an indemnity and reimbursement agreement with the City under terms consistent with the requirements of this condition.

The City shall notify the applicant within a reasonable period of time of its receipt of any action and the City shall cooperate in the defense. If the City fails to notify the applicant of any claim, action, or proceeding in a reasonable time, or if the City fails to reasonably cooperate in the defense, the applicant shall not thereafter be responsible to defend, indemnify or hold harmless the City.

The City shall have the sole right to choose its counsel, including the City Attorney's office or outside counsel. At its sole discretion, the City may participate at its own expense in the defense of any action, but such participation shall not relieve the applicant of any obligation imposed by this condition. In the event the Applicant fails to comply with this condition, in whole or in part, the City may withdraw its defense of the action, void its approval of the entitlement, or take any other action. The City retains the right to make all decisions with respect to its representations in any legal proceeding, including its inherent right to abandon or settle litigation.

For purposes of this condition, the following definitions apply:

"City" shall be defined to include the City, its agents, officers, boards, commissions, committees, employees, and volunteers.

"Action" shall be defined to include suits, proceedings (including those held under alternative dispute resolution procedures), claims, or lawsuits. Actions include actions, as defined herein, alleging failure to comply with any federal, state or local law.

Nothing in the definitions included in this paragraph are intended to limit the rights of the City or the obligations of the Applicant otherwise created by this condition.

FINDINGS

General Plan Findings

1. **General Plan Land Use Designation.** The Project Site, located generally at 2520 North Eastern Avenue, is located within the Northeast Los Angeles Community Plan, which was last updated by the City Council on June 15, 1999. The site is an irregularly-shaped site, comprised of multiple parcels and approximately 218,270 gross square feet of lot area. The Community Plan designates the site with a land use designation of Low Residential, which lists the RE9, RS, R1, RU, RD6, and RD5 as corresponding zones. The site is currently zoned [Q]R1-1D and [Q]RD6-1D and is subject to the Qualified "Q" Conditions and Development "D" Limitations contained within Ordinance No. 180,403, also known as the Northeast Los Angeles Hillsides Ordinance. The recommended Zone Change from R1 and RD6 to RD5 would be consistent with the land use designation. As further discussed under Finding No. 3, the recommended Q Conditions would be consistent with the Q Conditions contained within the Northeast Los Angeles Hillsides Ordinance. As a Height District Change has not been requested, the D Limitations of the Northeast Los Angeles Hillsides Ordinance has been maintained. The recommended Zone Change to (T)(Q)RD5-1D would be consistent with the purpose, intent, and provisions of the General Plan as it is reflected within the Central City Community Plan.

2. **General Plan Text.**

a. **Framework Element.** The Framework Element for the General Plan (Framework Element) was adopted by the City of Los Angeles in December 1996 and re-adopted in August 2001. The Framework Element provides guidance regarding policy issues for the entire City of Los Angeles, including the project site. The Framework Element also sets forth a Citywide comprehensive long-range growth strategy and defines Citywide policies regarding such issues as land use, housing, urban form, neighborhood design, open space, economic development, transportation, infrastructure, and public services. The Framework Element includes the following provisions, objectives and policies relevant to the instant request:

GOAL 3B: *Preservation of the City's stable single-family residential neighborhoods.*

Objective 3.5: Ensure that the character and scale of stable single-family residential neighborhoods is maintained, allowing for infill development provided that it is compatible with and maintains the scale and character of existing development.

Objective 3.6: Allow for the intensification of selected single-family areas that directly abut high-density development as "transitions" between these uses.

Policy 3.6.1: Ensure that the new development of "duplex" or multi-family units maintains the visual and physical character of adjacent single-family neighborhoods, including the maintenance of front property setbacks, modulation of building volumes and articulation of facade to convey the sense of individual units, and use of building materials that characterize single-family housing.

GOAL 3C: *Multi-family neighborhoods that enhance the quality of life for the City's existing and future residents.*

Objective 3.7: Provide for the stability and enhancement of multi-family residential neighborhoods and allow for growth in areas where there is sufficient public infrastructure and services and the residents' quality of life can be maintained or improved.

The Project Site is partially zoned [Q]R1-1D and [Q]RD6-1D and is adjacent to single-family and low-density multi-family neighborhoods. The recommended Zone Change to (T)(Q)RD5-1D in conjunction with the recordation of the approval of incidental Case No. VTT-73531-SL would permit the development of the site with 42 small lot homes. As stated under Finding No. 1, the RD5 Zone is consistent with the Low Residential land use designation. The designation by the Community Plan is consistent with Policy 3.5.1 of the Framework Element which identifies Low Residential land use designations as having the following corresponding zones: RE9, RS, R1, RD6, RD5, and RU. The RD5 Zone permits the multi-family developments that are consistent with minimum lot area per dwelling unit requirements of the R1 Zone. While the RD5 Zone would permit a net increase of five (5) dwelling units, it is consistent with the density that is generally permitted to the south of the site along the eastern side of Eastern Avenue.

As a small lot development, the Project would provide new fee-simple home ownership opportunities consistent with the adjacent properties to the south and east of the site. The proposed dwellings would range from two-stories with a basement to three stories with a maximum height of 26 feet. The proposed number of stories and height is consistent with what is currently permitted in the existing [Q]R1-1D and [Q]RD6-1D Zones and the surrounding areas. Consistent with Policy 3.6.1, the small lot homes have been designed to observe setbacks from the boundary of the site that is consistent with the surrounding neighborhood and utilize “modulation of building volumes and articulation of facade to convey the sense of individual units.” Additionally, 38 of the 42 small lot homes have been designed with private driveways, which are accessible from the common access driveway, similar to the traditional single-family dwelling. The recommended zone change would permit the development of the site to be located primarily along the southern and eastern property lines, maintaining greater setbacks along Eastern Avenue for a majority of the site's frontage and allowing the preservation of existing hillside. As recommended, the Zone Change would be consistent with the above stated goals, objectives, and policies of the Framework Element.

- b. Northeast Los Angeles Community Plan.** The Northeast Los Angeles Community Plan contains the following relevant goals, objectives, and policies:

GOAL 1: *A safe, secure, and attractive residential environment for all economic, age, and ethnic segments of the community.*

Objective 1-1: To preserve and enhance existing residential neighborhoods.

Policy 1-1.1: Protect existing stable single-family and other lower density residential neighborhoods from encroachment by higher density residential and other uses that are incompatible as to scale and character or would otherwise diminish the quality of life.

Policy 1-1.2: Promote neighborhood preservation, particularly in existing single-family neighborhoods, as well as in areas with existing multiple-family residences.

Objective 1-3: To preserve and enhance the residential character and scale of existing single- and multi-family neighborhoods.

Policy 1-3.1: Protect the quality and scale of residential environment through attention to the appearance of new construction including site planning and compatible building design.

Objective 1-5: To limit the intensity and density of development in hillside areas.

Policy 1-5.2: Ensure the availability of paved streets, adequate sewers, drainage facilities, fire protection services and facilities, and other emergency services and public utilities to support development in hillside areas.

Policy 1-5.3: Consider the steepness of the topography and the geological stability in any proposal for development within the Plan area

Policy 1-5.4: Require that any proposed development be designed to enhance and be compatible with adjacent development.

Policy 1-5.5: Encourage clustering of residential projects in order to use the natural terrain to best advantage.

Program: The Plan Map designates significant hillside areas for Low density residential corresponding to the RD6 or RD5 zones that permit lot assembly and concentration of units to optimize adaptation to site characteristics.

Objective 1-6: To promote and ensure the provision of fair and equal housing opportunities for all persons regardless of income and age groups or ethnic, religions, or racial backgrounds.

Policy 1-6.4: Provide for development of townhouses and duplex housing units to increase home ownership options.

As discussed in Finding No. 1, the RD5 Zone is consistent with the Low Residential Designation and would permit the development of the site with a density that is consistent with the surrounding existing single- and multi-family neighborhoods. The Project Site is a vacant, upsloping lot which has a steep slope along Eastern Avenue and a relatively flat pad along Lombardy Boulevard. The southern and eastern portions of the site consists of a gradual upsloping hillside. The recommended (T)(Q)RD5-1D Zone would permit the development of the site with a small lot development which would increase opportunities to fee-simple home ownership opportunities consistent with Objective 1-6 and Policy 1-6.4. The small lot homes will comply with the requirements of the existing Q Conditions, D Limitations, and the Code as it relates to Residential Floor Area, height, and stories.

The recommended zone change would allow the Project to be developed consistent with the Objective 1-5 and the policies related to development within the hillside area. Specifically, the recommended (T)(Q)RD5-1D would allow for the clustering of the residential dwelling units along the southern and eastern portion of the sites where the topography is less steep than the western portion of the site, consistent with the identified Program for Policy 1-5.5. Additionally, the clustering of the dwelling units along the southern and eastern property lines and along Lombardy Boulevard would allow the preservation of hillside and a greater number of existing Protected Trees

located on the western portion of the site. The recommended Zone Change to (T)(Q)RD5-1D would be consistent with the above referenced goals, objectives, policies, and programs of the Northeast Los Angeles Community Plan.

- c. **Housing Element.** The Housing Element of the General Plan contains the following relevant goals, objectives, and policies:

GOAL 1: Housing production and preservation.

Objective 1.1: Produce an adequate supply of rental and ownership housing in order to meet current and projected needs.

Policy 1.1.3: Facilitate new construction and preservation of a range of different housing types that address the particular needs of the City's households.

GOAL 2: Safe, Livable, and Sustainable Neighborhoods.

Objective 2.1: Promote safety and health within neighborhoods.

Objective 2.3: Promote sustainable buildings, which minimize adverse effects on the environment and minimize the use of non-renewable resources.

Policy 2.3.3: Promote and facilitate the reduction of energy consumption in new and existing housing.

The recommended zone change from [Q]R1-1D and [Q]RD6-1D to (T)(Q)RD5-1D would allow for the southeastern portion of the site to be subdivided and developed as a small lot development pursuant to LAMC Section 12.22 C.27. The Project Site is a vacant lot which consists of an upsloping hill, which has a relatively steep slope along the western portion of the site which is zoned [Q]RD6-1D. As shown in the survey prepared by PHB & Associates, Inc., the portion of the site located to the south and east of the site, which is zoned [Q]R1-1D and [Q]RD6-1D, has a more gradual slope and would be more suitable for construction. To help maintain a portion of the hillside and existing Protected Trees, the dwelling units have been clustered primarily along the southern and eastern portion of the site. While the existing [Q]R1-1D Zone could have been subdivided, it would have been limited to a maximum of 11 lots due to the minimum lot area requirements of the Zone. The recommended (T)(Q)RD5-1D Zone allows for a more efficient site configuration in addition to allowing for the small lot development. As a small lot development, the Project would introduce 42 new fee simple home ownership opportunities, which provides an alternative to the traditional options of single-family dwelling or condominium home ownership opportunities. The dwelling units would be required to comply with the Green Building Code which was adopted to increase sustainability in new construction. As recommended, the recommended Zone Change would be consistent with the above referenced goals, objectives, and policies of the Housing Element.

- d. **Mobility Element.** The Mobility Element of the General Plan is not likely to be affected by the recommended action herein. The Project, with the recommended Zone Change, proposes to construct 42 new small lot homes on a site that is bounded by Eastern Avenue to the west and Lombardy Boulevard to the northeast. Eastern Avenue is a designated Avenue II and is currently dedicated to a width of 80 feet at the site's frontage and is improved with roadway, curb, gutter, sidewalk, and a parkway. The Bureau of Engineering has recommended that a 3-foot wide strip of land be dedicated along Eastern Avenue adjoining the site to complete a 43-foot wide half right-of-way in accordance with the Avenue II standards. The area would be improved with a new concrete curb and full-

width concrete sidewalks with tree wells. Lombardy Boulevard is a designated Collector that is dedicated to a width of 80 feet and is improved with roadway, curb, gutter, sidewalk, and a parkway. The Bureau of Engineering has not recommended any dedications as the existing right-of-way is consistent with the Collector standards; however, the Bureau has recommended the existing sidewalk and parkway be improved to reconstruct the existing sidewalk and parkway. The dedication and improvements would to be provided would be consistent with the Street Standards adopted by the Mobility Element.

- e. **The Sewerage Facilities Element** of the General Plan will not be affected by the recommended action. While the sewer system might be able to accommodate the total flows for the proposed project, further detailed gauging and evaluation may be needed as part of the permit process to identify a specific sewer connection point. If the public sewer has insufficient capacity then the developer will be required to build sewer lines to a point in the sewer system with sufficient capacity. A final approval for sewer capacity and connection permit will be made at that time. Ultimately, this sewage flow will be conveyed to the Hyperion Treatment Plant, which has sufficient capacity for the project.

Entitlement Findings

3. Zone Change and Height District Change Findings.

- a. **Pursuant to Section 12.32 C of the Municipal Code, and based on these findings, the recommended action is deemed consistent with public necessity, convenience, general welfare and good zoning practice.**

Public Necessity, Convenience, and General Welfare

The Project Site is located within the Northeast Los Angeles Community Plan and is designated for Low Residential land uses and is zoned [Q]R1-1D and [Q]RD6-1D. As it relates to residential land uses, the Community Plan identifies opportunities for strategically targeting sites for appropriate low density hillside developments and clustering projects on undeveloped or underdeveloped land, and issues related to concerns with displacement and an imbalance in quality of housing stock. The site has long been vacant and has been utilized as a site for illegal dumping for dirt and has experienced fires in the past. The recommended zone change would allow for the construction of small lot development which would introduce 42 new fee-simple home ownership opportunities. As the site has been vacant, the Project would not result in the displacement of any residents on the site. As designed, the dwelling units are clustered along the southern, eastern, and northeastern portion of the site where the slope is relatively flatter than the western portion of the site. This would allow a large portion of the hillside to be maintained along Eastern Avenue and allow for the preservation of existing Protected Trees and the planting of replacement trees. As part of the approval of the incidental subdivision and as recommended as part of the Tentative T Classifications, the applicant would be required to dedicate land along Eastern Avenue and to improve the sidewalks along Eastern Avenue and Lombardy Boulevard. Improvements would include widening the sidewalks, installation of new or repair of existing curbs and gutters, as well as parkway landscaping. As recommended the zone change would be consistent with the public necessity, convenience, and general welfare.

Good Zoning Practice

The parcels located to the east of the site have a land use designation of Low Residential and are zoned [Q]RD6-1D and [Q]R1-1D. To the south, the lots have a land use designation of Low Residential and are zoned [Q]RD6-1D, [Q]RD5-1D, and

[Q]R1-1D. The parcels which are directly adjacent to the site consists of a gradual slope and are developed with primarily single-family dwellings which are located at the base of the hill. Parcels with direct frontage along the eastern side of Eastern Avenue are developed with single- and multi-family dwellings. The parcels to the west and northwest of the site, across Eastern Avenue, have a land use designation of Open Space and Public Facilities and are zoned [Q]OS-1XLD and [Q]PF-1D, respectively. The parcels are relatively flat and developed with the El Sereno Park and Recreation Center and Farmdale Elementary School. To the north, along the eastern side of Eastern Avenue, the parcels have a land use designation of Neighborhood Commercial and are zoned [Q]C4-1XL and are developed with dwelling units or commercial uses.

The site has approximately 780 feet of frontage along the eastern side of Eastern Avenue and 200 feet of frontage along the southern side of Lombardy Boulevard. Approximately 56,229 square feet of the southeastern portion of the site is zoned [Q]R1-1D and the remaining 162,041 square-foot portion of the site, located along Eastern Avenue and Lombardy Boulevard is zoned [Q]RD6-1D. The site is located within a transitional area of Eastern Avenue, which transitions from commercial uses to the north to low density multi-family and single-family residential uses to the south. The site's proximity to the commercial corridor on Huntington Drive would allow future residents easy accessibility to neighborhood serving uses. The site is unique from the lots immediately surrounding it due to its steep topography along Eastern Avenue. The recommended (T)(Q)RD5-1D Zone would allow for the small lot development along the gradual slope of the hillside and to maintain a portion of the hillside along the Eastern Avenue and preservation of a number of existing Protected Trees located within the western portion of the lot. As discussed in Finding No. 2, the recommended Zone Change would be consistent with a number of goals, objectives, policies, and programs related to the development of new housing within the hillsides of the Plan area. As recommended the zone change would be consistent with good zoning practices.

- b. Pursuant to Section 12.32-G and Q of the Municipal Code “T” and “Q” Classification Findings.** The current action, as recommended, has been made contingent upon compliance with new “T” and “Q” conditions of approval imposed herein for the proposed project. The “T” Conditions are necessary to ensure the identified dedications, improvements, and actions are undertaken to meet the public's needs, convenience, and general welfare served by the actions required. These actions and improvements will provide the necessary infrastructure to serve the proposed community at this site. The “Q” Conditions recommended herein have incorporated a majority of the “Q” Conditions adopted pursuant to Ordinance No. 180,403, by reference, which the site is currently subject to. The recommended “Q” Conditions modified conditions related to the construction of retaining walls and the maximum permitted amount of grading to take into consideration the unique nature of the site due to its size and topography. Additionally language has been added to clarify the requirements related to materials for hardscape as it relates to the construction of vehicular access ways which would be utilized for emergency vehicles. The “Q” Conditions that limit the scale and scope of future development on the site are also necessary to protect the best interests of and to assure a development more compatible with surrounding properties and the overall pattern of development in the community, to secure an appropriate development in harmony with the General Plan, and to prevent or mitigate the potential adverse environmental effects of the subject recommended action.

4. Zoning Administrator's Determination Findings.

- a. The project will enhance the built environment in the surrounding neighborhood or will perform a function or provide a service that is essential or beneficial to the community, city, or region.**

The Project Site is a vacant, irregular shaped site which consists of an upward sloping hillside and 102 Protected Trees and multiple non-Protected Trees. The site has approximately 780 feet of frontage along the eastern side of Eastern Avenue and 200 feet of frontage along the southern side of Lombardy Boulevard. The hillside has a gradual slope along the southern and eastern portions of the site, where the western portion of the site, along Eastern Avenue, has a steeper slope. The portion of the site located along Lombardy Boulevard was previously graded and consists of a relatively flat building pad and a gradual slope. While there are trees located throughout the site, there is a concentration of Protected Trees located along the western portion of the site. The site is located within the Northeast Los Angeles Community Plan area and has a land use designation of Low Residential and is zoned [Q]R1-1D and [Q]RD6-1D. The site is subject to the Q Conditions and D Limitations contained within Ordinance No. 180,403, also known as the Northeast Los Angeles Hillsides Ordinance, which became effective on January 16, 2009. The site is located within the Hillside Area, BOE Special Grading Area, Very High Fire Hazard Severity Zone, Landslide area, and Liquefaction area. The site is not located within the Alquist-Priolo Fault Zone, but is located 0.5 miles to the Upper Elysian Park Fault, Methane Hazard Site, or Fire District No. 1.

The parcels located to the east of the site have a land use designation of Low Residential and are zoned [Q]RD6-1D and [Q]R1-1D. To the south, the lots have a land use designation of Low Residential and are zoned [Q]RD6-1D, [Q]RD5-1D, and [Q]R1-1D. The parcels which are directly adjacent to the site consists of a gradual slope and are developed with primarily single-family dwellings which are located at the base of the hill. Parcels with direct frontage along the eastern side of Eastern Avenue are developed with single- and multi-family dwellings. The parcels to the west and northwest of the site, across Eastern Avenue, have a land use designation of Open Space and Public Facilities and are zoned [Q]OS-1XLD and [Q]PF-1D, respectively. The parcels are relatively flat and developed with the El Sereno Park and Recreation Center and Farmdale Elementary School. To the north, along the eastern side of Eastern Avenue, the parcels have a land use designation of Neighborhood Commercial and are zoned [Q]C4-1XL and are developed with dwelling units or commercial uses.

As discussed in Finding No. 3, the applicant has requested a Zone Change from [Q]R1-1D and [Q]RD6-1D to (T)(Q)RD5-1D to permit the development of the site with 42 small lot homes in conjunction with a small lot subdivision. The small lot subdivision was approved under incidental Case No. 73531-SL. Of the 42 dwelling units proposed, 38 dwelling units will be accessed from a common access driveway located at the southwest corner of the site from Eastern Avenue. The dwelling units will be located along the southern and eastern property lines of the site. The remaining four (4) dwelling units will be accessed from a common access driveway located at the northeast corner of the site from Lombardy Boulevard. The Project includes three floor plans which consists of a two-story dwelling with an optional rooftop deck, a two-story dwelling with a basement, or a three-story dwelling. While the number of stories vary, the dwelling units will have a maximum height of 26 feet. In addition to the construction of the small lot homes, the applicant proposes the construction of a total of 178 retaining walls throughout the site comprised of 129 walls which will be less than 3 feet in height, 18 walls with a height between 3 and 4 feet, and 32 walls with a height between 4 and 6 feet. The applicant proposed to have an additional 6-foot high

privacy/security fence constructed on top of 103 of the retaining wall. The applicant has requested a Zoning Administrator's Adjustment to permit over-in-height walls to be located within the front, side, and rear yards.

As part of the requested Zone Change, the applicant has requested consideration to modify Q Condition No. 3(a), which states the following:

The maximum total height of all freestanding retaining walls shall not exceed 12 feet, with no individual wall measuring higher than 6 feet on private property. Each freestanding retaining wall shall not exceed 75 feet in linear length, nor extend beyond one lot. Walls shall be separated by a minimum horizontal distance equal to the height of the highest wall. Freestanding garden walls 36" in height or less shall not be considered retaining walls for the purposes of this regulation.

As recommended as part of the zone change, the recommended Q Condition would modify the existing Q Condition to require compliance with the LAMC Section 12.21 C.8. With the approval and adoption of the zone change as recommended, the retaining walls would only require the approval of a Zoning Administrator's Determination (ZAD).

In conjunction with the requested modification to the Q Condition, the applicant has requested a ZAD to permit more than the maximum two retaining walls permitted pursuant to LAMC Section 12.21 C.8. As described by the applicant, the proposed retaining wall plan was preliminary reviewed by the Department of Building and Safety (DBS) which determined there may be up to a total of 178 freestanding retaining walls. Of those 178 walls, 129 retaining walls will have a maximum height of 3 feet and 18 walls with a height between 3 and 4 feet. It is possible that all or a portion of the 147 retaining walls may be considered freestanding garden walls and would be exempt from the existing Q Condition No. 3(a) and LAMC Section 12.21 C.8. A formal determination by the DBS would not be made until the plan check process.

The Project Site has remained vacant and consists of a variation of steep and gradual slopes on a site which is relatively larger than the surrounding residentially zoned lots. The Project has been designed to take into consideration the terrain of the hillside and the location of existing Protected Trees on the site. While the southern and eastern portion of the site are not as steep as the western portion of the site, there is a slope that would require stabilization to support the construction of the dwelling units and common access driveway. By clustering the dwelling units along the southern, eastern, and northeastern property lines, it would allow for a large portion of the western portion remain undisturbed with the existing Protected Trees and the future planting of the replacement trees. While the Q Condition does not limit the number of retaining walls that can be constructed, the Code only permits a maximum of two (2) retaining walls with a maximum height of 10 feet or one (1) retaining wall with a maximum height of 12 feet. The proposal would include a series of retaining walls which would have a maximum of 6 feet in height, allowing for the dwelling units to be built along the terrain of the hillside. As a small lot development, the retaining walls between the dwelling units serve a secondary function as a perimeter wall along the proposed property lines. As conditioned herein, the retaining walls which have a height between four (4) to six (6) feet will be required to be constructed with materials and colors matching the design of the dwelling units and will be required to be screened with landscaping to improve the aesthetics of the Project. Therefore, the Project will enhance the built environment in the surrounding neighborhood or will perform a function or provide a service that is essential or beneficial to the community, city, or region.

- b. The project's location, size, height, operations and other significant features will be compatible with and will not adversely affect or further degrade adjacent properties, the surrounding neighborhood, or the public health, welfare, and safety.**

As described in Finding No. 2.a, the Project would develop the vacant, upsloping lot with 42 small lot homes in conjunction with the merger and resubdivision of land into 42 small lots. Incidental Case No. VTT-73531-SL was approved by the Advisory Agency on March 3, 2020 and became effective on March 14, 2020. While the Project will include different floor plan options, the maximum height of the dwelling units will be 26 feet and will comply with the Residential Floor Area regulations contained within the D Limitations. The dwelling units will include attached two (2) car garages, which will be accessible from one of two common access driveways located on Eastern Avenue and Lombardy Boulevard.

The surrounding area to the northeast, east, south, and southwest are zoned either [Q]R1-1D, [Q]RD5-1D, or [Q]RD6-1D and are developed with single- and multi-family dwelling units on lots which are substantially smaller and have less topography than the subject site. The parcels to the west and northwest of the site, across Eastern Avenue, are zoned [Q]OS-1XLD and [Q]PF-1D, respectively. The parcels are relatively flat and developed with the El Sereno Park and Recreation Center and Farmdale Elementary School. To the north, along the eastern side of Eastern Avenue, the parcels are zoned [Q]C4-1XL and are developed with dwelling units or commercial uses.

Code Section 12.21 C.8(a) allows every hillside single-family lot to construct a maximum of two (2) retaining walls with a height of ten feet or one (1) retaining wall with a maximum height of 12 feet. The regulations are applicable Citywide and do not take into account the varying sizes of lots in the hillside area, thus the Code cannot universally apply to all lots Citywide. In consideration of this, the Code has an established procedure, Section 12.24 X,26, to allow for consideration of additional walls and increased heights depending on the nature of the lot. The Project is proposing a total of 178 freestanding retaining walls. Of those 178, it is possible that all or a portion of 147 retaining walls which have a height of four (4) feet or less may be considered freestanding garden walls and would be exempt from the existing Q Condition No. 3(a) and LAMC Section 12.21 C.8. A formal determination by the DBS would not be made until the plan check process.

The subject property is a 218,270 gross square-foot lot. The existing R1 Zone has a minimum lot area of 5,000 square feet, the RD6 Zone has a minimum lot area of 12,000 square feet, and the recommended RD5 Zone has a minimum lot of 10,000 square feet. The site has a larger than average lot area than traditional lots and the surrounding areas which are similarly zoned. The retaining walls will be located primarily along the southern, eastern, and northeastern portion of the site. Due to the terrain and the proposed site plan, a number of the walls will not be visible from the public right-of-way or adjacent neighbors as they are located between the small lot homes. The increased number of retaining walls allows for shorter walls to be constructed in lieu of less retaining walls with a taller wall. As conditioned herein, the retaining walls which have a height of four (4) feet or higher will be required to be constructed with materials and colors matching the design of the dwelling units and will be required to be screened with landscaping to improve the aesthetics of the Project. As conditioned, the retaining walls would not adversely affect or further degrade adjacent properties, the surrounding neighborhood and will not adversely affect the surrounding neighborhood, or the public health, welfare, and safety.

- c. The project substantially conforms with the purpose, intent and provisions of the General Plan, the applicable community plan, and any applicable specific plan.**

The Project proposes the construction of 42 small lot homes in conjunction with the merger and resubdivision of land into 42 small lots. Incidental Case No. VTT-73531-SL was approved by the Advisory Agency on March 3, 2020 and became effective on March 14, 2020. The Project would include a total of 178 retaining walls throughout the site comprised of 129 walls which will be less than 3 feet in height, 18 walls with a height between 3 and 4 feet, and 32 walls with a height between 4 and 6 feet. As discussed in Finding Nos. 2 and 3, with the approval and adoption of the recommended zone change to (T)(Q)RD5-1D, the Project will substantially conform with the purpose, intent and provisions of the General Plan and the Northeast Los Angeles Community Plan. The Project Site is not located within a specific plan.

5. Zoning Administrator's Determination Findings.

- a. While site characteristics or existing improvements make strict adherence to the zoning regulations impractical or infeasible, the project nonetheless conforms with the intent of those regulations.**

The Project Site is a vacant, irregular shaped site which consists of an upward sloping hillside and 102 Protected Trees and multiple non-Protected Trees. The site has approximately 780 feet of frontage along the eastern side of Eastern Avenue and 200 feet of frontage along the southern side of Lombardy Boulevard. The hillside has a gradual slope along the southern and eastern portions of the site, where the western portion of the site, along Eastern Avenue, has a steeper slope. The portion of the site located along Lombardy Boulevard was previously graded and consists of a relatively flat building pad and a gradual slope. While there are trees located throughout the site, there is a concentration of Protected Trees located along the western portion of the site.

The Project proposes the construction of 42 small lot homes in conjunction with the merger and resubdivision of land into 42 small lots. Of the 42 dwelling units, 38 dwelling units have been clustered along the southern and eastern property lines which lots developed with single-family dwellings located on lots zoned [Q]R1-1D and [Q]RD6-1D. The adjoining lots to the south and east are located at a lower grade than the proposed dwelling units. As discussed in Finding No. 4, the applicant proposes to construct a series of retaining walls resulting in a total of 178 retaining walls throughout the site comprised of 129 walls which will be less than 3 feet in height, 18 walls with a height between 3 and 4 feet, and 32 walls with a height between 4 and 6 feet. Up to 103 of the retaining walls are proposed to have an additional 6-foot high privacy/security fence constructed on top of the retaining wall. Of the 103 retaining walls that are proposed to have a privacy/security fence installed on top of the retaining wall, 17 walls are located within either the Front, Side, or Rear Yards of the site, or the boundary of the subdivision. As such, the retaining walls and privacy/security fence would be limited to a maximum height of 42 inches within the Front Yard and 6 feet within the Side and Rear Yards pursuant to LAMC Section 12.22 C.20(f). The applicant has proposed to construct a privacy/security fence with a maximum height of 6 feet on top of the proposed retaining walls that would vary in height. At its maximum, this would result in a retaining wall with a privacy/security fence with a height of 12 feet.

At the southwest corner of the site, on Lot No. 1, the applicant has requested to permit two retaining walls, Retaining Wall (RW) No. 1 and 2, with a height of 6 feet and RW

No. 3 with a height of 3 feet and to have a privacy/security fence with a maximum height of 6 feet. This would result in walls that vary in height from 9 to 12 feet. Along the northwest corner of the, on Lot Nos. 23 – 28 and 39, RW No. 175 is proposed to have a maximum height of 6 feet and to have a privacy/security fence with a maximum height of 6 feet. On Lot No. 39, RW Nos. 172 and 173 are proposed to have a maximum height of 6 and 3 feet, respectively. A portion of RW No. 1 and 175 are located along the western property line along Eastern Avenue, which is at a lower elevation than the proposed small lot homes located on the same lot. RW Nos. 2, 3, 172, and 173 are also at a higher elevation than RW Nos. 1 and 175. Due to the existing topography, a retaining wall with a maximum height of 6 feet may be necessary to retain the soil and assist in stabilizing the hillside; however, the placement of the retaining walls and location of the dwelling unit along the hillside do not warrant the construction of an additional 6-foot high privacy/security fence. As approved herein, RW Nos. 1, 172, and 175 which are located within the front yard along Eastern Avenue and Lombardy Boulevard may have a maximum height of 6 feet if determined to be necessary by the Grading Division of the Department of Building and Safety and no privacy/security fence may be permitted on top of the retaining walls. RW Nos. 2, 3, 65, and 173 may construct the retaining wall with a privacy/security fence on top of the wall; however, the combined height shall be limited to a maximum of six (6) feet.

A portion of RW No. 1 and the remaining retaining walls identified by the applicant are located within the side and rear yards along the southern and eastern property lines and vary in height from 2 to 6 feet. The southern and eastern property lines abut single-family dwellings which are generally built at a lower elevation than the proposed dwelling units and retaining wall. Comments have been received from some of the surrounding neighbors with concerns regarding the loss of privacy. While the dwelling units have provided adequate setbacks from the adjoining property lines, the setback area is anticipated to be utilize as private open space for the future residents. In consideration of the topography of the site and comments received from the public, permitting an over-in-height retaining wall with a privacy/security fence on top of the wall will provide additional security and address the concerns related to the loss of privacy from the adjoining neighbors. As approved herein, the retaining walls located along the outer perimeter along the southern and eastern property lines may be constructed with a privacy/security fence on top of the wall; however, the combined height shall be limited to a maximum of six (6) feet.

As conditioned herein, the retaining walls and privacy/security fences will be required to be constructed with materials and colors matching the design of the dwelling units and will be required to be screened with landscaping to improve the aesthetics of the Project. In consideration of the proposed location and topography of the hillside, as approved, the proposed height of the retaining walls and privacy/security walls would not impact the accessibility of light or air.

- b. In light of the project as a whole including any mitigation measures imposed, the project's location, size, height, operations and other significant features will be compatible with and will not adversely affect or further degrade adjacent properties, the surrounding neighborhood, or the safety, public health, welfare and safety.**

The Project will construct 42 new small lot homes on the approximately 218,270 gross square-foot lot located within the Northeast Los Angeles Community Plan area. The small lot homes will have approximately 2,000 square feet of floor area and a maximum height of 26 feet. The Project will require the removal of 39 of 102 Protected Trees (Black Walnut trees) located on the site and the grading of 82,000 cubic yards of

dirt and export of approximately 78,000 cubic yards of dirt. As the site is zoned [Q]R1-1D and [Q]RD6-1D, the Project requires the adoption of the recommended Zone Change to (T)(Q)RD5-1D to permit the small lot development. As discussed in Finding Nos. 1, 2, and 3, the Project is consistent with the goals, objectives, policies, and programs of the General Plan and Community Plan and would develop the site with dwelling units at a density that is consistent with the Low Residential land use designation. Furthermore, the determination has incorporated the mitigation measures identified in the Mitigated Negative Declaration prepared for the Project, Case No. ENV-2015-1918-MND, as enforceable conditions. Compliance with the conditions of approval and existing regulatory measures would allow the development of the site with a project that is consistent with the surrounding area. As such, the Project will be compatible with the surrounding area and will not adversely affect or further degrade adjacent properties, surrounding neighborhoods, or the safety, public health, welfare and safety.

- c. The project is in substantial conformance with the purpose, intent and provisions of the General Plan, the applicable community plan, and any specific plan.**

The Project proposes the construction of 42 small lot homes in conjunction with the merger and resubdivision of land into 42 small lots. Incidental Case No. VTT-73531-SL was approved by the Advisory Agency on March 3, 2020 and became effective on March 14, 2020. The Project would include a total of 178 retaining walls throughout the site comprised of 129 walls which will be less than 3 feet in height, 18 walls with a height between 3 and 4 feet, and 32 walls with a height between 4 and 6 feet. As discussed in Finding Nos. 2 and 3, with the approval and adoption of the recommended zone change to (T)(Q)RD5-1D, the Project will substantially conform with the purpose, intent and provisions of the General Plan and the Northeast Los Angeles Community Plan. The Project Site is not located within a specific plan.

Environmental Findings

- 6. Environmental Finding.** A Mitigated Negative Declaration (MND), Case No. ENV-2015-1918-MND, and Mitigation Monitoring Program was prepared for the project and published for re-circulated on Thursday, April 6, 2017. The analysis in the MND found that potential negative impacts could occur from the project's implementation due to Aesthetics, Air Quality, Biological Resources, and Transportation/Traffic and that those impacts could be mitigated to a less than significant level with the implementation of the identified mitigations. The MND found that there were no potentially significant impacts that could not be mitigated to a less than significant level. On the basis of the whole of the record before the lead agency including any comments received, the lead agency finds that, with imposition of the mitigation measures described in the MND there is no substantial evidence that the proposed project will have a significant effect on the environment.
- 7. Flood Insurance.** The National Flood Insurance Program rate maps, which are a part of the Flood Hazard Management Specific Plan adopted by the City Council by Ordinance No. 172,081, have been reviewed and it has been determined that this project is located in Zone C, areas of minimal flooding.

PUBLIC HEARING AND COMMUNICATIONS

A joint public hearing on this matter with the Deputy Advisory Agency and Hearing Officer was held at the City Hall on Thursday, January 23, 2020 for Case Nos. VTT-73531-SL and APCE-2015-2048-ZC-ZAD-ZAA.

The public hearing was attended by the applicant, representative, architect, and engineer for the project, as well as members of public. Thirteen members of the public spoke during the public comment period of the public hearing. Letters submitted at the public hearing have been included as part of Exhibit F.

The applicant's representative provided a presentation of the project, which gave an overview of the location of the site, the project, requested discretionary actions, issues related to the removal of the protected trees, removal of the billboard, as well as the outreach that was conducted by the current applicant. Information related to the outreach has been included as part of Exhibit F.2.

From the public, nine members spoke in opposition of the project, citing the following:

- Traffic concerns
- No walkability or sidewalks for the project
- Loss of open space, which needs to be reclaimed
- Will leave a negative impact, no value to the neighborhood
- Infringes on the surrounding single-family dwellings
- Does not compliment the area
- Noise impacts

From the public three members spoke in support of the project, citing the following:

- Not enough housing in the area
- There is a housing shortage
- The area needs new construction

One member from the public stated that they were not oppose to the project, but was concern about the number of parking spaces and traffic impacts.

After the public comment portion of the hearing, the following was discussed between the Subdivision Committee and the Hearing Officer:

- Amount of area which would be utilized for green space
- Replacement of Protected and non-Protected Trees
- Traffic study was completed
- Width of the common access easement
- Dwelling units are in conformance with the Zoning Code and the Northeast Los Angeles Hillside Ordinance
- Building design and articulation is in conformance with the Northeast Los Angeles Hillside Ordinance
- Provide additional landscaping along the southern and eastern property lines.

Public Communications Received (Included as part Exhibit F)

In a letter dated August 6, 2019, the LA-32 Neighborhood Council stated that the Project was presented at a regularly scheduled General Board Meeting on April 3, 2019 and that after public discussion, the board voted against the Project.

In a letter dated May 22, 2017, the Santa Monica Mountains Conservancy submitted a letter reaffirming its opposition to the redesign of the Project stating that the Project is receiving a significant up-zoning and is not providing public or protected natural open space. The Conservancy states that a project should avoid the removal of protected California black walnut trees and that the approval of the project should require substantial, onsite fee simple open space dedication and a permanent revenue stream from the homeowner's association to maintain it.

In a letter dated July 7, 2016, the Community Forest Advisory Committee submitted a letter stating that there concerns regarding the proposed removal of 68 Protected Trees and provided comments related to the proposed mitigation measures and that an Environmental Impact Report should be required.

Letters in opposition of the project have been submitted stating that there are a limited number of parks in the El Sereno area and that the Project Site is an ideal location for a park or other amenities for the community. Additional comments include concerns related to the loss of native trees, the loss of the hillside as a scenic resource and area of recreation, impacts to air quality, construction noise impacts to older dwelling units, and traffic impacts, including those related to the site's proximity to the El Sereno Regional Recreational Facility and El Sereno Middle School, and Sierra Park Elementary School. Additional comments included that an Environmental Impact Report should be required.

On September 26, 2019, the applicant's representative submitted documentation outlining public outreach which was conducted on behalf of the applicant. Outreach included visiting, talking to, and providing project information to businesses along Huntington Drive and meeting with representatives of Farmdale Elementary School, El Sereno Middle School, and El Sereno Recreational Center. Additionally, signatures of residents along Klamath Street/Harmony Lane were obtained stating their support of the Project.

EXHIBIT A



Updated Retaining Walls Matrix in progress and to be submitted by Applicant.

LOT AREA / FAR MATRIX										
TOTAL										
LOT #	LOT AREA	Area A	Area B	Area C	Area D	Area E	Area F	RFA	Actual	
		75%	65%	55%	45%	35%	25%			
1	5,686	5,307	254	59	66	-	-	4,208	2,279	
2	3,550	3,323	30	70	116	11	-	2,606	2,279	
3	3,419	3,353	7	15	44	-	-	2,547	2,279	
4	3,419	3,337	-	16	67	-	-	2,541	2,279	
5	3,419	3,337	-	-	82	-	-	2,540	2,279	
6	3,656	3,459	-	107	90	-	-	2,694	2,279	
7	3,419	3,382	-	38	-	-	-	2,557	1,729	
8	3,419	3,381	-	38	-	-	-	2,557	1,729	
9	3,419	3,080	203	136	-	-	-	2,517	1,729	
10	3,571	3,537	29	-	5	-	-	2,673	2,001	
11	3,352	3,314	38	-	-	-	-	2,510	2,001	
12	5,096	4,467	599	-	30	-	-	3,753	2,001	
13	3,349	2,862	140	214	133	-	-	2,415	1,729	
14	3,245	2,903	-	342	-	-	-	2,365	1,729	
15	3,473	3,423	-	27	23	-	-	2,592	1,729	
16	3,469	3,280	-	102	88	-	-	2,555	2,279	
17	3,239	2,903	192	86	58	-	-	2,375	2,279	
18	3,236	3,144	28	20	44	-	-	2,407	2,279	
19	3,691	3,619	-	8	65	-	-	2,747	2,279	
20	6,680	2,630	1,053	2,238	759	-	-	4,229	2,001	
21	4,690	3,492	1,198	-	-	-	-	3,398	2,001	
22	5,415	3,306	831	1,279	-	-	-	3,722	2,001	
23	6,777	3,562	478	750	1,926	62	-	4,283	2,001	
24	15,444	2,964	1,063	1,883	1,389	8,949	97	7,731	2,001	
25	9,930	3,582	825	279	1,595	2,357	292	4,992	2,279	
26	10,174	3,373	848	690	1,951	3,312	-	5,498	2,279	
27	11,041	3,372	1,017	1,187	3,073	2,392	-	6,063	2,279	
28	11,847	3,371	997	1,200	5,347	705	227	6,546	2,279	
29	12,629	3,612	616	1,525	6,584	123	169	6,996	2,279	
30	4,155	4,121	-	-	34	-	-	3,106	2,279	
31	3,203	3,171	-	-	32	-	-	2,393	2,279	
32	3,203	3,171	-	-	32	-	-	2,393	2,279	
33	3,203	3,171	-	-	32	-	-	2,393	2,279	
34	3,203	3,171	-	-	32	-	-	2,393	2,279	
35	3,203	3,171	-	-	32	-	-	2,393	2,279	
36	3,203	3,171	-	-	32	-	-	2,393	2,279	
37	3,203	3,054	-	115	34	-	-	2,369	2,279	
38	5,568	5,024	-	276	268	-	-	4,040	2,279	
39	6,266	4,257	1,041	680	73	215	-	4,352	2,279	
40	3,755	3,188	408	123	36	-	-	2,740	2,279	
41	3,999	3,377	-	574	48	-	-	2,870	2,279	
42	8,471	4,151	502	1,772	1,297	749	-	5,260	1,729	
									143,711	89,644

VISTA EL SERENO

SHEET INDEX COVER SHEET

DATE	VENDOR	ITEM	NOTES
7/10/2018	Forma Engineering	VTIM #73531	Vesting Tentative Track Map with Setback Matrix
7/10/2018	Forma Engineering	Grading & Drainage Plan	Sheets 1, 2 and 3
	Forma Engineering	Retaining Wall Plan	Showing Wall Number & Height
7/10/2018	Forma Engineering	Slope Band Analysis	Plan with FAR Calculation Matrix
9/16/2016	KIGY	Sheet A1.0	Plan 1 - Floor Plan
9/16/2016	KIGY	Sheet A1.1	Plan 1 - Elevation Alt. 1
9/16/2016	KIGY	Sheet A1.2	Plan 1 - Elevation Alt. 2
9/16/2016	KIGY	Sheet A3.0	Plan 3 - Floor Plan
9/16/2016	KIGY	Sheet A3.1	Plan 3 - Elevation Alt. 1
9/16/2016	KIGY	Sheet A3.2	Plan 3 - Elevation Alt. 2
9/16/2016	KIGY	Sheet A4.0	Plan 4 - Floor Plan
9/16/2016	KIGY	Sheet A4.1	Plan 4 - Elevation Alt. 1
9/16/2016	KIGY	Sheet A4.2	Plan 4 - Elevation Alt. 2
9/16/2016	KIGY	Sheet A4.3	Plan 4 - Elevation Alt. 3
7/10/2018			Unit Mx Plan
6/29/2018			Building Height Diagrams
7/17/2018	SMP	Conceptual Landscape Plan	Site Plan with Protected Tree Legend
6/17/2014	PHB & Associates	ALTA Survey	Sheets 1 & 2 of 2

VISTA EL SERENO

CLEARWATER
COMMUNITIES

4685 MacArthur Court, Suite 375
Newport Beach, Ca 92660

DEVELOPER'S ENGINEER
FORMA ENGINEERING INC.
10814 Reseda Boulevard,
Northridge, CA 91326

LANDSCAPE
SMP ENVIRONMENTAL DESIGN
SUMMERS/MURPHY & PARTNERS, INC.
34197 Coast Hwy, Suite 200
Dana Point, CA 92629

TITLE SHEET

CITY OF LOS ANGELES

Architecture + Planning
1255 West Jefferson Blvd
Los Angeles, CA 90066
ktgy.com 310 394 2633



KTGY # 140493 7/17/2018

VESTING TENTATIVE TRACT MAP NO. 73531

IN THE CITY OF LOS ANGELES
42 LOT SMALL LOT SUBDIVISION AND MERGER
COUNCIL DISTRICT # 14

LEGAL DESCRIPTION:
THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:

PARCEL A:
THAT PORTION OF THE LOT MARKED "CORNWALL" ON THE PARTITION MAP OF THE SMITH ESTATE, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA AS PER MAP RECORDED IN BOOK 6 PAGE 156 OF MISCELLANEOUS RECORDS, ON THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHEAST CORNER OF SAID LOT MARKED "CORNWALL", BEING ALSO THE SOUTHEAST CORNER OF LOT 1 OF THE FARMDALE TRACT, AS PER MAPS RECORDED IN BOOK 4 PAGE 37 OF MAPS, IN SAID OFFICE OF THE COUNTY RECORDER; THENCE SOUTH ALONG THE EAST LINE OF SAID "CORNWALL" LOT, 180.87 FEET; THENCE WEST PARALLEL WITH THE SOUTH LINE OF SAID FARMDALE TRACT, WHICH IS ALSO THE NORTH LINE OF SAID "CORNWALL" LOT, 538.78 FEET MORE OR LESS, TO THE EASTERLY LINE OF FARMDALE AVENUE, FORMERLY KNOWN AS BROWN ROAD, AS SAID ROAD WAS DESCRIBED IN THE DEED TO THE COUNTY RECORDER, THENCE NORTH 32°28' EAST ALONG THE EASTERLY LINE OF SAID AVENUE, 214.57 FEET, MORE OR LESS, TO THE NORTH LINE OF SAID LOT MARKED "CORNWALL"; THENCE EAST ALONG SAID LAST MENTIONED NORTH LINE 424.62 FEET TO THE POINT OF BEGINNING.

PARCEL B:
PARCEL 1:
LOT 1 OF FARMDALE TRACT, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 4 PAGE 37 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

PARCEL 2:
THAT PORTION OF EASTERN AVENUE, FORMERLY BROWN ROAD, AS SHOWN ON SAID MAP OF FARMDALE TRACT, VACATED, ADJOINING SAID LOT 1 ON THE NORTHWEST WHICH LIES SOUTHEASTERLY OF THE SOUTHEASTERLY LINE OF EASTERN AVENUE 80 FEET WIDE, AS ESTABLISHED BY VACATION ORDINANCE NO. 83066, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA.

PARCEL C:
LOT 562 OF TRACT NO. 6900, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 108 PAGE 55 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

BASIS OF BEARINGS:
THE BEARINGS FOR THIS MAP ARE BASED ON THE CENTERLINE OF MALLORY STREET AS SHOWN ON TRACT 6900, FILED IN BOOK 86, PAGES 34-39, RECORDS OF LOS ANGELES COUNTY, BEING NORTH 00°00'00" WEST.

BENCH MARK:
NO. 11-05092 ELEV = 447.371 ADJ 2000
WIRE SPK W. CURB FACE EASTERN AVE; 24 FEET S/O S CURB LINE PROD. OF LOMBARDY BLVD, 1 FOOT S/O N END CB

EXISTING EASEMENTS:

- PURPOSE: EASEMENT FOR PUBLIC STREET, ROAD, OR HIGHWAY IN FAVOR OF: CITY OF LOS ANGELES
RECORDING DATA: IN BOOK 12435, PAGE 200, OF OFFICIAL RECORDS
AFFECTS: THE EASTERLY 40 FEET OF SAID LAND
NOT PLOTTED, EASTERN AVE RIGHT-OF-WAY
- PURPOSE: SLOPE CONSTRUCTION AND MAINTENANCE ALONG EASTERN AVE
RECORDING DATA: BOOK 16886, PAGE 99 OF OFFICIAL RECORDS
AFFECTS: VARIABLE WIDTH AS REQUIRED ALONG EASTERN AVENUE FROM VALLEY BLVD TO LOMBARDY BLVD
NOT PLOTTED, ADJACENT TO EASTERN AVE
- PURPOSE: SLOPE CONSTRUCTION AND MAINTENANCE ALONG EASTERN AVE
RECORDING DATA: BOOK 16886, PAGE 99 OF OFFICIAL RECORDS AND BOOK 16945, PAGE 69 OF OFFICIAL RECORDS
AFFECTS: VARIABLE WIDTH AS REQUIRED ALONG EASTERN AVENUE FROM VALLEY BLVD TO LOMBARDY BLVD
NOT PLOTTED, ADJACENT TO EASTERN AVE
- PURPOSE: PUBLIC UTILITY EASEMENT
RECORDING DATA: BOOK 15644, PAGE 194 OF OFFICIAL RECORDS
AFFECTS: 5' WIDE AS SHOWN ON THE MAP, TO BE ABANDONED PRIOR TO FINAL MAP RECDORATION

- NOTES:
- EXISTING USE: 3 VACANT PARCELS
 - PROPOSED USE: SUBDIVISION OF 42 LOTS IN CONJUNCTION WITH THE CONSTRUCTION OF 42 SINGLE FAMILY HOMES, UTILIZING THE SMALL LOT ORDINANCE.
 - AREA: EXISTING LOT AREA=218,354 SF (5.0127 ACRES)
MERGER AREA = 921 SF (0.0213 ACRES) - LOMBARDY BLVD
DEDICATION AREA = 1,879 SF (0.0431 ACRES) - EASTERN AVE
NET AREA = 217,396 SF (4.9907 ACRES)
GROSS AREA TO CENTERLINE = 257,716 SF (5.9163 ACRES)
 - EXISTING ZONE - [Q]R1-1D, [Q]R06-1D
PROPOSED ZONE - [Q]R05-1D
 - TREES:
ALL ONSITE PROTECTED TREES ARE PLOTTED ON THE TENTATIVE MAP.
 - EXISTING UTILITIES:
30" SEWER MAIN LINE IN EASTERN AVENUE
10" SEWER MAIN LINE IN LOMBARDY BLVD
24" WATER MAIN LINE IN EASTERN AVENUE
12" WATER MAIN LINE IN LOMBARDY BLVD
7"4.5" STORM WATER MAIN LINE IN EASTERN AVENUE
30" RCP MAIN LINE IN LOMBARDY BLVD
 - DRAINAGE:
WHOLE SITE DRAINS TO EASTERN AVENUE. OR LOMBARDY BLVD.
 - FLOOD ZONE: X
FEMA MAP #: 06037-C1635F
 - THIS PROJECT IS IN A LANDSLIDE AREA.
 - THIS PROJECT IS IN A LIQUEFACTION AREA.
 - THIS PROJECT IS IN A VERY HIGH FIRE HAZARD AREA.
 - THERE ARE NO KNOWN POTENTIALLY DANGEROUS AREAS WITHIN THIS PROPOSED DEVELOPMENT.
 - ALL EXISTING STRUCTURES TO BE REMOVED.
 - 28' VEHICULAR BACKUP SPACE IS PROVIDED.
 - DISTRICT MAP NO. 139-54233
 - NORTHEAST HILLSIDE ZONE, LOS ANGELES PLANNING AREA
 - TRASH WILL BE COLLECTED ON EACH LOT.
 - THERE ARE PROTECTED TREE SPECIES ON SITE AS SHOWN.
 - SMALL LOT SINGLE FAMILY SUBDIVISION IN THE R05 ZONE, PURSUANT TO ORDINANCE 176,354.
 - GRADING QUANTITIES:
CUT - 80,407
FILL - 1,628
P&R - 8,000 (SHRINKAGE 15%, 1,200)
EXPORT 77,958

ASSESSOR'S PARCEL NUMBERS:
5216-008-016; 5216-008-034; 5216-009-037

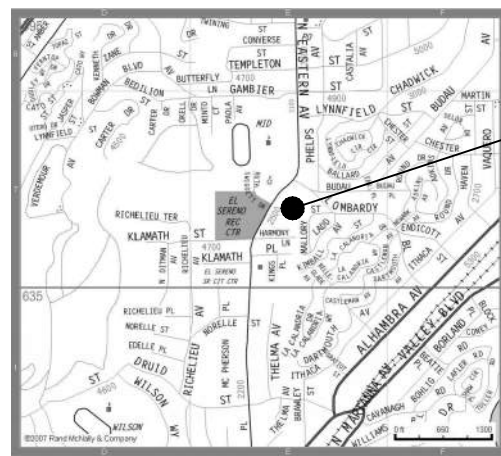
ZONING INFORMATION:
EXISTING: R06 AND RD1
PROPOSED: R05

SITE ADDRESS:
2520-2608 NORTH EASTERN AVE &
2657 LOMBARDY BLVD
LOS ANGELES, CA 90032

THOMAS GUIDE:
PAGE 595, GRID E7

SUBDIVIDER:
CLEARWATER COMMUNITIES, LLC
DAN KASSEL
dan@clearwatercommunities.com
4685 MACARTHUR COURT, SUITE 375
NEWPORT BEACH, CA 92660
(949) 220-0580

OWNER:
CLEARWATER COMMUNITIES, LLC
DAN KASSEL
dan@clearwatercommunities.com
4685 MACARTHUR COURT, SUITE 375
NEWPORT BEACH, CA 92660
(949) 220-0580



VICINITY MAP
NOT TO SCALE

SETBACK MATRIX				
LOT NO.	NORTH	SOUTH	EAST	WEST
1	22 [S]	23 [S]	4 [R]	2 [F]
2	26 [F]	16 [R]	4 [S]	4 [S]
3	26 [F]	16 [R]	4 [S]	4 [S]
4	24 [F]	18 [R]	4 [S]	4 [S]
5	26 [F]	16 [R]	4 [S]	4 [S]
6	24 [F]	18 [R]	6 [S]	4 [S]
7	28 [F]	19 [R]	4 [S]	4 [S]
8	28 [F]	19 [R]	4 [S]	4 [S]
9	28 [F]	19 [R]	4 [S]	4 [S]
10	28 [F]	12 [R]	4 [S]	6 [S]
11	11 [F]	12 [R]	4 [S]	4 [S]
12	20 [F]	12 [R]	18 [S]	4 [S]
13	4 [S]	1.5 [S]	13 [R]	25 [F]
14	4 [S]	4 [S]	14 [R]	25 [F]
15	6 [S]	4 [S]	14 [R]	25 [F]
16	4 [S]	6 [S]	13 [R]	25 [F]
17	4 [S]	4 [S]	13 [R]	25 [F]
18	4 [S]	4 [S]	13 [R]	25 [F]
19	9 [S]	4 [S]	13 [R]	25 [F]
20	36 [R]	5 [F]	14 [S]	7 [S]
21	35 [R]	29 [F]	4 [S]	4 [S]
22	55 [R]	23 [F]	4 [S]	4 [S]
23	75 [R]	24 [F]	4 [S]	7 [S]
24	56 [F]	3 [R]	4 [S]	56 [F]
25	6 [S]	4 [S]	27 [R]	130 [F]
26	4 [S]	4 [S]	27 [R]	150 [F]
27	4 [S]	4 [S]	27 [R]	170 [F]
28	4 [S]	4 [S]	27 [R]	190 [F]
29	4 [S]	4 [S]	27 [R]	210 [F]
30	15 [R]	27 [F]	15 [S]	3 [S]
31	15 [R]	27 [F]	3 [S]	3 [S]
32	15 [R]	27 [F]	3 [S]	3 [S]
33	15 [R]	27 [F]	3 [S]	3 [S]
34	15 [R]	27 [F]	3 [S]	3 [S]
35	15 [R]	27 [F]	3 [S]	3 [S]
36	15 [R]	27 [F]	3 [S]	3 [S]
37	15 [R]	27 [F]	3 [S]	3 [S]
38	22 [S]	32 [S]	5 [R]	6 [F]
39	20 [F]	23 [R]	3 [S]	5 [F]
40	22 [F]	30 [R]	4 [S]	4 [S]
41	22 [F]	31 [R]	4 [S]	3 [S]
42	7 [F]	62 [R]	23 [S]	15 [S]

SETBACKS ARE TO GROUND FLOOR

OR SECOND FLOOR

INDICATED ON PLAN AS:

[F] = FRONT YARD

[R] = REAR YARD

[S] = RIGHT SIDE YARD

PROPOSED EASEMENTS:

- PROPOSED VARIABLE WIDTH PRIVATE EASEMENT FOR VEHICULAR AND PEDESTRIAN INGRESS/EGRESS, PARKING, EMERGENCY ACCESS, UTILITIES, INFRASTRUCTURE AND CROSS LOT DRAINAGE PURPOSES AND A VARIABLE WIDTH PUBLIC EMERGENCY SERVICE EASEMENT AND UTILITY EASEMENT.

INDICATES PROTECTED TREE TO REMAIN TAG NUMBER, SEE TREE REPORT FOR DESCRIPTIONS.

INDICATES PROTECTED TREE TO BE REMOVED, TAG NUMBER, SEE TREE REPORT FOR DESCRIPTIONS.

INDICATES PROTECTED TREE REMOVED PRIOR TO MAY 8, 2015, TAG NUMBER, SEE TREE REPORT FOR DESCRIPTIONS.

DEVELOPER'S ENGINEER:

FORMA ENGINEERING INC.

10814 Reseda Boulevard, Northridge, CA 91326
Phone: (818) 832-1710 • Fax: (818) 832-1740

LAWRENCE WILSON

P.L.S. 6712

7/10/2018

DATE

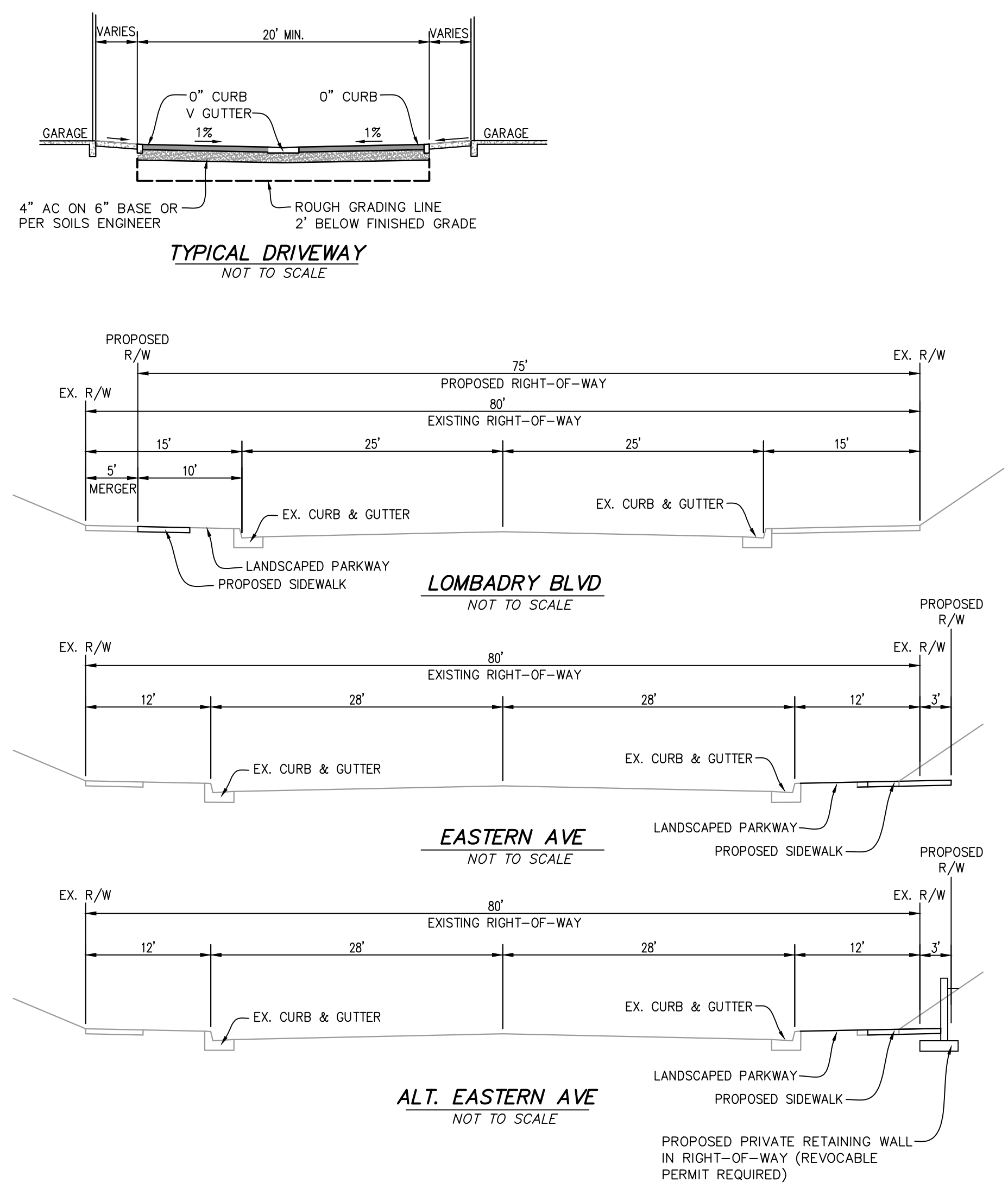
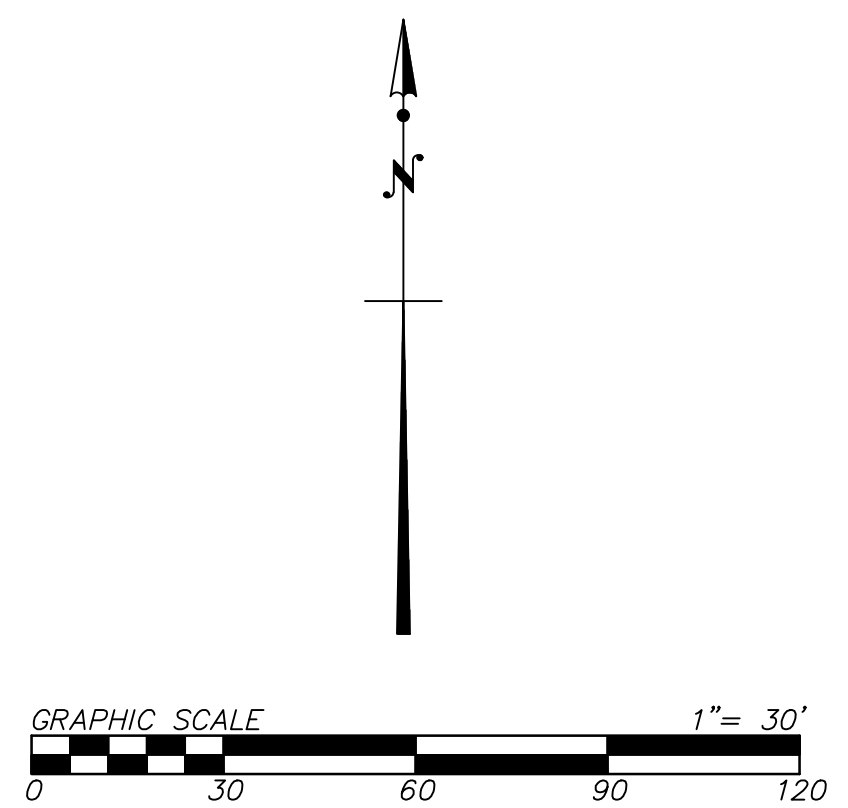
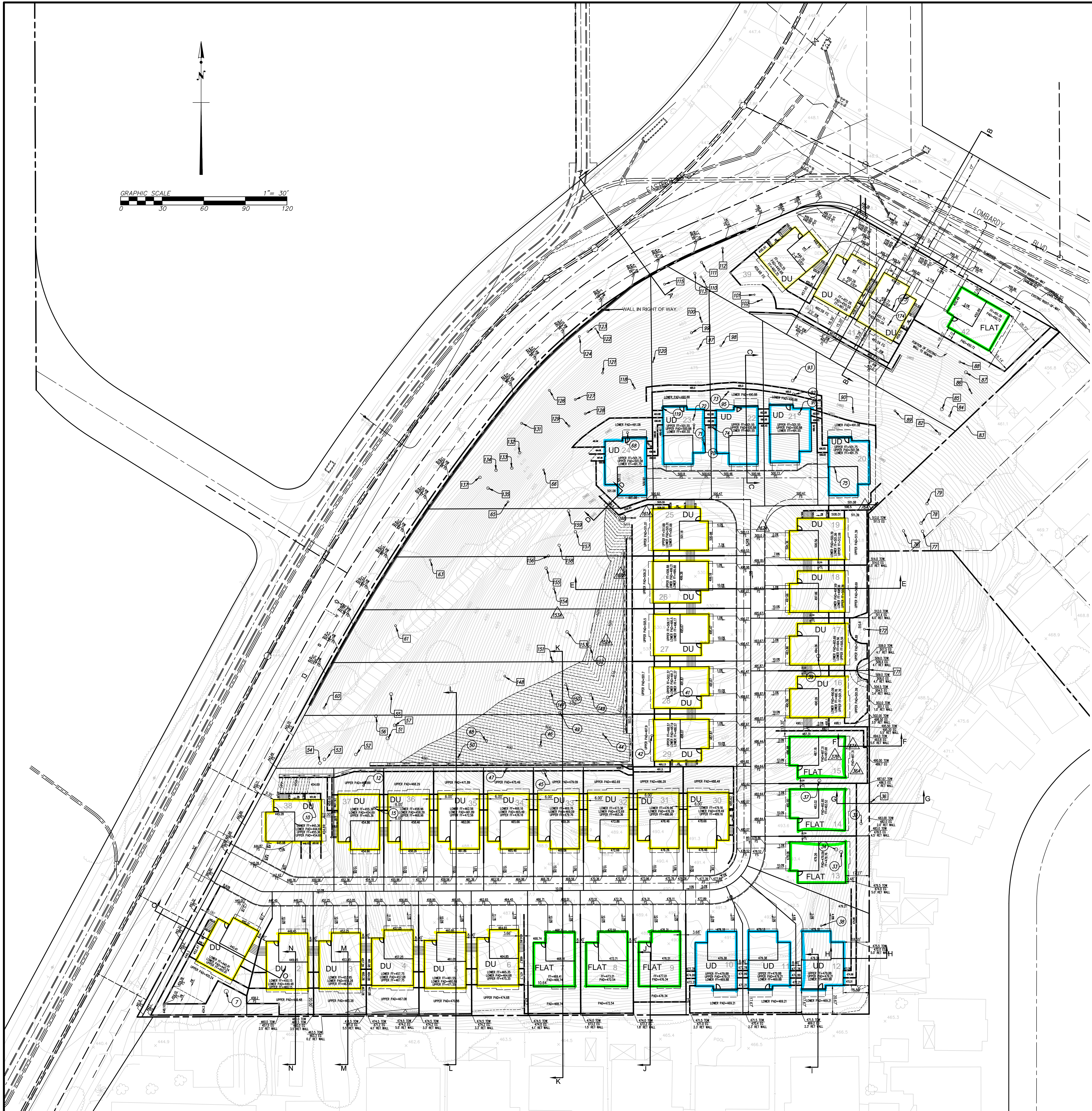


No.	DATE	REVISION	DESIGNER:
			C.K.
			CHECKED BY:
			W.M.W.
			DATE:
			7/10/2018
			SHEET
			1
			OF
			1

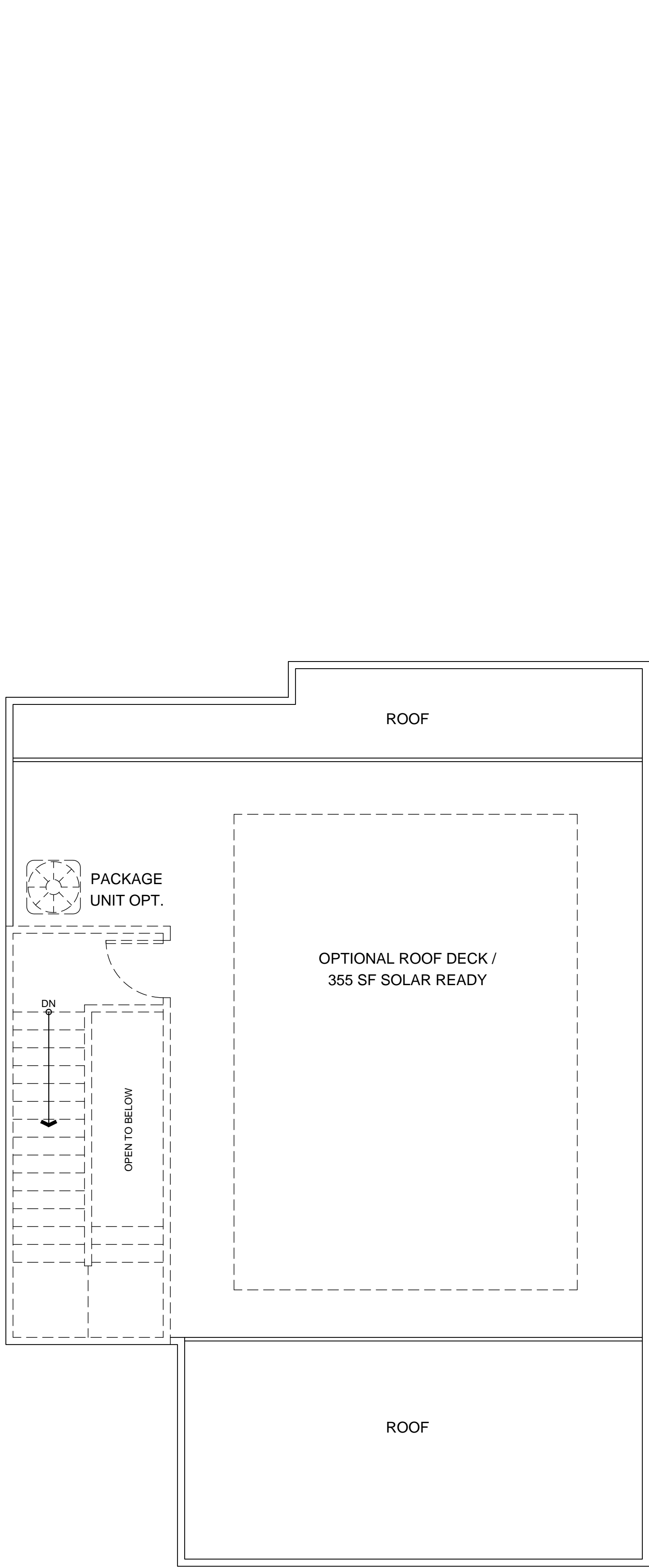
PREPARED FOR:

CLEARWATER COMMUNITIES, LLC
4685 MACARTHUR CT NEWPORT BEACH, CA 92660

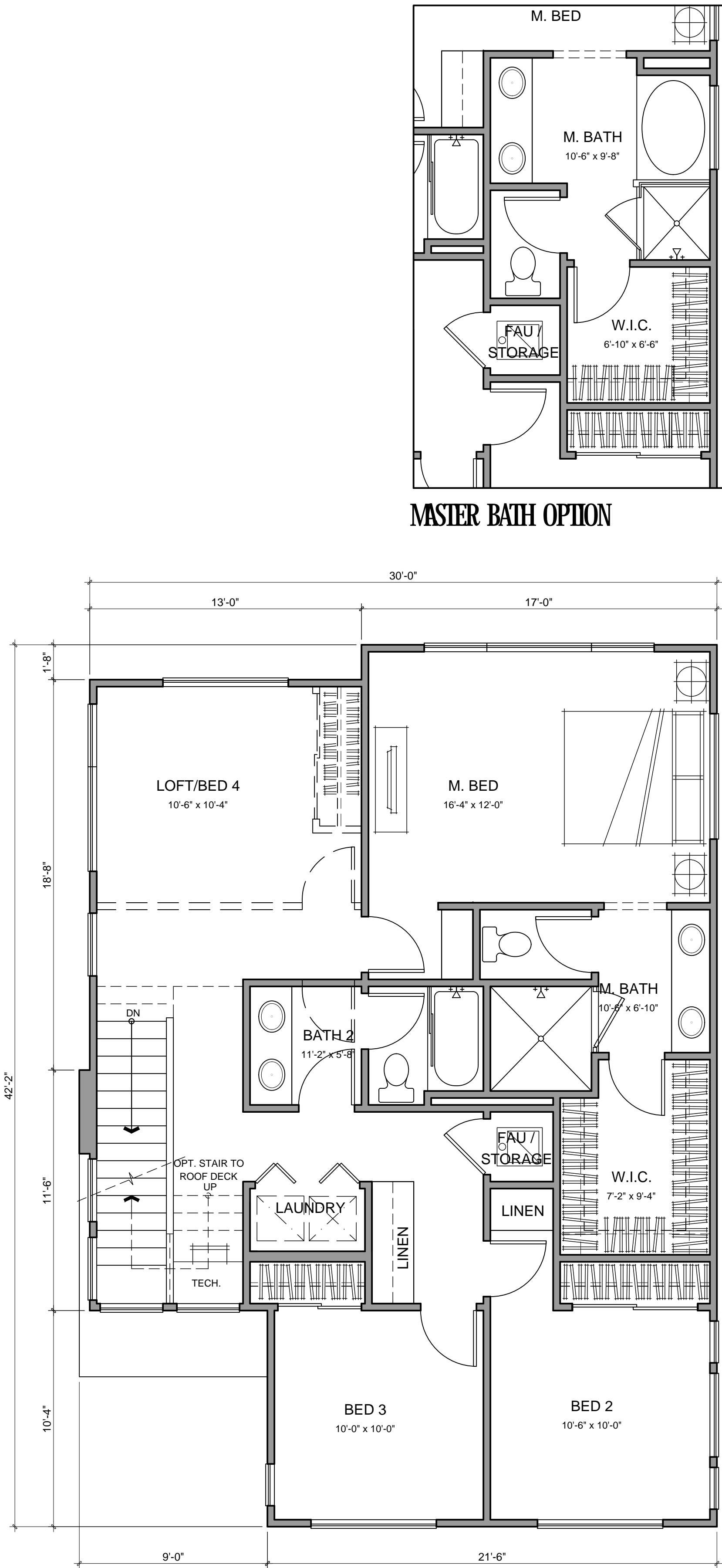
VESTING TENTATIVE MAP
TRACT 73531
2520 N. EASTERN AVE
LOS ANGELES, CA 90032



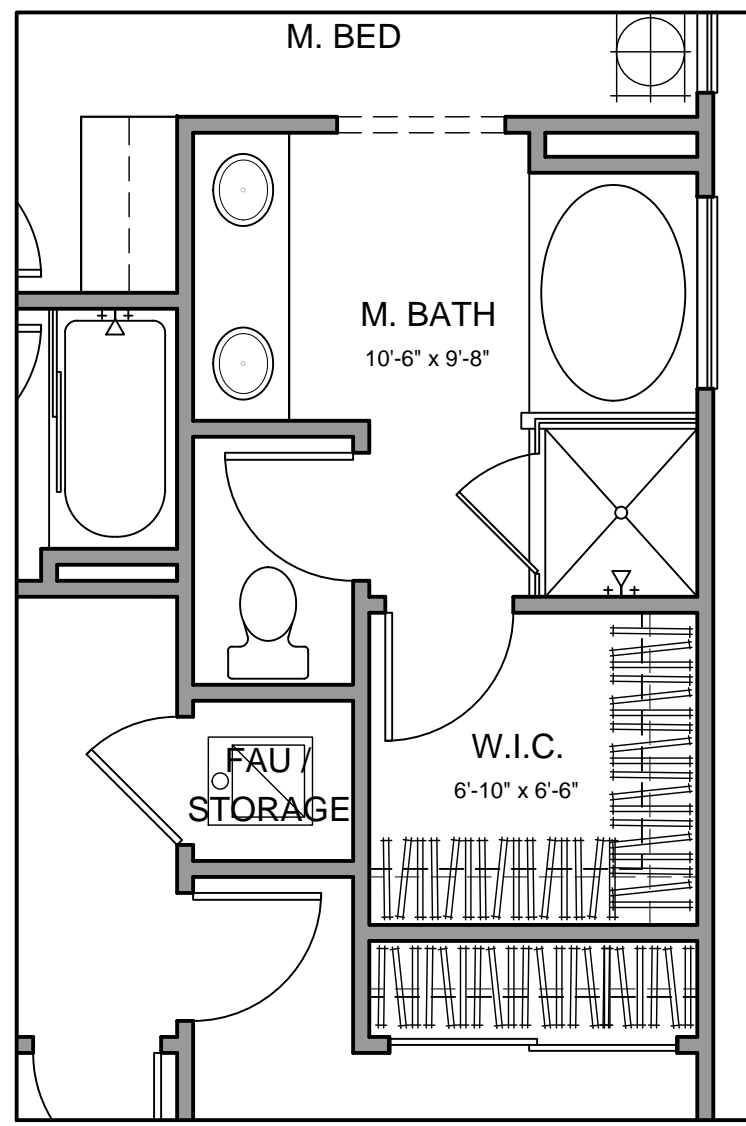
- (27) PLAN 4
- (7) PLAN 1
- (8) PLAN 3



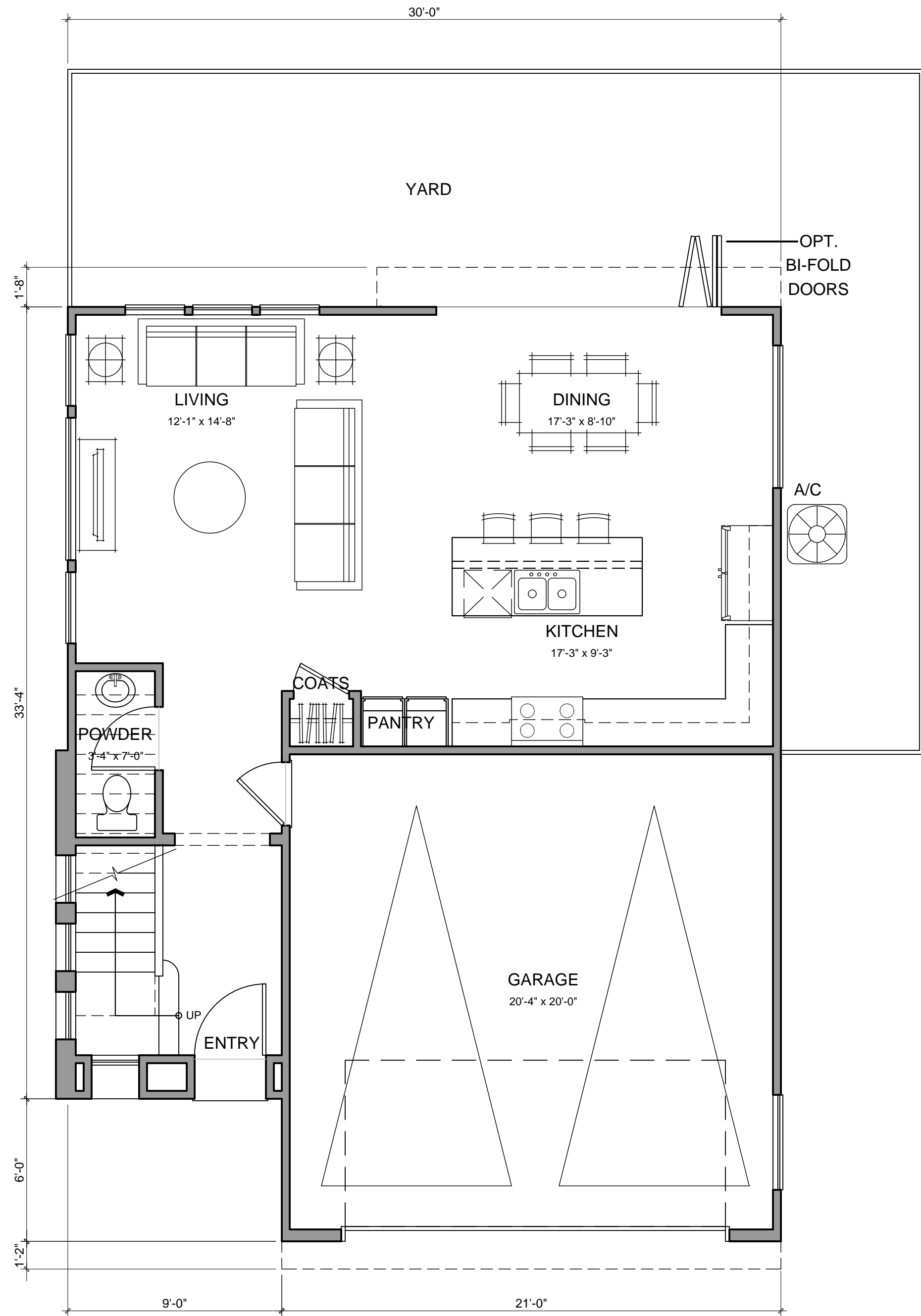
ROOF PLAN / OPTIONAL ROOF DECK



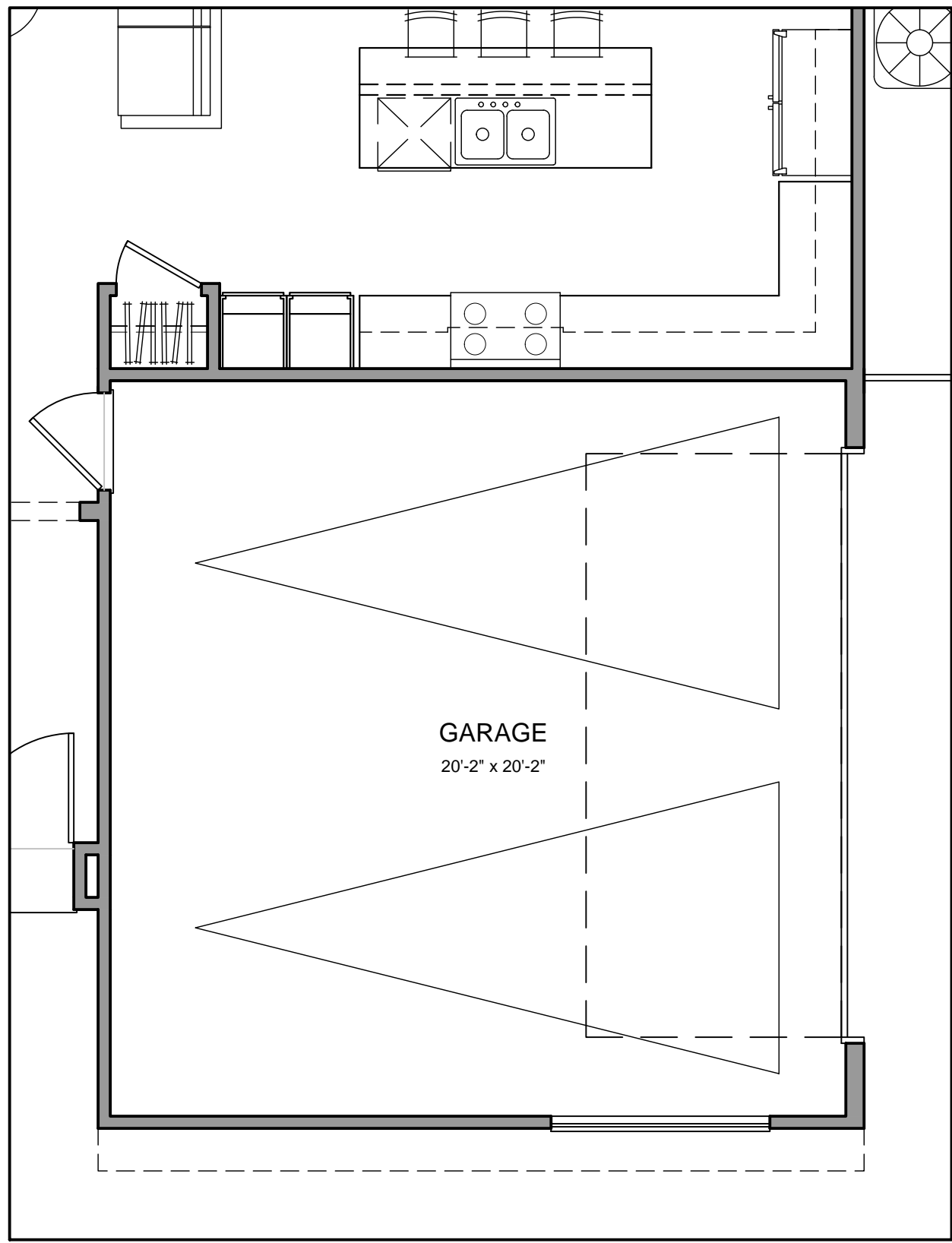
SECOND FLOOR W/ROOF DECK ACCESS



MASTER BATH OPTION



FIRST FLOOR



FIRST FLOOR - SIDE LOAD GARAGE ALT

VISTA EL SERENO



PLAN 1 - FLOOR PLAN (NET-1729 SF)

*NET FLOOR AREA "WITHIN THE WALLS"

A1.0

CITY OF LOS ANGELES

SQUARE FOOTAGES

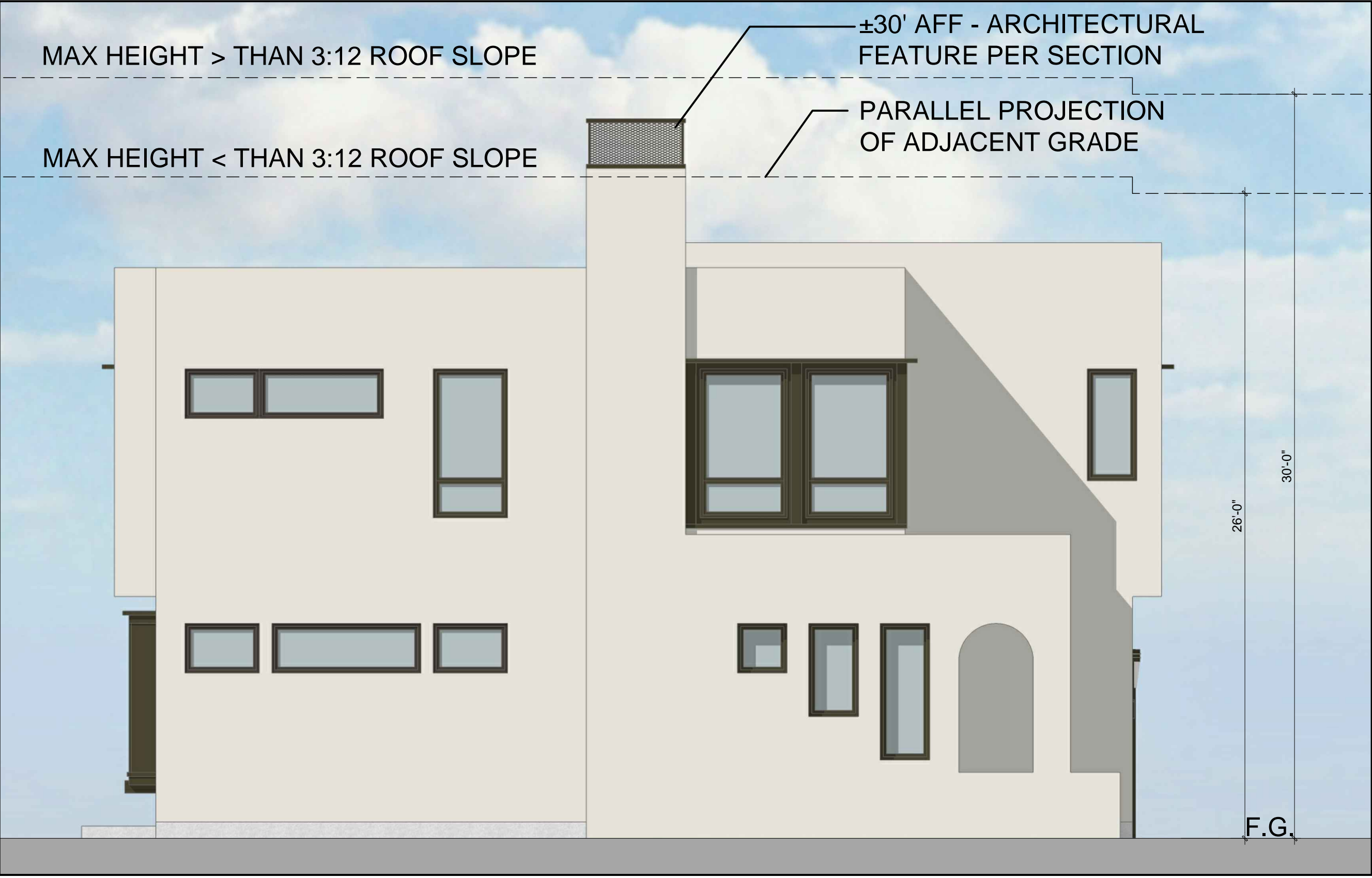
1ST FLOOR	646 SQ. FT.
2ND FLOOR	1084 SQ. FT.
NET TOTAL LIVING	1729 SQ. FT.
GARAGE	426 SQ. FT.
ROOF DECK	667 SQ. FT.

SCALE: 0 2 4 8

Architecture + Planning
1255 West Jefferson Blvd
Los Angeles, CA 90066
ktgy.com 310 394 2633

KTGY # 140493 9/16/2016





MATERIAL LEGEND	
1	STUCCO, LIGHT SAND FINISH
2	STAIN GRADE FIBER CEMENT SIDING (NOT USED)
3	VINYL GLAZING
4	FIBERGLASS ENTRY DOOR
5	AWNING
6	METAL AWNING (NOT USED)
7	METAL GUARDRAIL (NOT USED)
8	OUTLOOKER (NOT USED)
9	METAL SECTIONAL GARAGE DOOR
10	DECORATIVE EXTERIOR LIGHTING
11	ILLUMINATED ADDRESS SIGN
12	STANDING METAL SEAM (NOT USED)
13	CORRUGATED METAL (NOT USED)
14	WOODEN TRELLIS (NOT USED)



VISTA EL SERENO



PLAN 1 - ELEVATION ALT. 1 (SHOWN W/O ROOF DECK)

A1.1

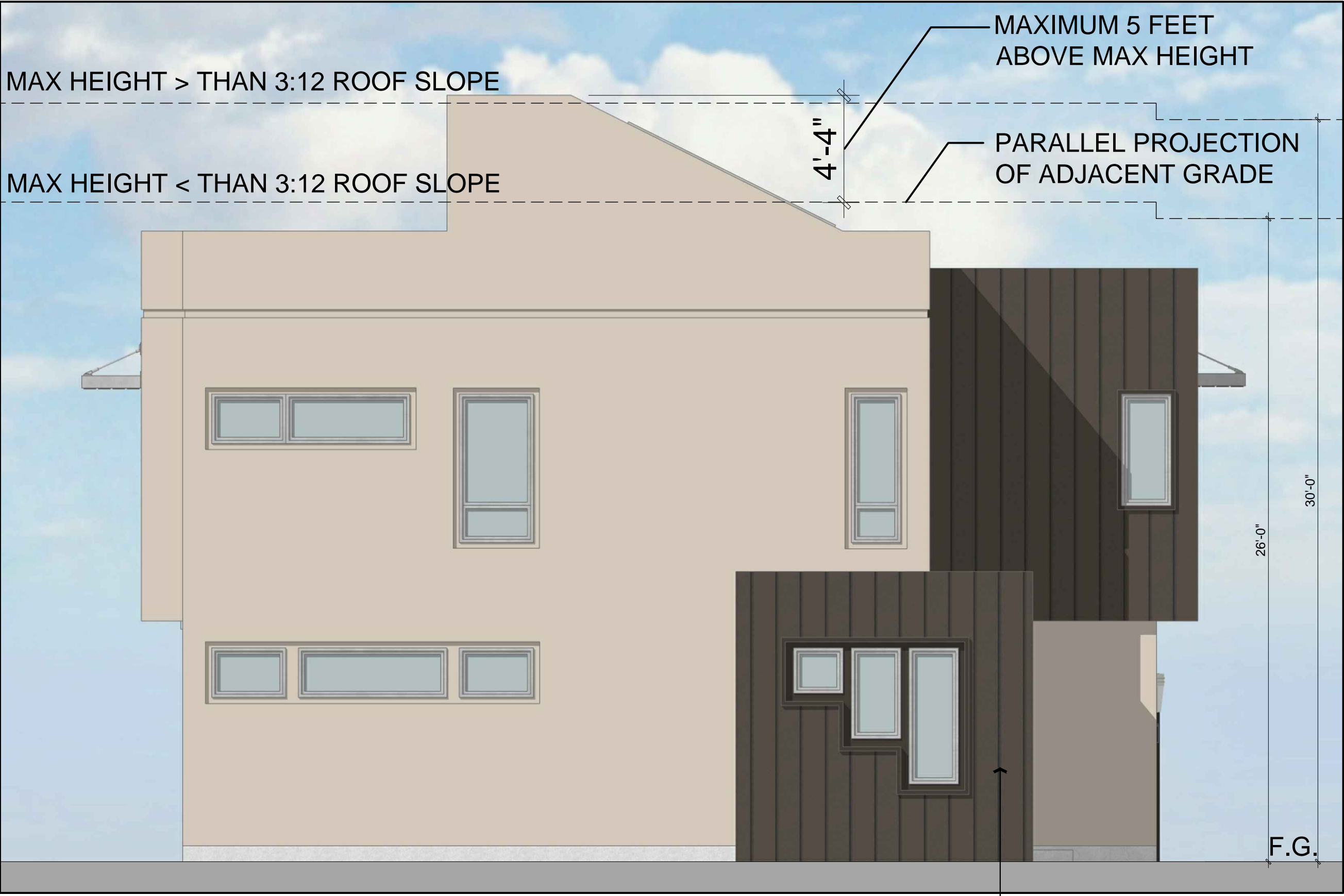
CITY OF LOS ANGELES

SCALE: 0 2 4 8

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Los Angeles, CA 90066
ktgy.com 310 394 2633



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LEFT ELEVATION



FRONT ELEVATION

MATERIAL LEGEND	
1	STUCCO, LIGHT SAND FINISH
2	STAIN GRADE FIBER CEMENT SIDING
3	VINYL GLAZING
4	FIBERGLASS ENTRY DOOR
5	AWNING (NOT USED)
6	METAL AWNING
7	METAL GUARDRAIL
8	OUTLOOKER (NOT USED)
9	METAL SECTIONAL GARAGE DOOR
10	DECORATIVE EXTERIOR LIGHTING
11	ILLUMINATED ADDRESS SIGN
12	STANDING METAL SEAM
13	CORRUGATED METAL (NOT USED)
14	WOODEN TRELLIS (NOT USED)



RIGHT ELEVATION



REAR ELEVATION

VISTA EL SERENO



PLAN 1 - ELEVATION ALT. 2 (SHOWN WITH ROOF DECK)

A1.2

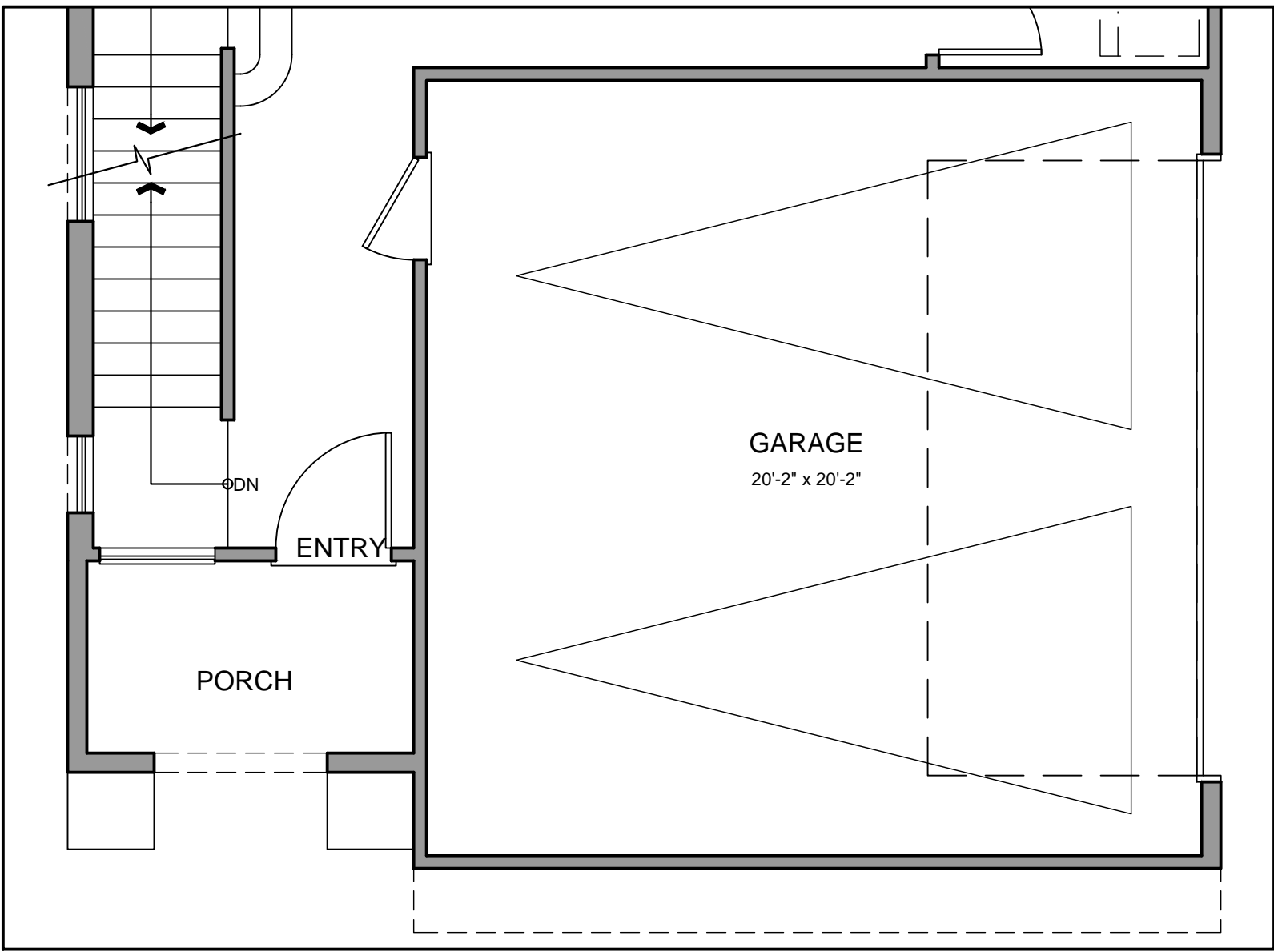
CITY OF LOS ANGELES

SCALE: 0 2 4 8

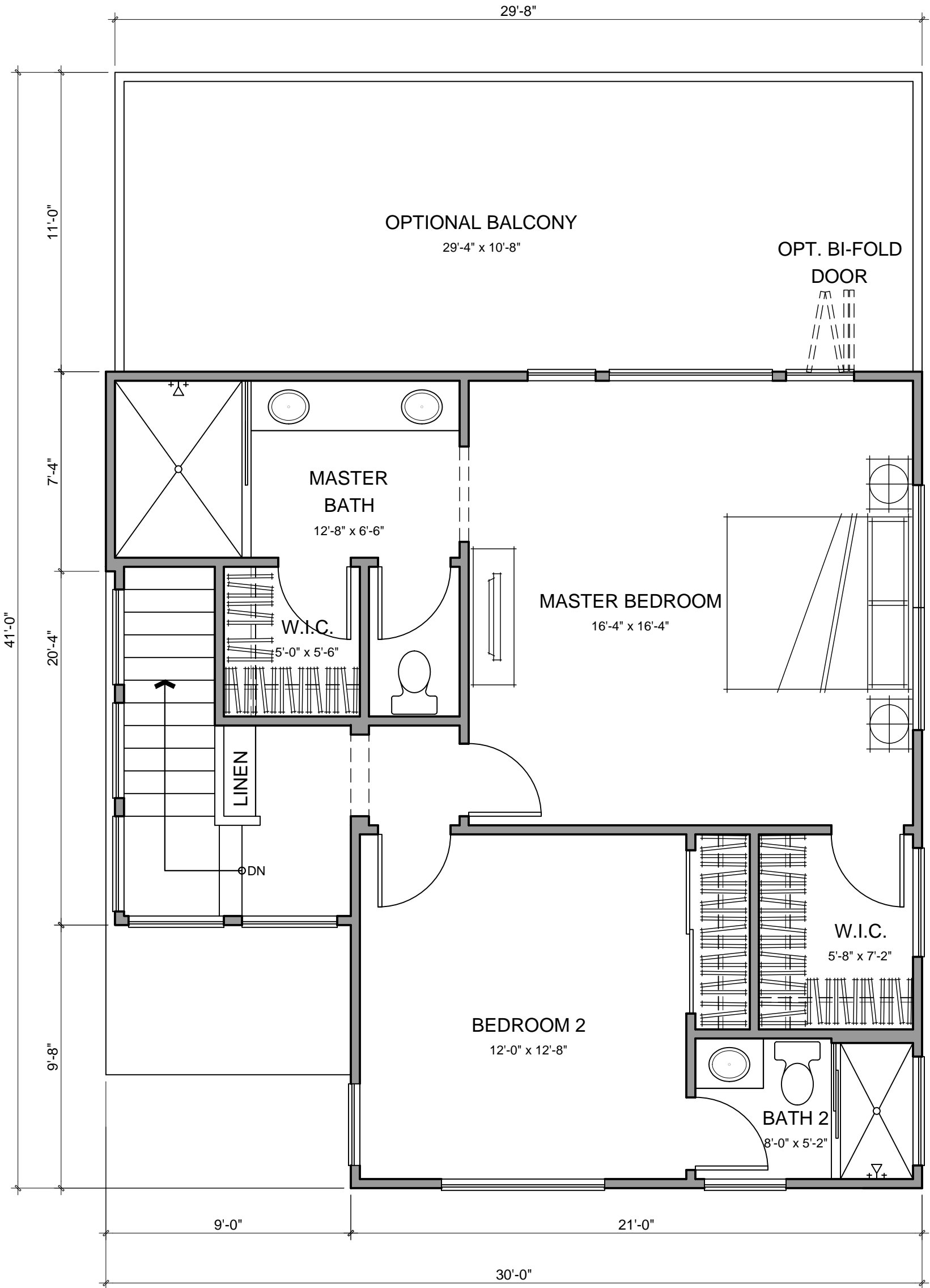
Architecture + Planning
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Los Angeles, CA 90066
ktgy.com 310 394 2633

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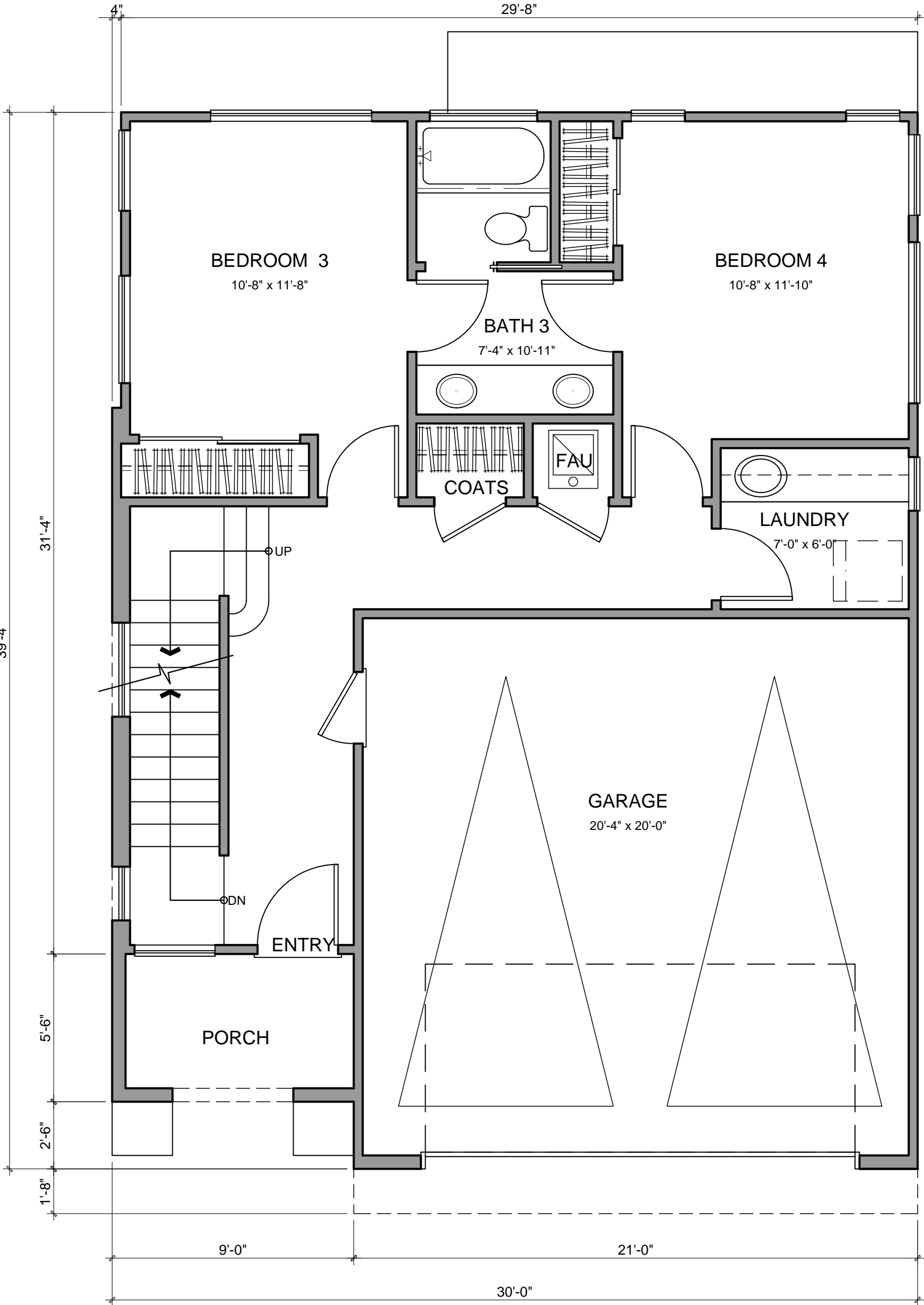




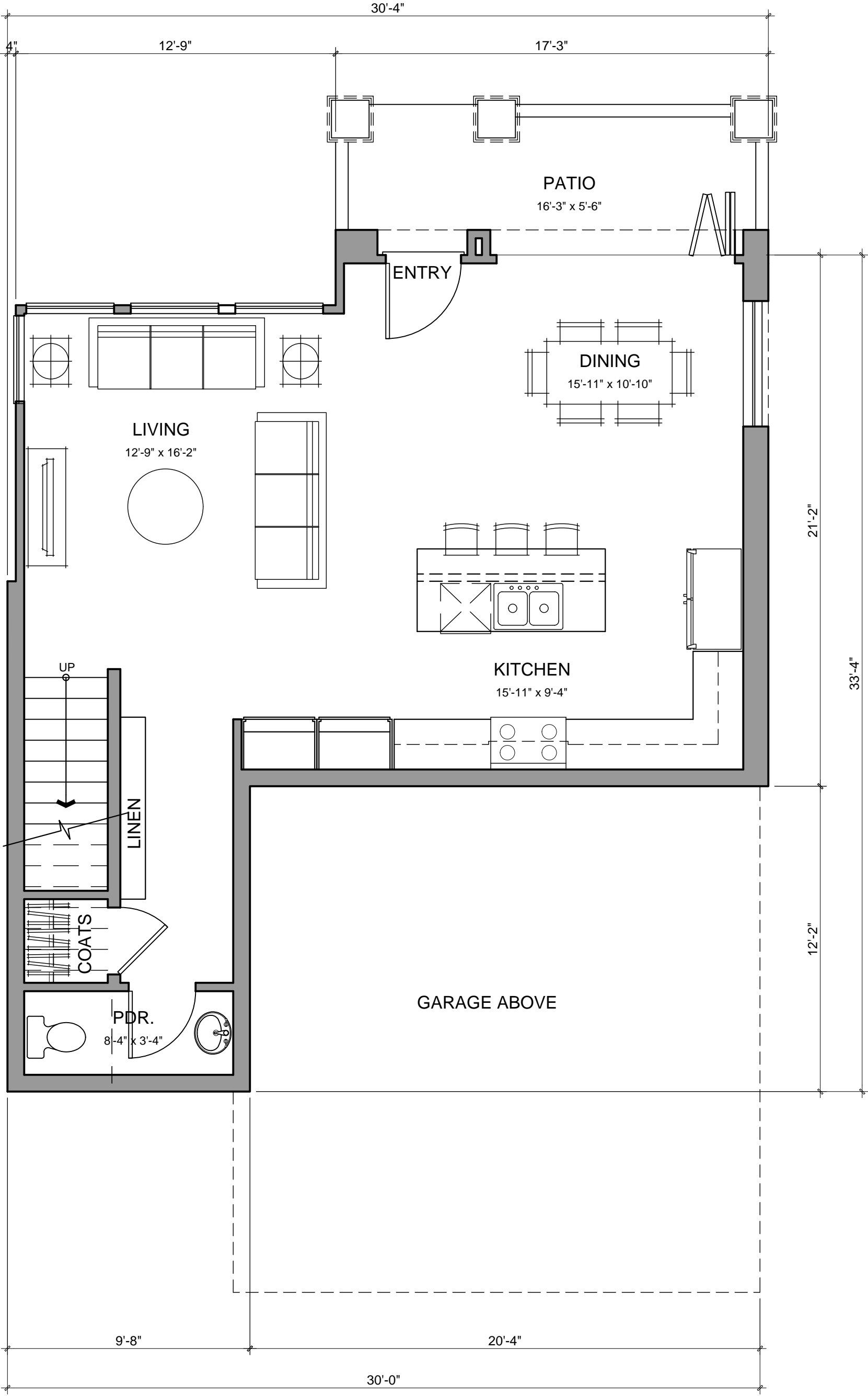
FIRST FLOOR - SIDE LOAD GARAGE ALT.



SECOND FLOOR



FIRST FLOOR



BASEMENT

VISTA EL SERENO



PLAN 3 - FLOOR PLAN (NET-2001 SF)

*NET FLOOR AREA "WITHIN THE WALLS"

A3.0

CITY OF LOS ANGELES

SQUARE FOOTAGES

BASEMENT	654 SQ. FT.
1ST FLOOR	578 SQ. FT.
2ND FLOOR	769 SQ. FT.
NET TOTAL LIVING	2001 SQ. FT.
GARAGE	426 SQ. FT.
ROOF DECK	326 SQ. FT.

SCALE: 0 2 4 8

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ktgy.com 310 394 2633

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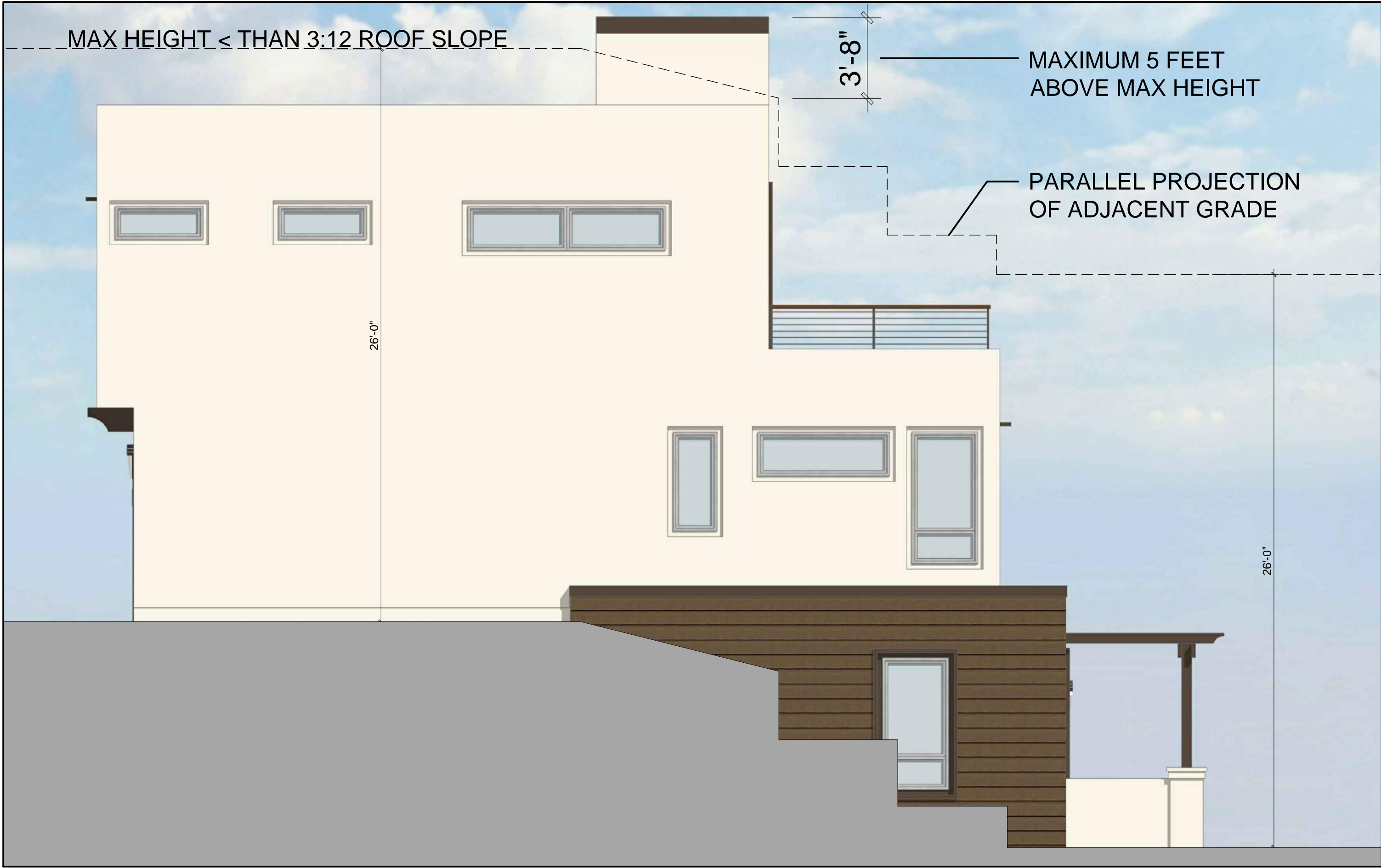


LEFT ELEVATION



FRONT ELEVATION

MATERIAL LEGEND	
1	STUCCO, LIGHT SAND FINISH
2	STAIN GRADE FIBER CEMENT SIDING
3	VINYL GLAZING
4	FIBERGLASS ENTRY DOOR
5	AWNING
6	METAL AWNING (NOT USED)
7	METAL GUARDRAIL
8	OUTLOOKER
9	METAL SECTIONAL GARAGE DOOR
10	DECORATIVE EXTERIOR LIGHTING
11	ILLUMINATED ADDRESS SIGN
12	STANDING METAL SEAM (NOT USED)
13	CORRUGATED METAL (NOT USED)
14	WOODEN TRELLIS



RIGHT ELEVATION



REAR ELEVATION

VISTA EL SERENO



PLAN 3 - ELEVATION ALT. 1

A3.1

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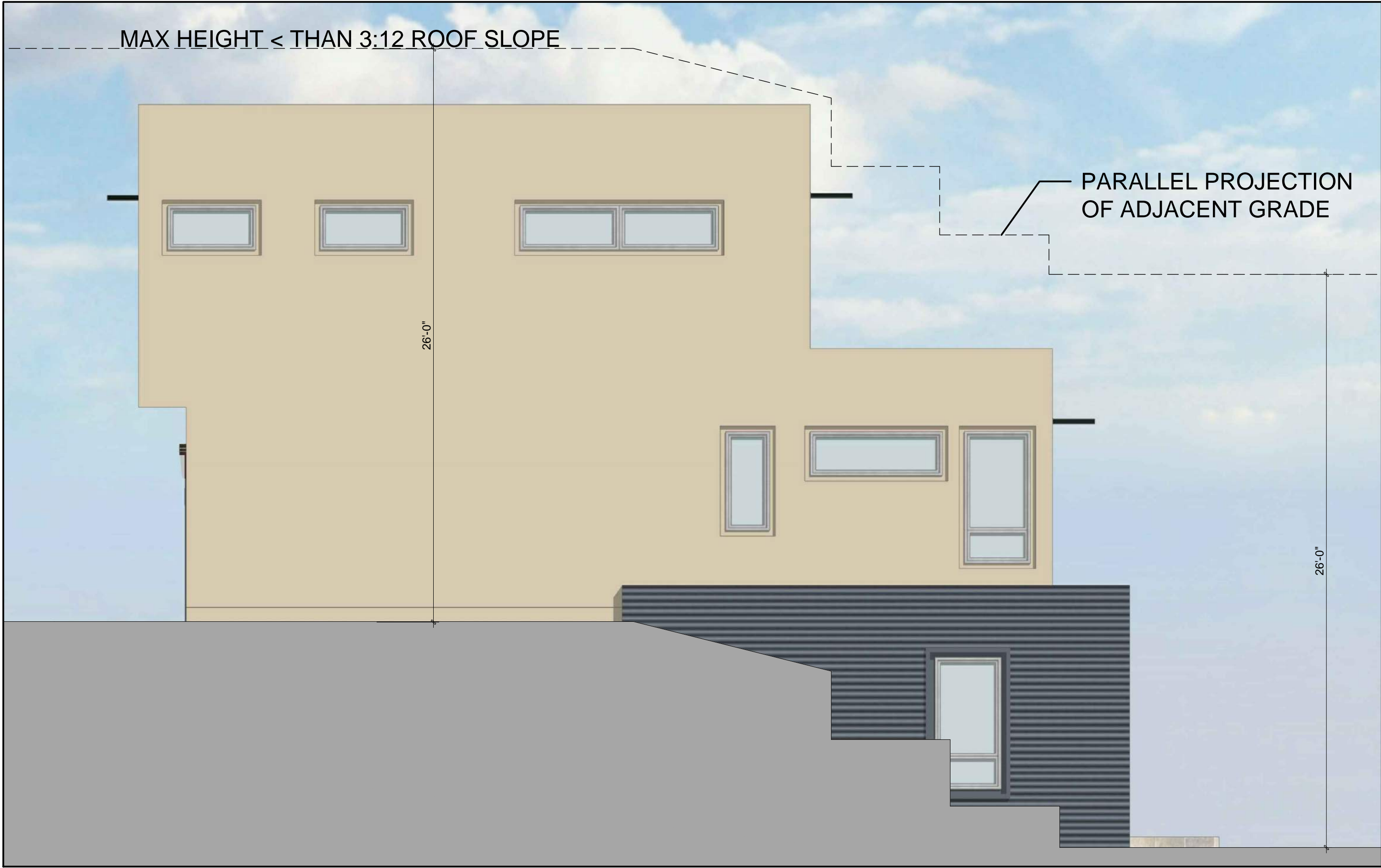


LEFT ELEVATION

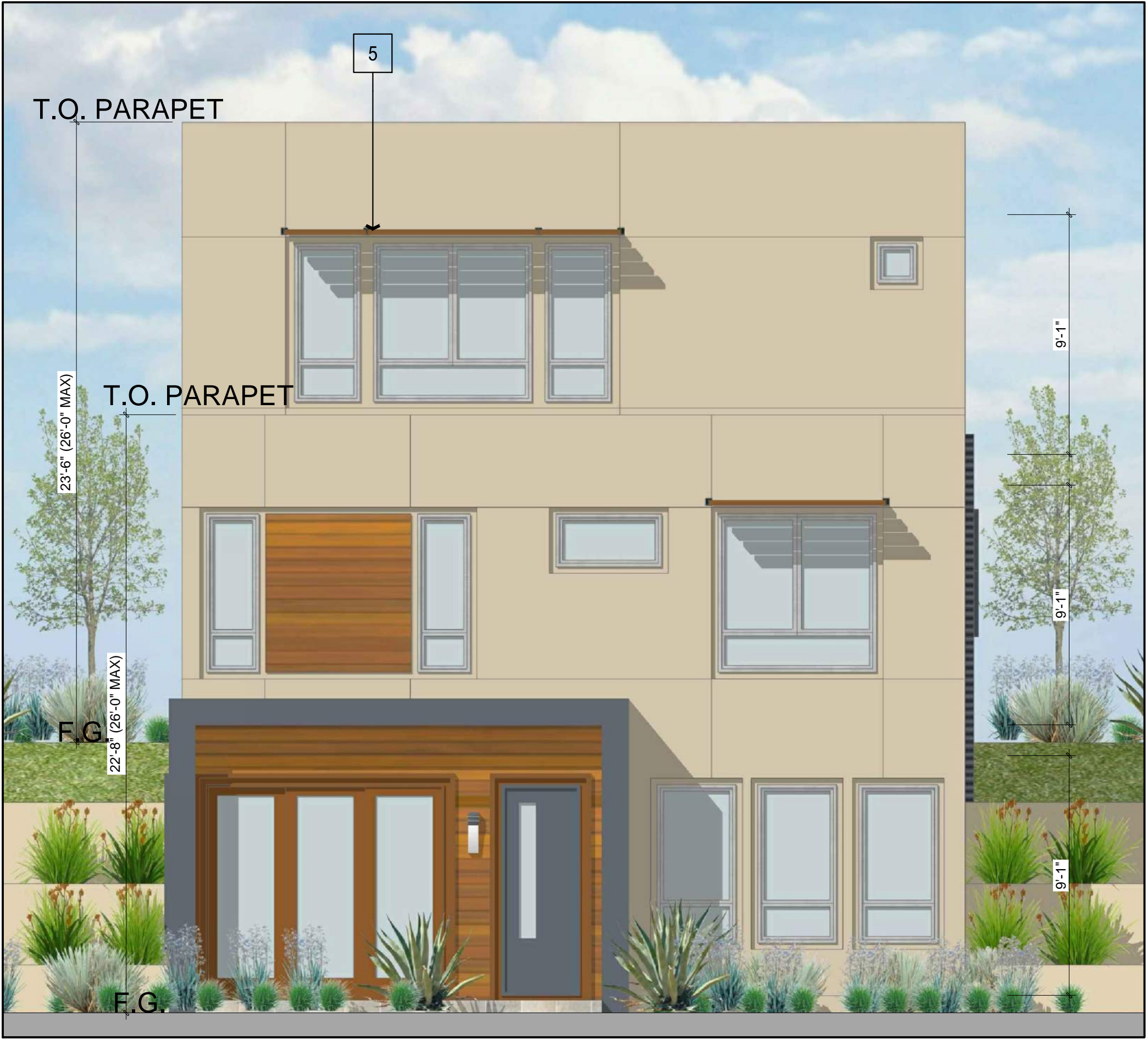


FRONT ELEVATION

MATERIAL LEGEND	
1	STUCCO, LIGHT SAND FINISH
2	STAIN GRADE FIBER CEMENT SIDING
3	VINYL GLAZING
4	FIBERGLASS ENTRY DOOR
5	AWNING
6	METAL AWNING (NOT USED)
7	METAL GUARDRAIL (NOT USED)
8	OUTLOOKER (NOT USED)
9	METAL SECTIONAL GARAGE DOOR
10	DECORATIVE EXTERIOR LIGHTING
11	ILLUMINATED ADDRESS SIGN
12	STANDING METAL SEAM (NOT USED)
13	CORRUGATED METAL
14	WOODEN TRELLIS (NOT USED)



RIGHT ELEVATION



REAR ELEVATION

VISTA EL SERENO



PLAN 3 - ELEVATION ALT. 2

A3.2

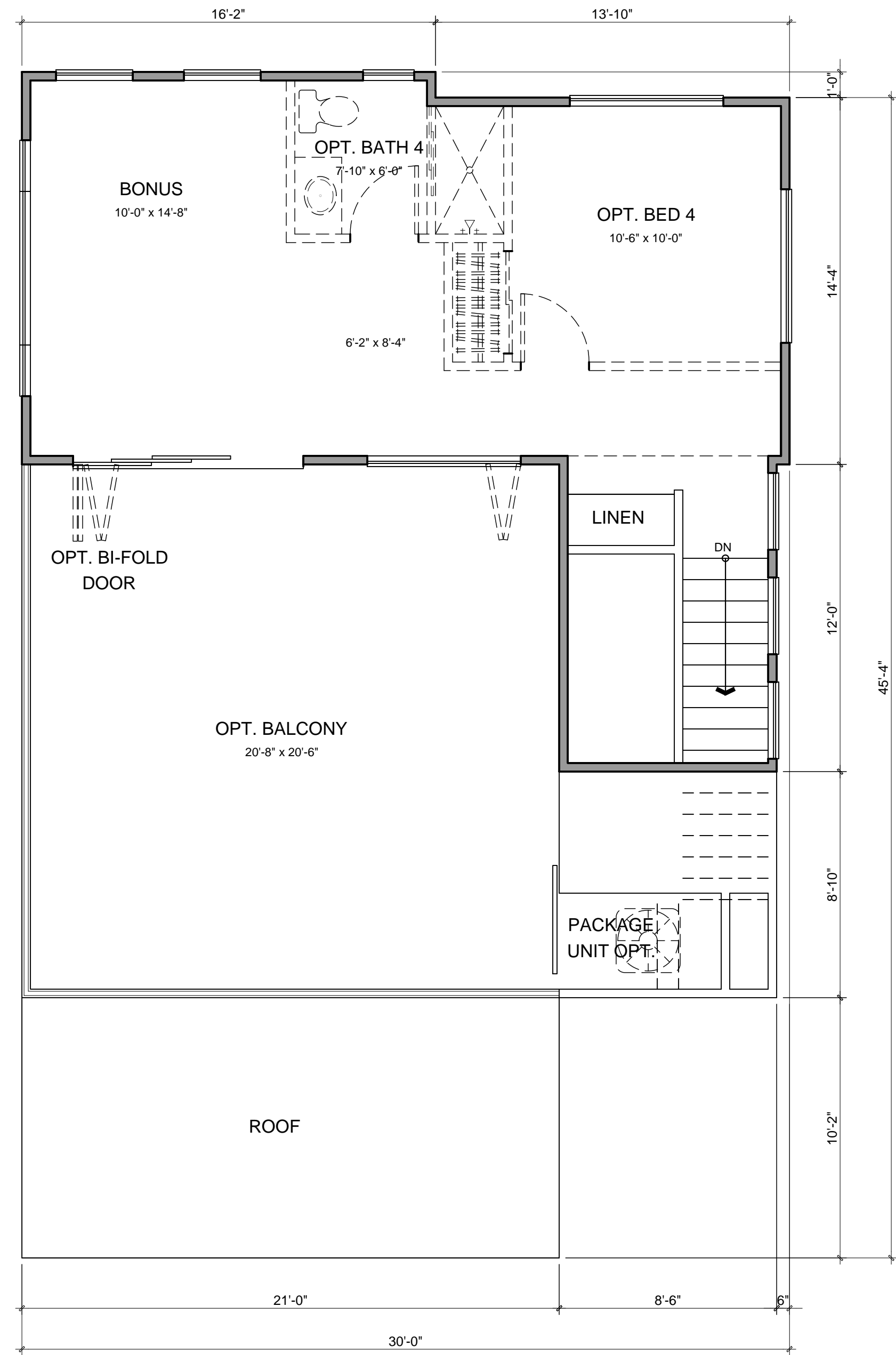
CITY OF LOS ANGELES

SCALE: 0 2 4 8

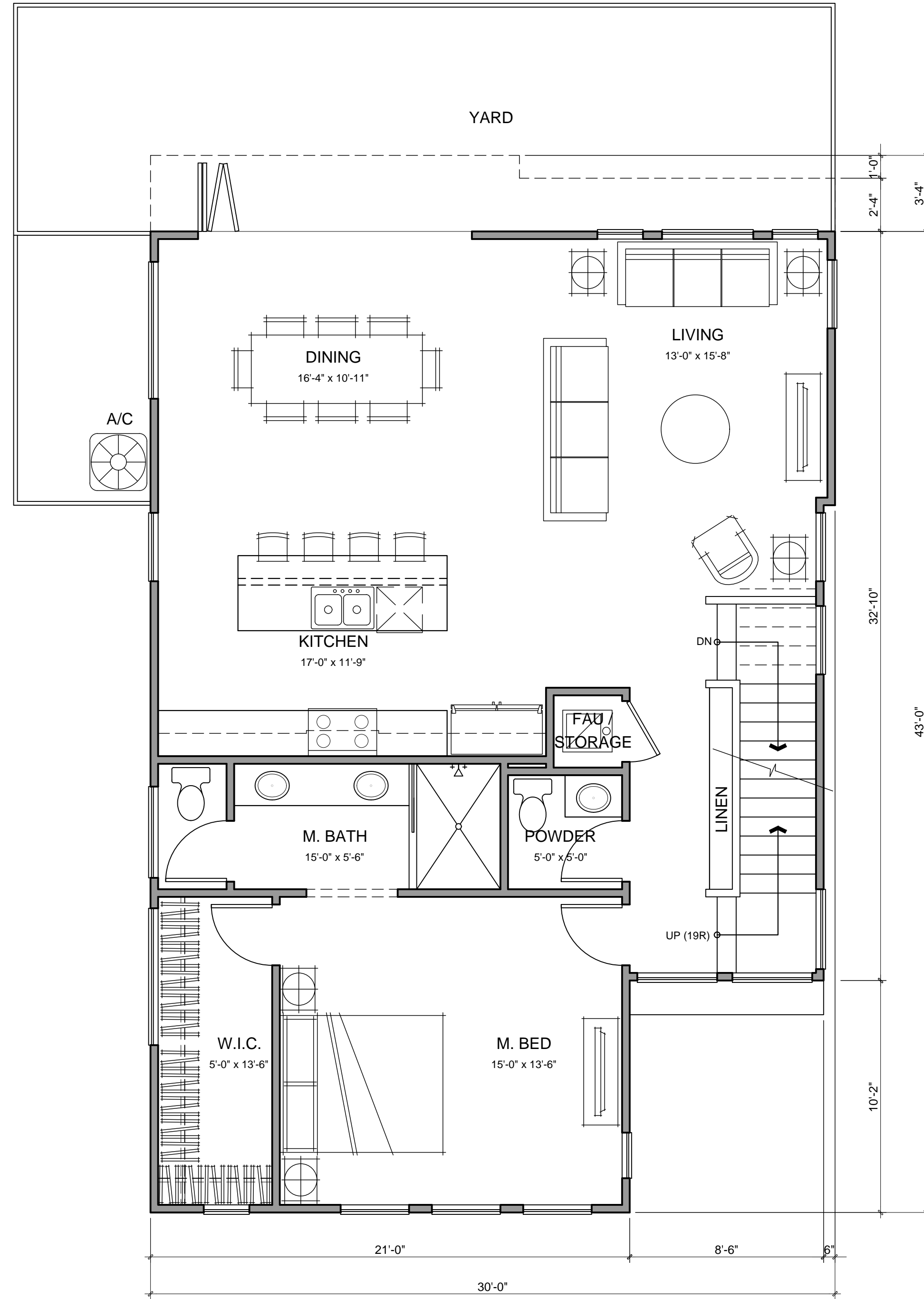
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1255 West Jefferson Blvd
Los Angeles, CA 90066
ktgy.com 310 394 2633

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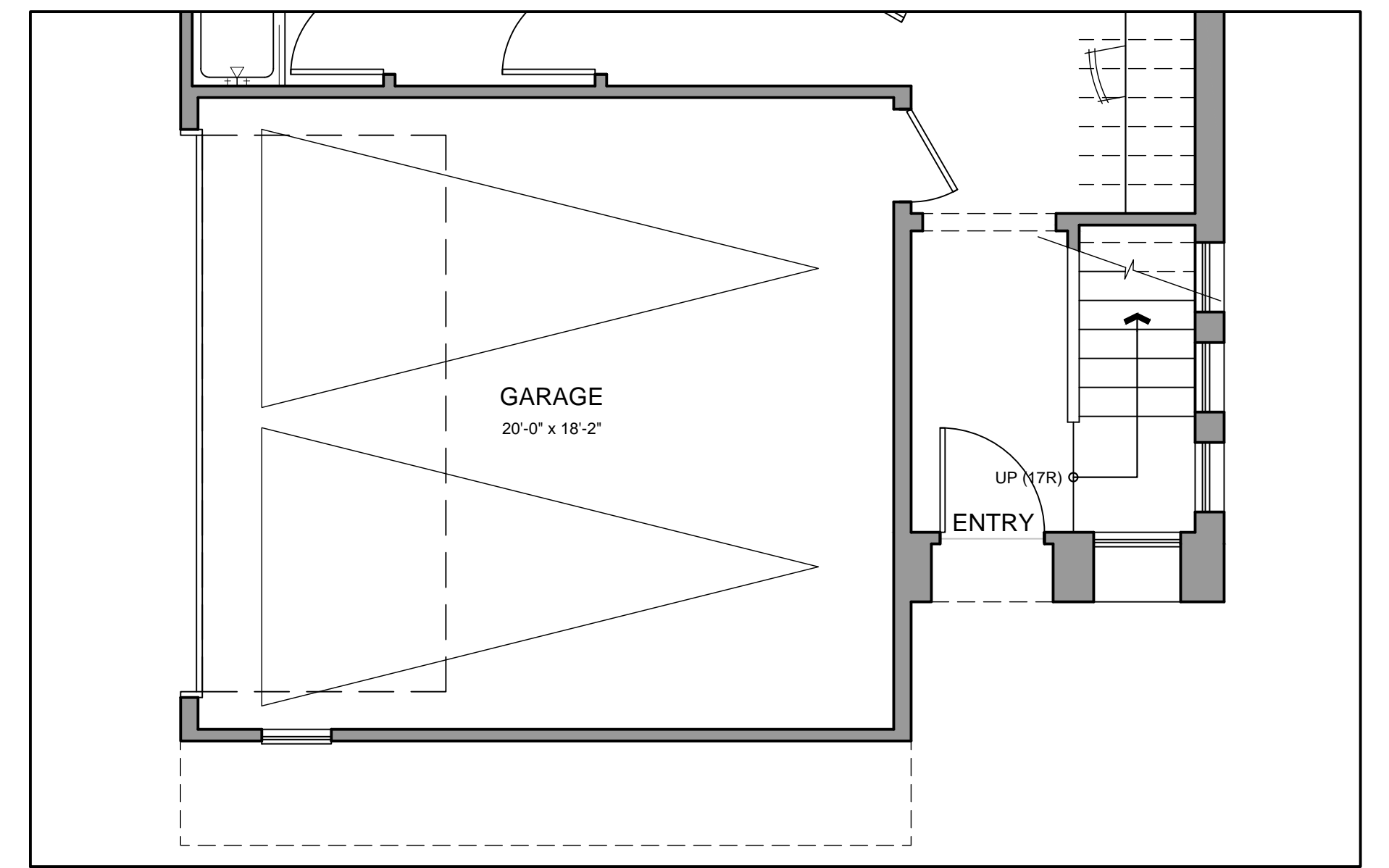




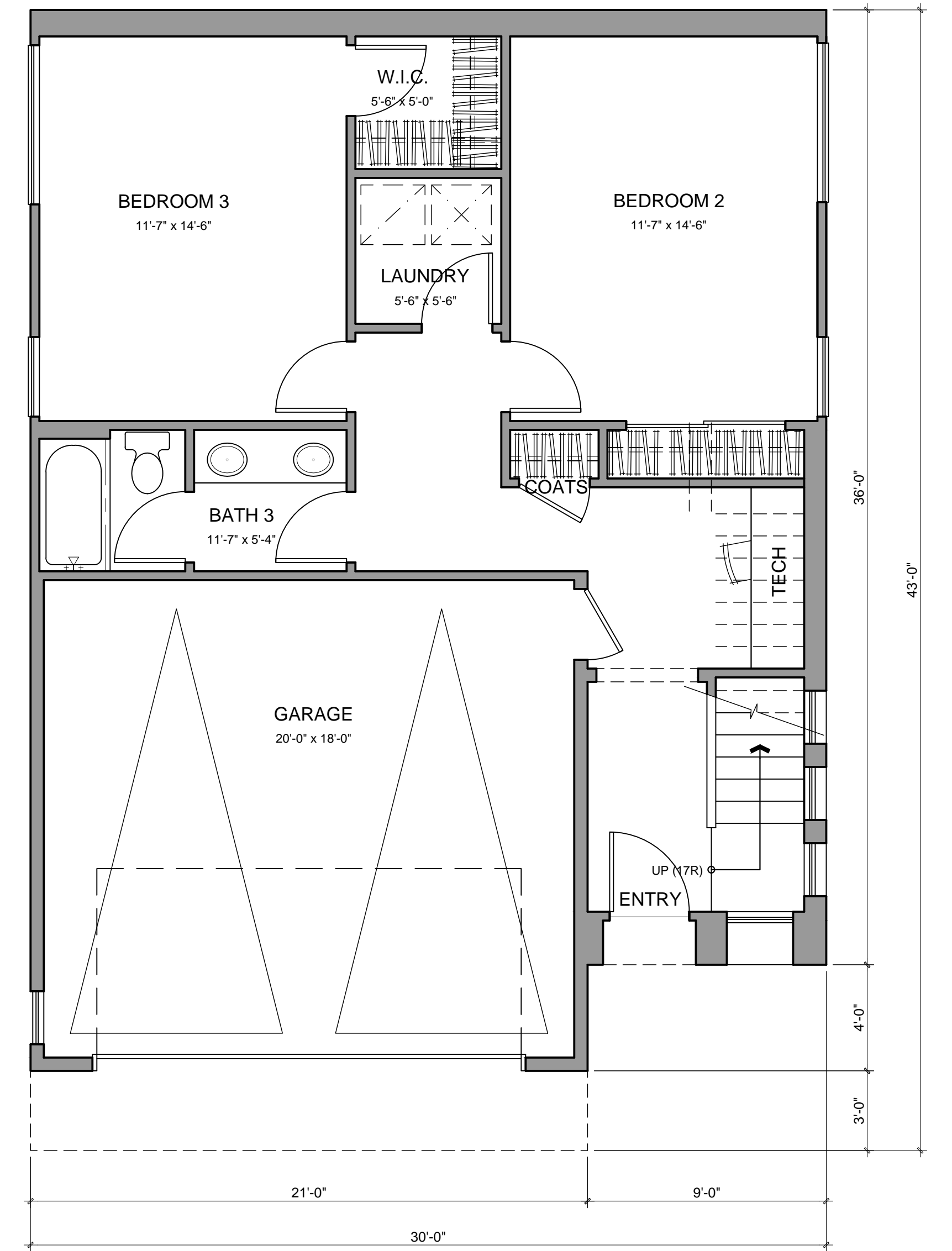
THIRD FLOOR



SECOND FLOOR



FIRST FLOOR - SIDE LOAD GARAGE ALT



FIRST FLOOR

VISTA EL SERENO



PLAN 4 - FLOOR PLAN (NET-2279 SF)

*NET FLOOR AREA "WITHIN THE WALLS"

A4.0

CITY OF LOS ANGELES

SQUARE FOOTAGES

1ST FLOOR	694 SQ. FT.
2ND FLOOR	1140 SQ. FT.
3RD FLOOR	446 SQ. FT.
NET TOTAL LIVING	2279 SQ. FT.
GARAGE	382 SQ. FT.
DECK	437 SQ. FT.

SCALE: 0 2 4 8

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1255 West Jefferson Blvd
Los Angeles, CA 90066
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LEFT ELEVATION



FRONT ELEVATION

MATERIAL LEGEND	
1	STUCCO, LIGHT SAND FINISH
2	STAIN GRADE FIBER CEMENT SIDING (NOT USED)
3	VINYL GLAZING
4	FIBERGLASS ENTRY DOOR
5	AWNING
6	METAL AWNING
7	METAL GUARDRAIL
8	OUTLOOKER (NOT USED)
9	METAL SECTIONAL GARAGE DOOR
10	DECORATIVE EXTERIOR LIGHTING
11	ILLUMINATED ADDRESS SIGN
12	STANDING METAL SEAM (NOT USED)
13	CORRUGATED METAL (NOT USED)
14	WOODEN TRELLIS (NOT USED)



RIGHT ELEVATION



REAR ELEVATION

VISTA EL SERENO

PLAN 4 - ELEVATION ALT. 1

A4.1



CITY OF LOS ANGELES

SCALE: 0 2 4 8

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LEFT ELEVATION



FRONT ELEVATION

MATERIAL LEGEND	
1	STUCCO, LIGHT SAND FINISH
2	STAIN GRADE FIBER CEMENT SIDING
3	VINYL GLAZING
4	FIBERGLASS ENTRY DOOR
5	AWNING
6	METAL AWNING (NOT USED)
7	METAL GUARDRAIL
8	OUTLOOKER (NOT USED)
9	METAL SECTIONAL GARAGE DOOR
10	DECORATIVE EXTERIOR LIGHTING
11	ILLUMINATED ADDRESS SIGN
12	STANDING METAL SEAM (NOT USED)
13	CORRUGATED METAL
14	WOODEN TRELLIS (NOT USED)



RIGHT ELEVATION



REAR ELEVATION

VISTA EL SERENO



PLAN 4 - ELEVATION ALT. 2

A4.2

CITY OF LOS ANGELES

SCALE: 0 2 4 8

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Los Angeles, CA 90066
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KTGY # 140493 9/16/2016



LEFT ELEVATION



RIGHT ELEVATION



FRONT ELEVATION

MATERIAL LEGEND	
1	STUCCO, LIGHT SAND FINISH
2	STAIN GRADE FIBER CEMENT SIDING
3	VINYL GLAZING
4	FIBERGLASS ENTRY DOOR
5	AWNING
6	METAL AWNING (NOT USED)
7	METAL GUARDRAIL
8	OUTLOOKER (NOT USED)
9	METAL SECTIONAL GARAGE DOOR
10	DECORATIVE EXTERIOR LIGHTING
11	ILLUMINATED ADDRESS SIGN
12	STANDING METAL SEAM (NOT USED)
13	CORRUGATED METAL
14	WOODEN TRELLIS (NOT USED)



REAR ELEVATION

VISTA EL SERENO



PLAN 4 - ELEVATION ALT. 3

A4.3

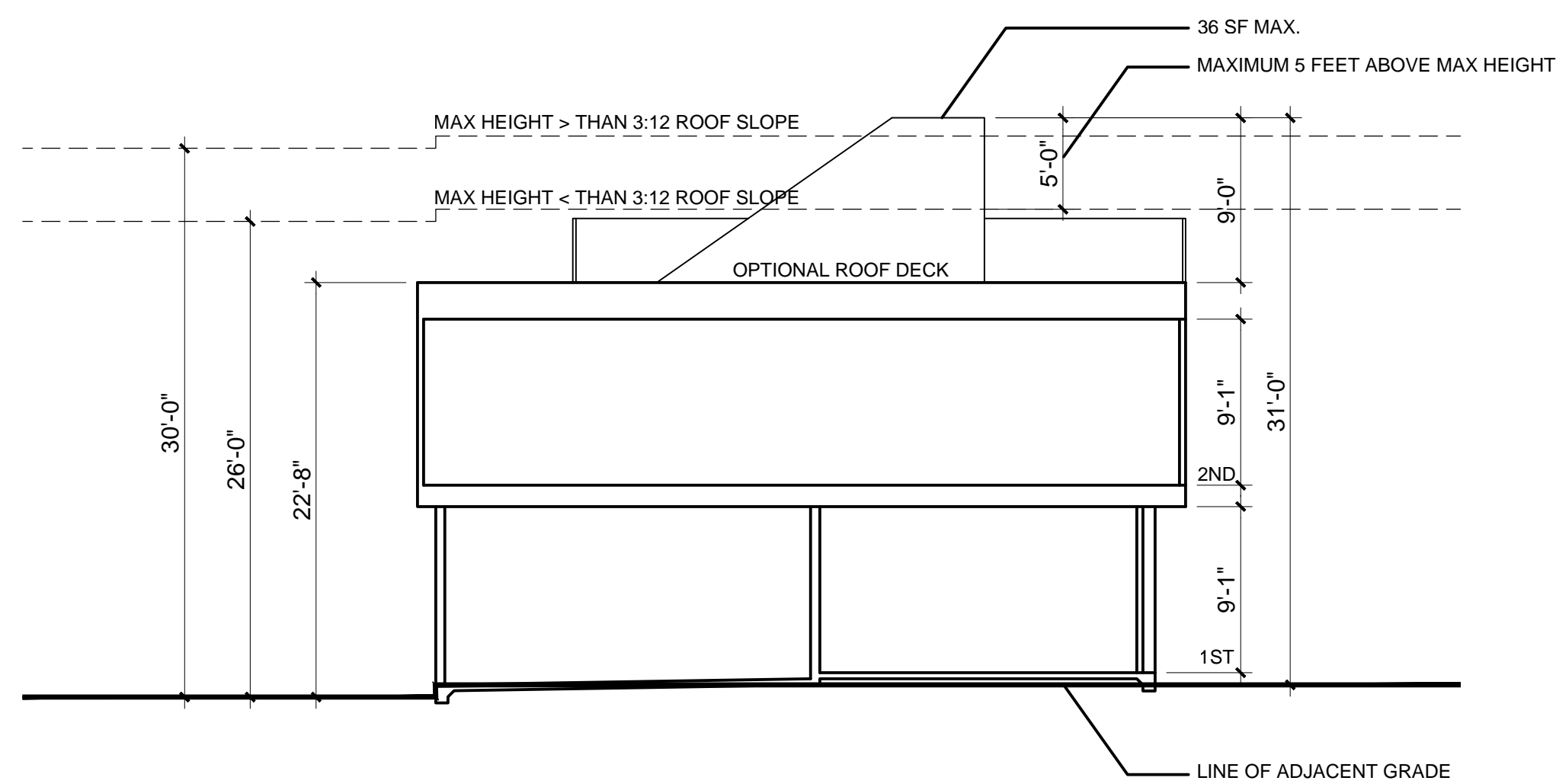
CITY OF LOS ANGELES

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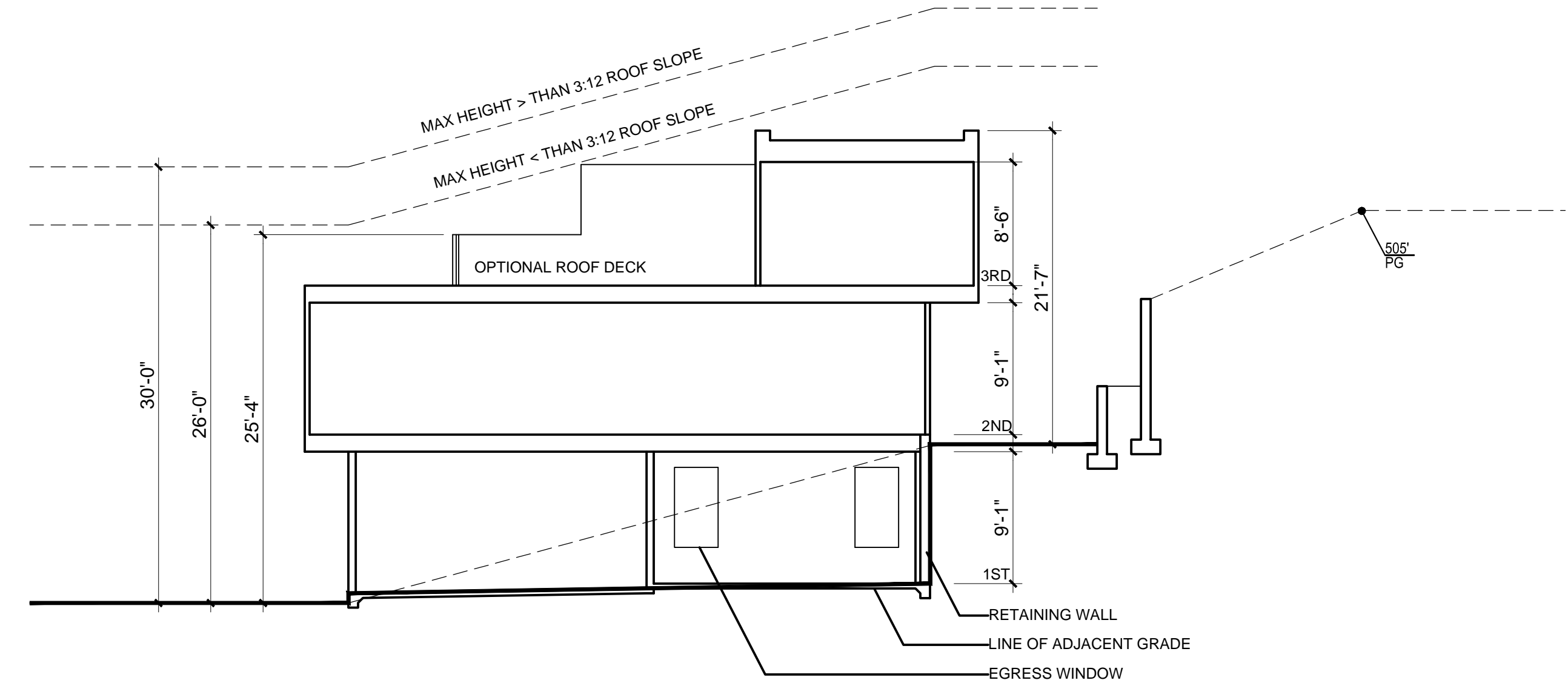
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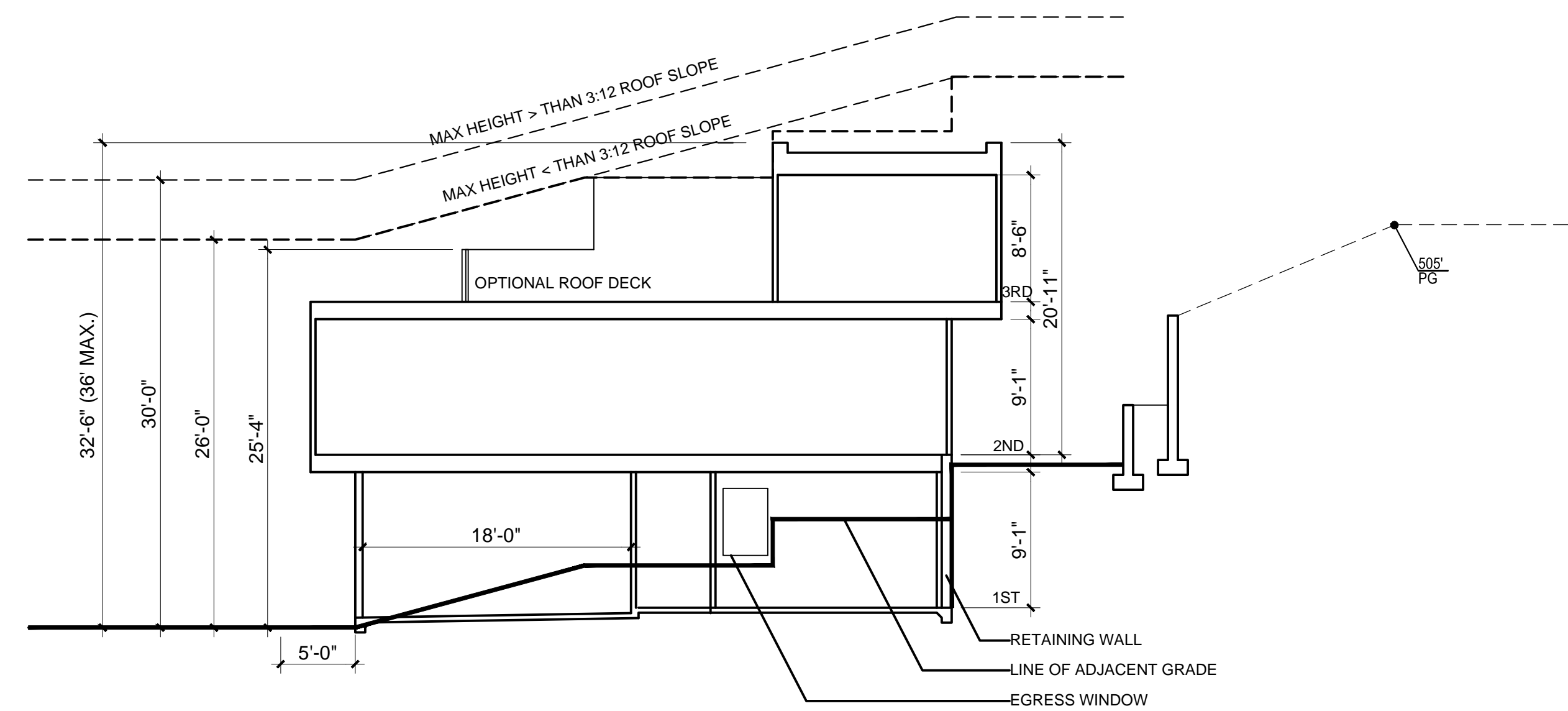
KTGY # 140493 9/16/2016



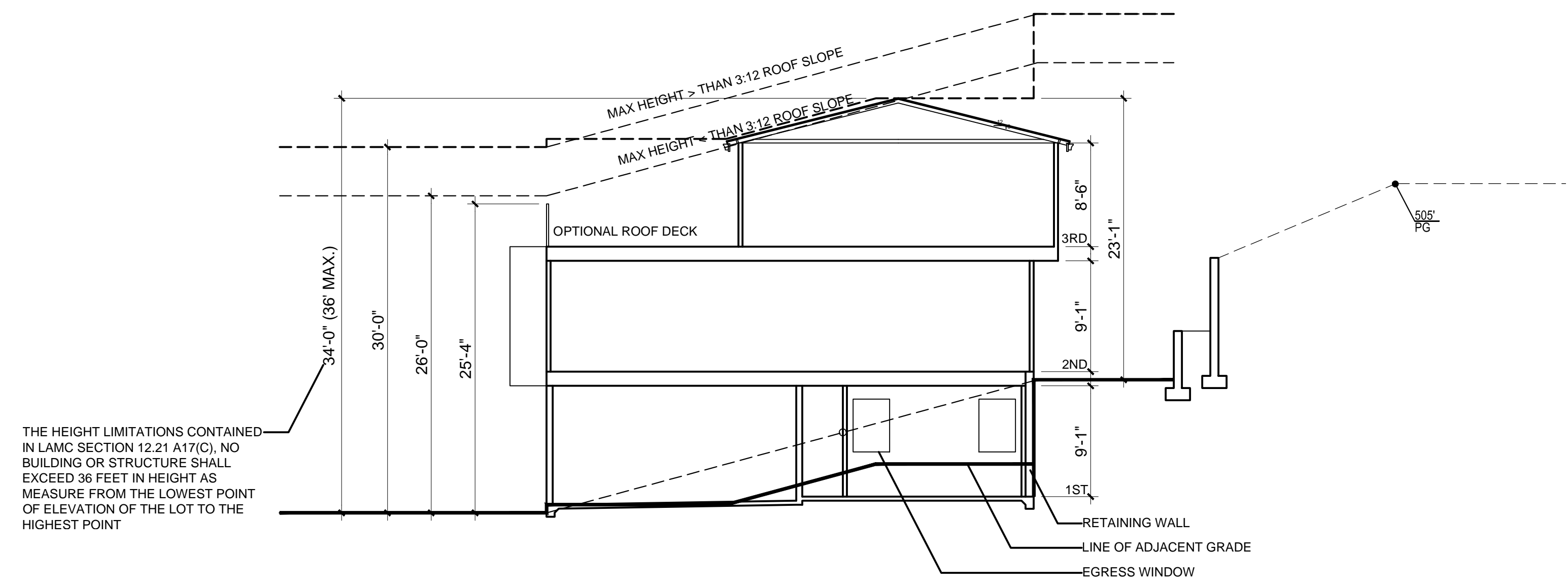
2-STORY TYPICAL INTERIOR HOUSE



TYPICAL UPHILL HOUSE - OPT. 1



TYPICAL UPHILL HOUSE



TYPICAL UPHILL HOUSE

THE HEIGHT LIMITATIONS CONTAINED IN LAMC SECTION 12.21 A17(C), NO BUILDING OR STRUCTURE SHALL EXCEED 36 FEET IN HEIGHT AS MEASURED FROM THE LOWEST POINT OF ELEVATION OF THE LOT TO THE HIGHEST POINT

VISTA EL SERENO



4685 MacArthur Court, Suite 375
Newport Beach, Ca 92660

BUILDING HEIGHT DIAGRAMS

CITY OF LOS ANGELES

SCALE: 0 4 8 16

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Los Angeles, CA 90066
ktgy.com 310 394 2633

KTGY # 140493 6/29/2018



WALNUT TREES LEGEND (TO REMAIN & REMOVED)		
SYMBOL		QUANTITY
	EXISTING PROTECTED WALNUT TREES TO REMAIN- TREE NUMBER REFERS TO TREE REPORT	65
	EXISTING WALNUT TREES TO BE REMOVED (NOTE: WILL BE REPLACED AT A 4:1 RATIO WITH JUGLANS CALIFORNICA AND QUERCUS AGRIFOLIA)- SEE PRELIMINARY TREE & SHRUB PLANTING LEGEND BELOW	37
	TREES REMOVED- STUMPS OR SEEDLINGS VISIBLE	
NOTE: 50 NON-PROTECTED TREES REPLACED AT A 5:1 RATIO WITH PODOCARPUS GRACILIOR & PLATANUS ACERFOLIA- SEE PRELIMINARY TREE & SHRUB PLANTING LEGEND BELOW.		

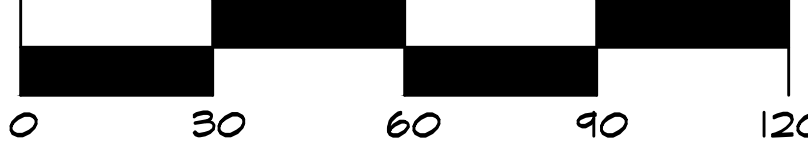
PRELIMINARY TREES PLANTING LEGEND								
SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	NOTES	HEIGHT AND WIDTH AT PLANTING	HEIGHT AND WIDE AT MATURITY	TIME OF MATURITY	QUANTITY
	JUGLANS CALIFORNICA	BLACK WALNUT	15 GAL	SINGLE STAKE	6' H. X 5'-8" W.	70' H. X 10' W.	30 YEARS	48
	QUERCUS AGRIFOLIA	COASTAL LIVE OAK	24" BOX	DOUBLE STAKE	12' H. X 8'-10' W.	30-40' H. X 25' W.	25 YEARS	97
	QUERCUS AGRIFOLIA	COASTAL LIVE OAK	36" BOX	DOUBLE STAKE	12' H. X 8'-10' W.	30-40' H. X 25' W.	20 YEARS	37
	PLATANUS ACERFOLIA	LONDON PLANE TREE	24" BOX	DOUBLE STAKE	6-7' H. X 4'-5' W.	70' H. X 30' W.	30 YEARS	11
	PODOCARPUS GRACILIOR	FERN PINE	24" BOX	SINGLE TRUNK DOUBLE STAKE	7' H. X 4' W.	20' H. X 10' W.	15 YEARS	52
PRELIMINARY SHRUB PLANTING LEGEND								
SYMBOLS	BOTANICAL NAME	COMMON NAME						
	Agave desertiana	Agave						
	Dodonaea v. 'Purpurea'	Hopsseed Bush						
	Phormium tenax	New Zealand Flax						
	Rhus integrifolia	Lemonade Berry						
	Tecoma stans	Yellow Bells						
	Arctostaphylos glauca	Eastwood Manzanita						
	Ceanothus spp.	Ceanothus						
	Grevillea noelii	Grevillea						
	Mahonia nevinii	Nevin's Barberry						
	Myrica californica	Pacific Wax Myrtle						
	Rosmarinus officinalis	Rosemary						
	Salvia spp.	Sage						
	Baccharis p. 'Pigeon Point'	Dwarf Coyote Bush						
	Iris douglasiana	Douglas Iris						
	Juncus patens 'Elk Blue'	Elk Blue California Gray Rush						
	Ficus repens	Creeeping Fig						
	Rosa banksiae	Banksiae						
	Carex divulsa	Berkeley Sedge						
	WALNUT GROVE AREA / OPEN SPACE		65,809 sq.ft. (1.51 ACRES)					
	TO BE MAINTAINED BY MAINTENANCE ASSOCIATION							



VISTA EL SERENO CONCEPTUAL LANDSCAPE PLAN
LOS ANGELES, CALIFORNIA
CLEARWATER COMMUNITIES, NEWPORT, CALIFORNIA



SCALE: 1"=30'-0"



SMP

ENVIRONMENTAL DESIGN

SUMMERSMURPHY & PARTNERS, INC.

34197 COAST HWY SUITE 200

DANA POINT CA 92629

(949) 443-1446

7-17-2018

SMP #742174

A.L.T.A. SURVEY

2520-2608 North Eastern Avenue & 2657 Lombardy Boulevard, Los Angeles, CA

TITLE EXCEPTIONS

A. Property taxes, which are a lien not yet due and payable, including any assessments collected with taxes to be levied for the fiscal year 2014–2015.

B. Property taxes, including any personal property taxes and any assessments collected with taxes are as follows:
Code Area: 0000004
Tax Identification No.: 5216–008–016
Fiscal Year: 2013–2014
1st Installment: \$3,294.47, PAID
2nd Installment: \$,294.45, OPEN
Exemption: \$0.00
Land: \$449,940.00
Improvements: \$0.00
Personal Property: \$0.00

C. Property taxes, including any personal property taxes and any assessments collected with taxes are as follows:
Code Area: 0000004
Tax Identification No.: 5216–008–034
Fiscal Year: 2013–2014
1st Installment: \$3,635.86, PAID
2nd Installment: \$3,635.84, OPEN
Exemption: \$0.00
Land: \$538,323.00
Improvements: \$0.00
Personal Property: \$0.00

D. Property taxes, including any personal property taxes and any assessments collected with taxes are as follows:

Code Area: 0000004
Tax Identification No.: 5216–009–037
Fiscal Year: 2013–2014
1st Installment: \$3,518.44, PAID
2nd Installment: \$3,518.42, OPEN
Exemption: \$0.00
Land: \$440,448.00
Improvements: \$0.00
Personal Property: \$0.00

E. Any liens or other assessments, bonds, or special district liens including without limitation, Community Facility Districts, that arise by reason of any local, City, Municipal or County Project or Special District.

F. The lien of supplemental or escaped assessments of property taxes, if any, made pursuant to the provisions of Chapter 3.5 or Part 2, Chapter 3, Articles 3 and 4 respectively (commencing with Section 75) of the Revenue and Taxation Code of the State of California as a result of the transfer of title to the vestee named in Schedule A; or as a result of changes in ownership or new construction occurring prior to date of policy.

1. Water rights, claims or title to water, whether or not disclosed by the public records.
2. Easement(s) for the purpose(s) shown below and rights incidental thereto as set forth in a document:

Purpose: Public street, road or highway
Recording No: in Book 12435, Page 200, Official Records
Affects: The Easterly 40 feet of said land

3. The right of the City of Los Angeles to slope said land wherever in their opinion, interest is necessary, as granted to the City of Los Angeles by Deed and Agreement executed by Wm. Raymond and Grace K. Raymond, his wife, recorded in Book 16886, Page 99, Official Records. Reference is made to said document for full particulars.

4. The privilege and right to slope said land wherever in the opinion of the City of Los Angeles, such slope is necessary for later or vertical support or protection of said property, said slopes to be approximately at a ratio of 1 foot horizontally to 1 foot vertically cuts and 1–1/2 foot horizontally to 1 foot vertically for fills of said land adjacent to Eastern Avenue, as granted to the City of Los Angeles, a municipal corporation, by agreements recorded September 26, 1939 in Book 16815 Page 325, Official Records and in Book 16945 Page 69, Official Records.

5. Waiver of any claims for damages to said Land by reason of the location, construction, landscaping or maintenance of the street or highway adjoining said Land, as contained in the deed to

County/City/State: City of Los Angeles
Recorded: September 26, 1939 in Book 16815, Page 325 and in Book 16945, Page 69, both of Official Records

6. A deed of trust to secure an indebtedness in the amount shown below,

Amount: \$1,000,000.00
Dated: April 30, 2010
Trustor/Grantor City Terrace, LLC
Trustee: Pan American Bank, a corporation?
Beneficiary: Pan American Bank, organized and existing under the laws of California
Loan No.: 4200168
Recording Date: January 26, 2011
Recording No: 20110145480, Official Records

THE FOLLOWING MATTERS AFFECT PARCEL B

7. The effect of a release and agreement executed by D.D. ThralkeId and Louise E. ThralkeId, husband and wife, as owners of the land described in favor of the City of Los Angeles, a municipal corporation, wherein said parties release said city from all damages in connection with the change of grade and/or improvement Eastern Avenue, Lombardy Boulevard, to Valley Boulevard, recorded July 17, 1940 in Book 17630, Page 364, Official Records. Reference is made to said document for full particulars.

8. The right to slope said land wherever necessary for lateral or vertical support or protection of said land in connection with the improvement of Eastern Avenue, as shown on plan and profile Nos. P7913 and P7917 on file in the office of the City Engineer, said right to terminate 90 days after the completion of said improvement, as granted to the City of Los Angeles, by the above mentioned release and agreement.Reference is a made to said document for full particulars.

9. The rights of the public to use that portion herein described property lying within a publicly maintained road.

THE FOLLOWING MATTERS AFFECT PARCEL C

10. Covenants, conditions and restrictions but omitting any covenants or restrictions, if any, including but not limited to those based upon race, color, religion, sex, sexual orientation, familial status, marital status, disability, handicap, national origin, ancestry, source of income, gender, gender identity, gender expression, medical condition or genetic information, as set forth in applicable state or federal laws, except to the extent that said covenant or restriction is permitted by applicable law, as set forth in the document

Recording No: in Book 3867, Page 340, Official Records, and various other document of record
Said covenants, conditions and restrictions provide that a violation thereof shall not defeat the lien of any mortgage or deed of trust made in good faith and for value.

11. Such rights or easements in favor of the City of Los Angeles, as successor to Los Angeles Gas & Electric Corporation, affecting the portion of said land herein stated, for pole lines, conduits and incidental purposes, disclosed by a declaration by the Department of Water and Power of said City.

Affects: The rear 5 feet of Parcel C
Recorded: in Book 15644, Page 194, Official Records

12. A Notice of Substandard property as disclosed by a document

Recording Date: October 27, 1997
Recording No: 97–1687380, Official Records
Reference is hereby made to said document for full particulars.

THE FOLLOWING MATTERS AFFECT ALL PARCELS

13. A deed of trust to secure an indebtedness in the amount shown below,

Amount: \$750,000.00
Dated: September 7, 2005
Trustor/Grantor Bancomer Construction and Development, Inc., a California corporation and City Terrace, LLC
Trustee: Preferred Bank
Beneficiary: Preferred Bank
Recording Date: September 12, 2005
Recording No: 05–2186835, Official Records

An agreement to modify the terms and provisions of said deed of trust as therein provided

Executed by: Bancomer Construction & Development, Inc. and City Terrace, LLC and Preferred Bank
Recording Date: February 7, 2007
Recording No: 20070417926, Official Records

An agreement to modify the terms and provisions of said deed of trust as therein provided

Executed by: City Terrace, LLC and Bancomer Construction & Development, Inc.
Recording Date: December 4, 2012
Recording No: 20120826589, Official Records

14. An assignment of all moneys due, or to become due as rental or otherwise from said Land, to secure payment of an indebtedness, shown below and upon the terms and conditions therein

Amount: \$750,000.00
Assigned to: Preferred Bank
Assigned By: Bancomer Construction and Development, Inc., a California corporation and City Terrace, LLC, a California limited liability company
Recording Date: September 12, 2005
Recording No: 05–2186836, Official Records

15. Matters contained in that certain document
Entitled:

Hazardous Substances Certificate and Indemnity Agreement
Dated: September 7, 2005
Executed by: Bancomer Construction & Development, Inc. and City Terrace, LLC and Preferred Bank
Recording Date: September 12, 2005
Recording No: 05 2186837, Official Records
Reference is hereby made to said document for full particulars.

16. Matters contained in that certain document
Entitled: Memorandum Withdrawing Non–Rented Housing Accommodations from the Rental Market

Executed by: Bancomer Construction & Development, Inc., and City Terrace, LLC
Recording Date: February 22, 2007
Recording No: 20070384762, Official Records
Reference is hereby made to said document for full particulars.yk,

17. Matters contained in that certain document
Entitled: Memorandum Summarizing Non–Confidential Provisions of a Notice of Intent to Withdraw Units from Rental Housing Use

Executed by: Bancomer Construction & Development, Inc., and City Terrace, LLC
Recording Date: April 2, 2008
Recording No: 20080570225, Official Records
Reference is hereby made to said document for full particulars.

18. Matters contained in that certain document
Entitled: Notice of Constraints to Landlords and Successors in Interestom

Executed by: City of Los Angeles Housing Department
Recording Date: February 1, 2010
Recording No: 20100143613, Official Records
Reference is hereby made to said document for full particulars.

19. A lien for unsecured property taxes filed by the tax collector of the county shown, for the amount set forth, and any other amounts due.
County: Los Angeles
Fiscal Year: 2005
Taxpayer: City Terrace LLC
County Identification Number: 06080–00455
Amount: \$1,381.50
Recording Date: March 23, 2006
Recording No: 20060626754, Official Records

20. A lien for unsecured property taxes filed by the tax collector of the county shown, for the amount set forth, and any other amounts due.
County: Los Angeles
Fiscal Year: 2005
Taxpayer: City Terrace LLC
County Identification Number: 06080–00456
Amount: \$151.99
Recording Date: March 23, 2006
Recording No: 20060626755, Official Records

21. A lien for unsecured property taxes filed by the tax collector of the county shown, for the amount set forth, and any other amounts due.

County: Los Angeles
Fiscal Year: 2006
Taxpayer: City Terrace LLC
County Identification Number: 07106–00938
Amount: \$931.45
Recording Date: April 17, 2007
Recording No: 07 0916585, Official Records

22. A state tax lien for the amount shown and any other amounts due,

State Identification No: W101202034
Filed by: State of California Employment Development Department
Taxpayer: Bancomer Construction & Development, Inc.
Amount: \$62,059.41
Recording Date: May 20, 2010
Recording No: 20100693001, Official Records

23. Any rights of the parties in possession of a portion of, or all of, said Land, which rights are not disclosed by the public records.

The Company will require, for review, a full and complete copy of any unrecorded agreement, contract, license and/or lease, together with all supplements, assignments and amendments thereto, before issuing any policy of title insurance without excepting this item from coverage.r

The Company reserves the right to except additional items and/or make additional requirements after reviewing said documents.

24. The Company will require the following documents for review prior to the issuance of any title assurance predicated upon a conveyance or encumbrance by the corporation named below:

Name of Corporation: Bancomer Construction and Development, Inc., a California corporation

a) A Copy of the corporation By–laws and Articles of Incorporation

b) An original or certified copy of a resolution authorizing the transaction contemplated herein

c) If the Articles and/or By–laws require approval by a 'parent' organization, a copy of the Articles and Bylaws of the parent.

The Company reserves the right to add additional items or make further requirements after review of the requested documentation.

25. The Company will require the following documents for review prior to the issuance of any title assurance predicated upon a conveyance or encumbrance from the entity named below:

Limited Liability Company: City Terrace, LLC, a California limited liability company

a) A copy of its operating agreement, if any, and any and all amendments, supplements and/or modifications thereto, certified by the appropriate manager or member

b) If a domestic Limited Liability Company, a copy of its Articles of Organization and all amendments thereto with the appropriate filing stamps

c) If the Limited Liability Company is member–managed, a full and complete current list of members certified by the appropriate manager or member

d) If the Limited Liability Company was formed in a foreign jurisdiction, evidence, satisfactory to the Company, that it was validly formed, is in good standing and authorized to do business in the state of origin

e) If less than all members, or managers, as appropriate, will be executing the closing documents, furnish evidence of the authority of those signing.

The Company reserves the right to add additional items or make further requirements after review of the requested documentation.

26. In order to complete this report, the Company requires a Statement of Information to be completed by the following party(s),

Party(s): All Parties
The Company reserves the right to add additional items or make further requirements after review of the requested Statement of Information.

NOTE: The Statement of Information is necessary to complete the search and examination of title under this order.

Any title search includes matters that are indexed by name only, and having a completed Statement of Information assists the Company in the elimination of certain matters which appear to involve the parties but in fact affect another party with the same or similar name. Be assured that the Statement of Information is essential and will be kept strictly confidential to this file.

END OF ITEMS

LEGAL DESCRIPTION

THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:

PARCEL A:
THAT PORTION OF THE LOT MARKED "CORNWALL" ON THE PARTITION MAP OF THE SMITH ESTATE, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 6 PAGE 156 OF MISCELLANEOUS RECORDS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHEAST CORNER OF SAID LOT MARKED "CORNWALL", BEING ALSO THE SOUTHEAST CORNER OF LOT 1 OF THE FARMDALE TRACT, AS PER MAPS RECORDED IN BOOK 4 PAGE 37 OF MAPS, IN SAID OFFICE OF THE COUNTY RECORDER; THENCE SOUTH ALONG THE EAST LINE OF SAID "CORNWALL" LOT, 180.87 FEET; THENCE WEST PARALLEL WITH THE SOUTH LINE OF SAID FARMDALE TRACT, WHICH IS ALSO THE NORTH LINE OF SAID "CORNWALL" LOT, 538.78 FEET, MORE OR LESS, TO THE EASTERLY LINE OF FARMDALE AVENUE, FORMERLY KNOWN AS BROWN ROAD, AS SAID ROAD WAS DESCRIBED IN THE DEED TO THE COUNTY OF LOS ANGELES, RECORDED IN BOOK 177 PAGE 339 OF DEEDS, IN SAID OFFICE OF THE COUNTY RECORDER; THENCE NORTH 32° 28' EAST ALONG THE EASTERLY LINE OF SAID AVENUE, 214.57 FEET, MORE OR LESS, TO THE NORTH LINE OF SAID LOT MARKED "CORNWALL"; THENCE EAST ALONG SAID LAST MENTIONED NORTH LINE 424.62 FEET TO THE POINT OF BEGINNING.

PARCEL B:

PARCEL 1:
LOT 1 OF FARMDALE TRACT, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 4 PAGE 37 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

PARCEL 2:
THAT PORTION OF EASTERN AVENUE, FORMERLY BROWN ROAD, AS SHOWN ON SAID MAP OF FARMDALE TRACT, VACATED, ADJOINING SAID LOT 1, ON THE NORTHWEST WHICH LIES SOUTHEASTERLY OF THE SOUTHEASTERLY LINE OF EASTERN AVENUE 80 FEET WIDE, AS ESTABLISHED BY VACATION ORDINANCE NO. 83066, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA.

PARCEL C:
LOT 562 OF TRACT NO. 6900, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 108 PAGE 55 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

APN: 5216–008–016, 5216–008–034, 5216–009–037

BASIS OF BEARINGS

THE BEARINGS FOR THIS MAP ARE BASED ON THE CENTERLINE OF LOMBARDY AVENUE AS SHOWN ON A RECORD OF SURVEY, FILED IN BOOK XXX, PAGES 20–50, RECORDS OF SAN BERNARDINO COUNTY, BEING NORTH XX°XX'XX" WEST.

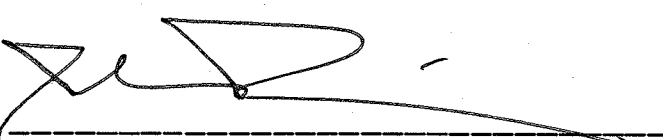
LAND SURVEYOR'S CERTIFICATE

TO: BANCOMER CONSTRUCTION and DEVELOPMENT, INC., CITY TERRACE, LLC, AND FIDELITY NATIONAL TITLE COMPANY.

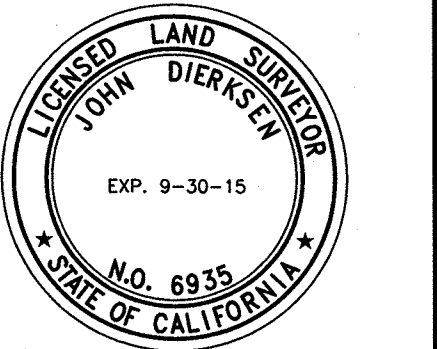
THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE MAY 5, 2014 ON THE PREMISES DESCRIBED IN SAID FIDELITY NATIONAL TITLE COMPANY PRELIMINARY TITLE REPORT (ORDER NO. 08–23044254–PP2), DATED MARCH 11, 2014 IN ACCORDANCE WITH THE "MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA AND ACSM LAND TITLE SURVEYS," JOINTLY ESTABLISHED AND ADOPTED BY ALTA, ACSM AND NSPS IN 2011, AS DEFINED THEREIN AND INCLUDES ITEMS 1, 2, 3, 4, 6, 7(B)(1) AND MEETS THE ACCURACY REQUIREMENTS OF AN "URBAN SURVEY," AS DEFINED THEREIN.

THE SUBJECT PROPERTY HAS INGRESS AND EGRESS TO AND FROM LOMBARDY ROAD (PAVED) AND EASTERN AVENUE (PAVED) WHICH ARE PUBLIC RIGHTS–OF–WAY.


ASSEMBLY BILL "ARE AB7073" DECLARED THAT THE USE OF THE WORDS "CERTIFY" AND "CERTIFICATION" BY A REGISTERED CIVIL ENGINEER OR LICENSED LAND SURVEYOR CONSTITUTES A PROFESSIONAL OPINION AND IS NOT A WARRANTY OR GUARANTY.

 6/17/14

JOHN DIERKSEN, L.S. NO. 6935 DATE
MY REGISTRATION EXPIRES 09–30–15



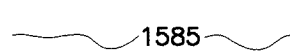

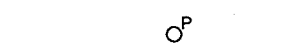



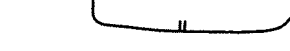

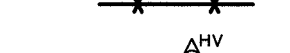

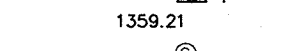
















UPDATED 06.17.14
PREPARED 05.29.14 1 OF 2

A.L.T.A. SURVEY - EXISTING SITE	
PREPARED FOR:	
BANCOMER CONSTRUCTION and DEVELOPMENT, INC., a California corporation and CITY TERRACE, LLC, a California limited liability company,	
PREPARED BY:	
 PHB & ASSOCIATES, INC. <small>(800) 914-5258 FAX (800) 914-5258 1620 SOUTH GRAND AVENUE, GLENDORE, CALIFORNIA 91740</small>	

A.L.T.A. SURVEY

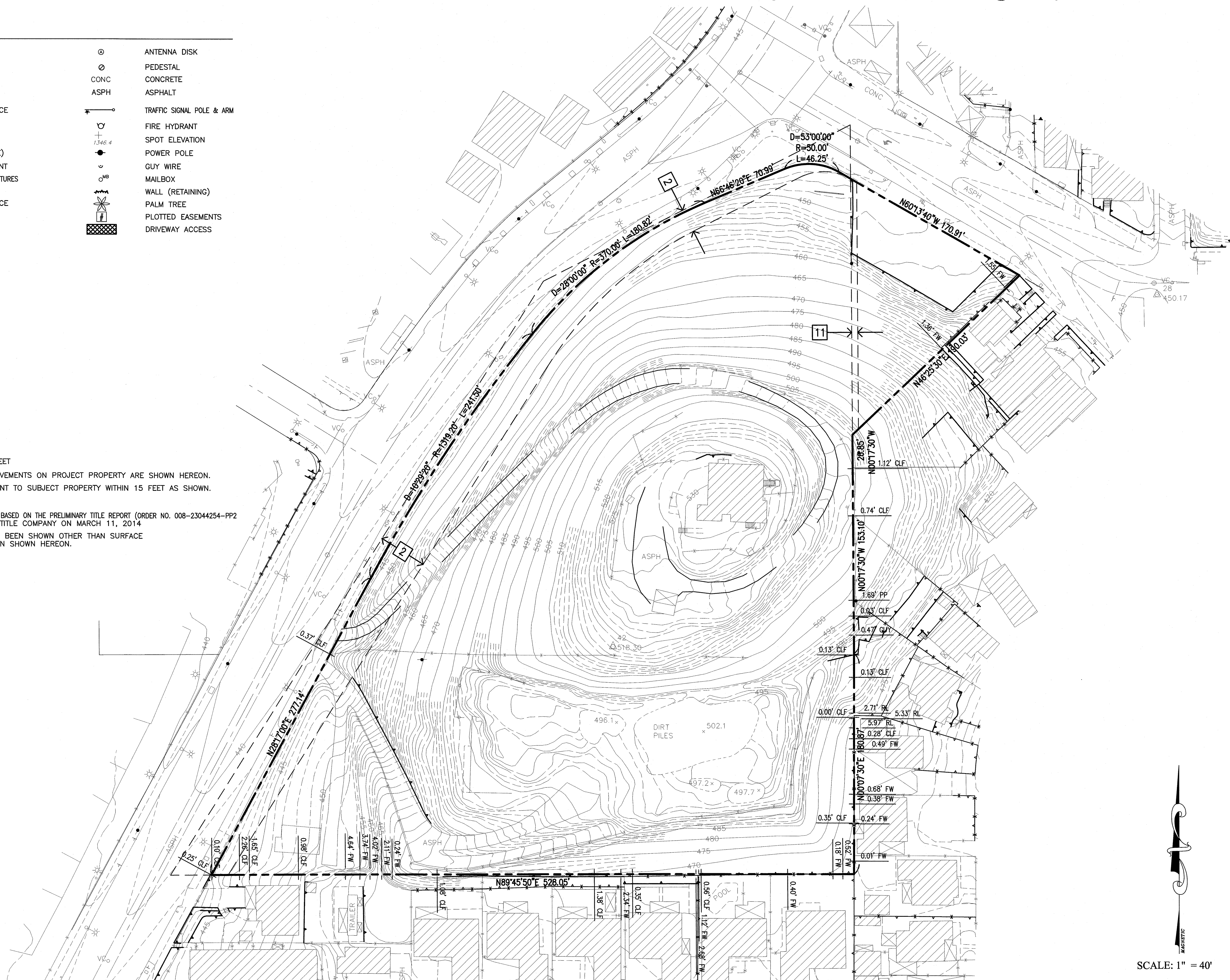
2520-2608 North Eastern Avenue & 2657 Lombardy Boulevard, Los Angeles, CA

LEGEND:

	CONTOUR LINE		ANTENNA DISK
	POST		PEDESTAL
	MANHOLE		CONCRETE
	ROCK		ASPHALT
	CHAIN LINK FENCE		TRAFFIC SIGNAL POLE & ARM
	AERIAL TARGET		FIRE HYDRANT
	SIGN		SPOT ELEVATION
	TREE (DRIP LINE)		POWER POLE
	FOUND MONUMENT		GUY WIRE
	ROOFLINE OF STRUCTURES		MAILBOX
	CLF		WALL (RETAINING)
	FW		PALM TREE
	RL		PLOTTED EASEMENTS
			DRIVEWAY ACCESS

NOTES

1. DATE OF SURVEY: MAY 5, 2014
2. SCALE: 1" = 40'
3. AREA: 218,532 GROSS SQUARE FEET
4. EXISTING STRUCTURES AND IMPROVEMENTS ON PROJECT PROPERTY ARE SHOWN HEREON.
5. EXISTING STRUCTURES ON ADJACENT TO SUBJECT PROPERTY WITHIN 15 FEET AS SHOWN.
6. FLOOD HAZARD STATUS: ZONE X
7. THE EASEMENTS MENTIONED HEREON ARE BASED ON THE PRELIMINARY TITLE REPORT (ORDER NO. 008-23044254-PP2 PREPARED BY FIDELITY NATIONAL TITLE COMPANY ON MARCH 11, 2014
8. NO UNDERGROUND UTILITIES HAVE BEEN SHOWN OTHER THAN SURFACE EVIDENCE AND EASEMENT LOCATION SHOWN HEREON.



SCALE: 1" = 40'

*** PRELIMINARY ***

UPDATED 06.17.14
PREPARED 05.21.14

2 OF 2

A.L.T.A. SURVEY - EXISTING SITE

PREPARED FOR:

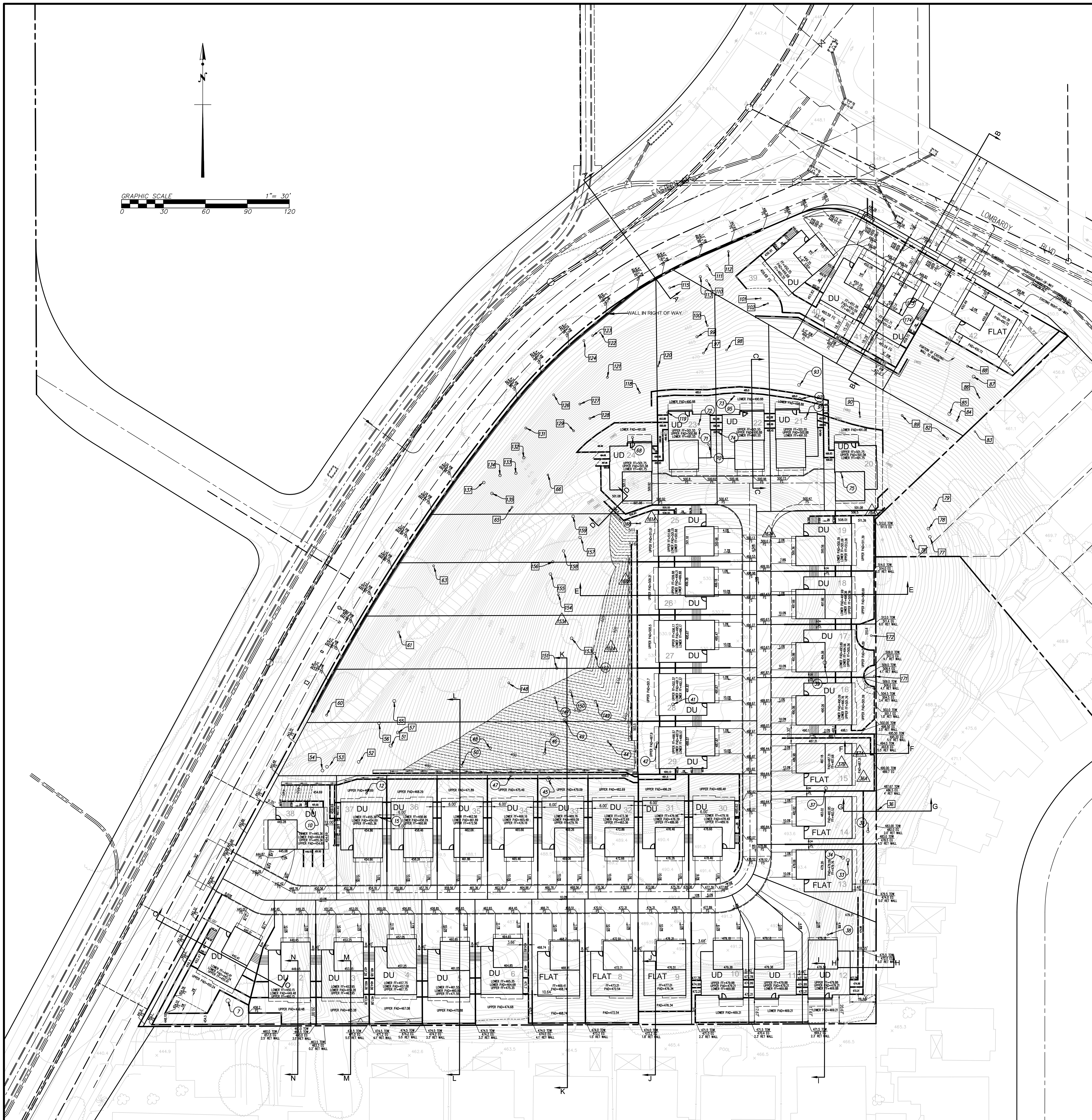
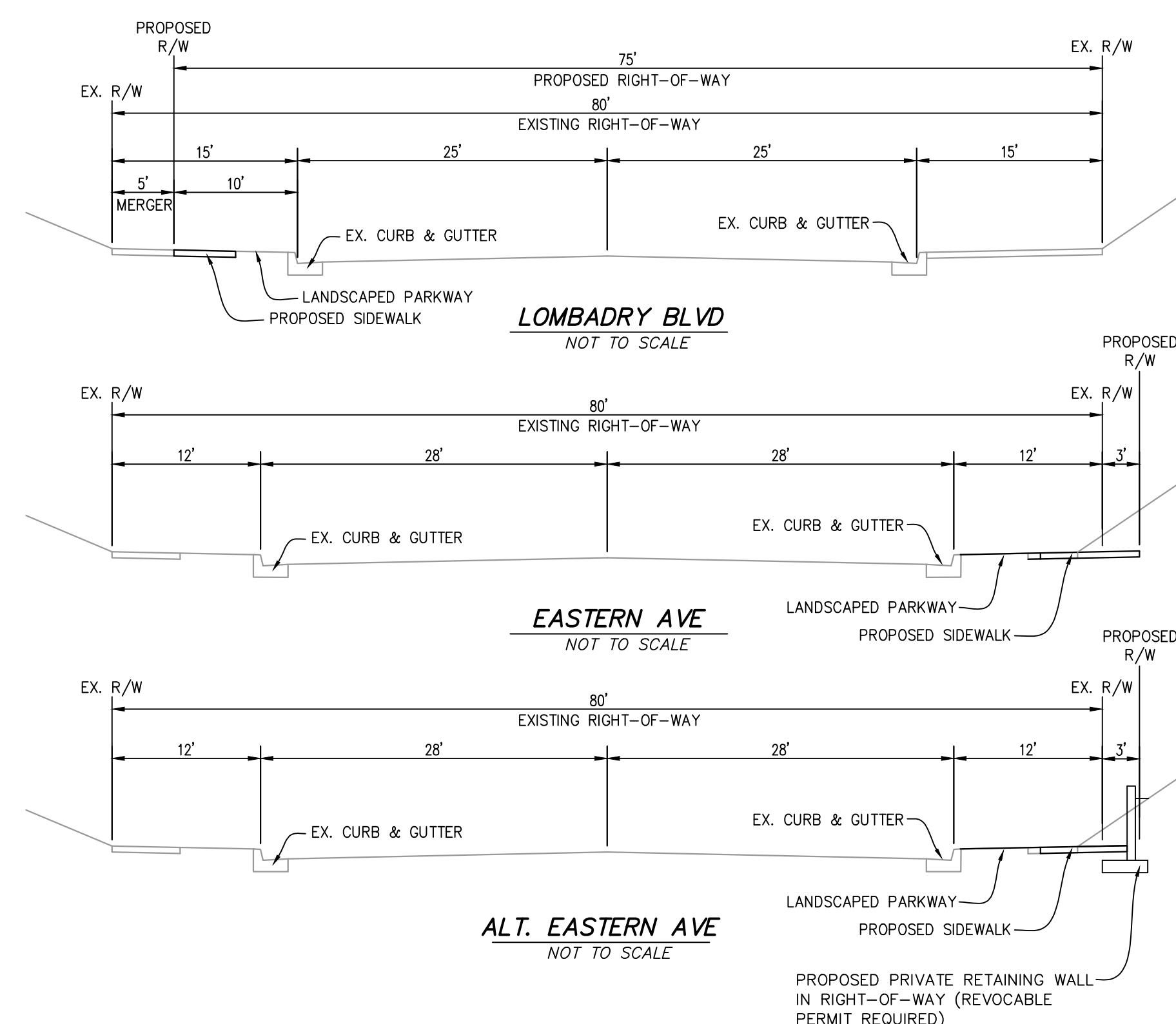
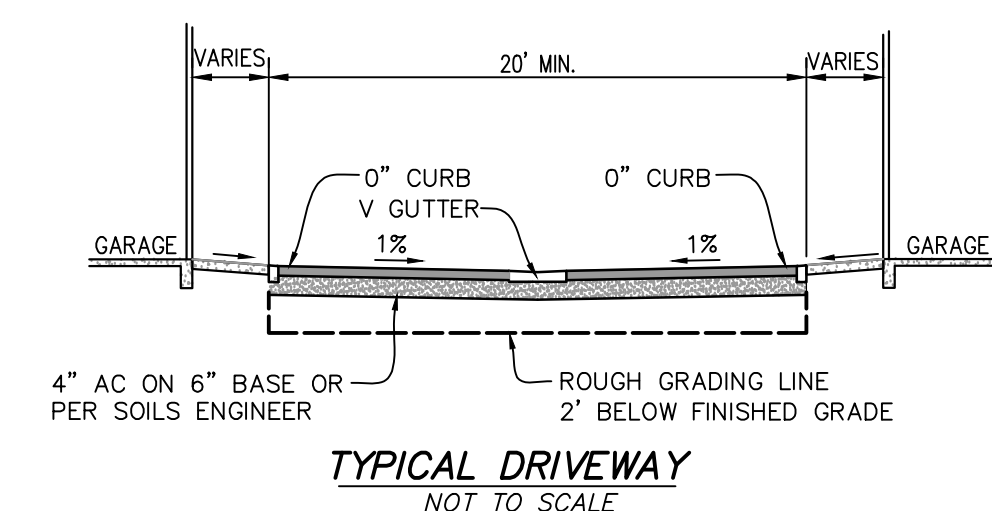
BANCOMER CONSTRUCTION and DEVELOPMENT, INC.,
a California corporation
and
CITY TERRACE, LLC,
a California limited liability company.

PREPARED BY:

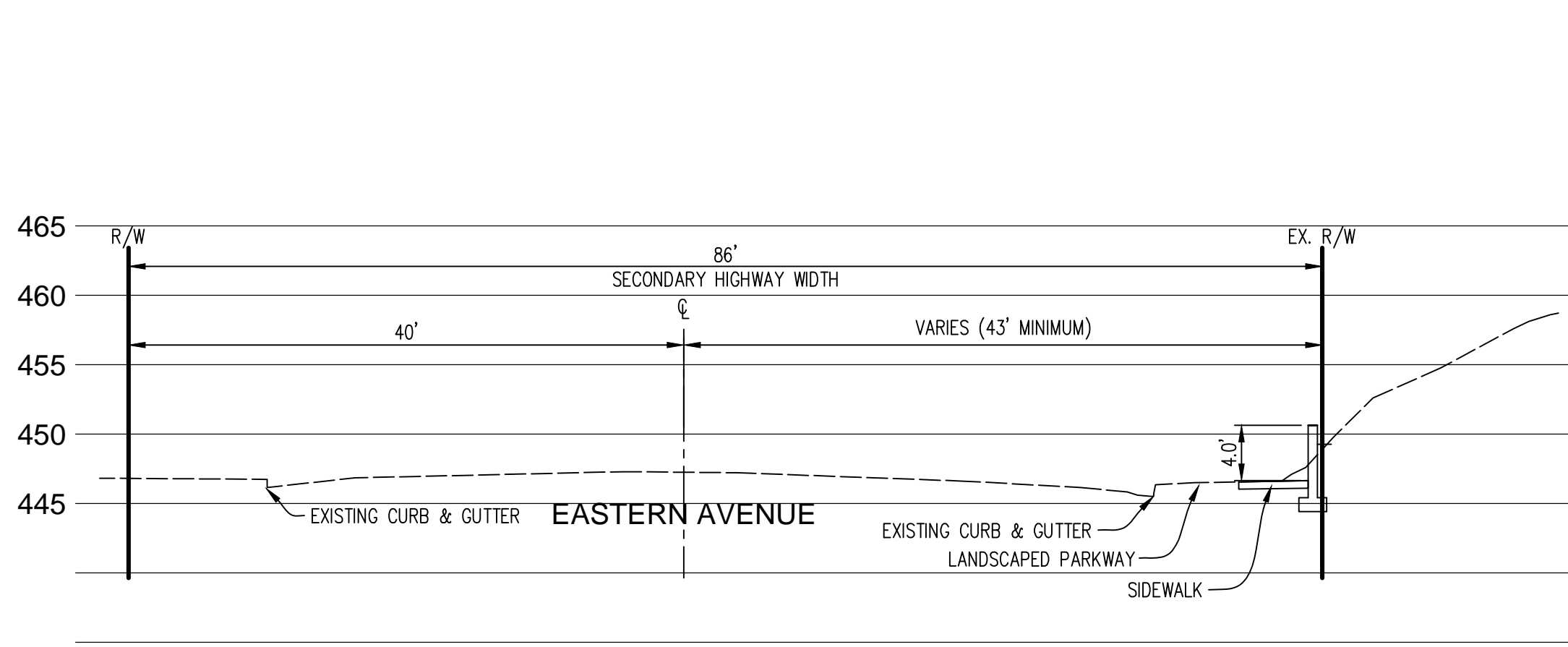
PHB & ASSOCIATES, INC.

(626) 914-6256 FAX: (626) 914-5756
1620 SOUTH GRAND AVENUE, GLENORA, CALIFORNIA 91740

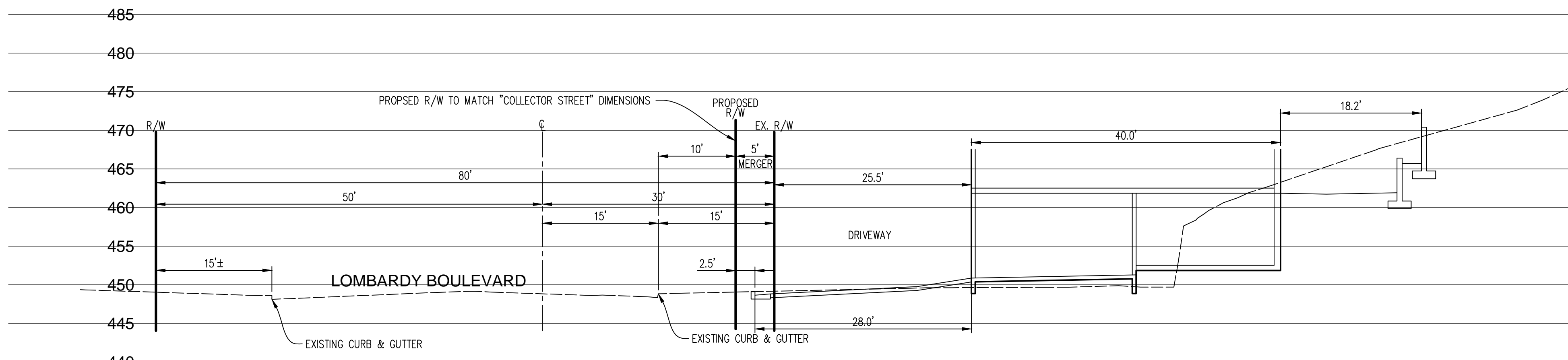
EXHIBIT A.1

[illegible]

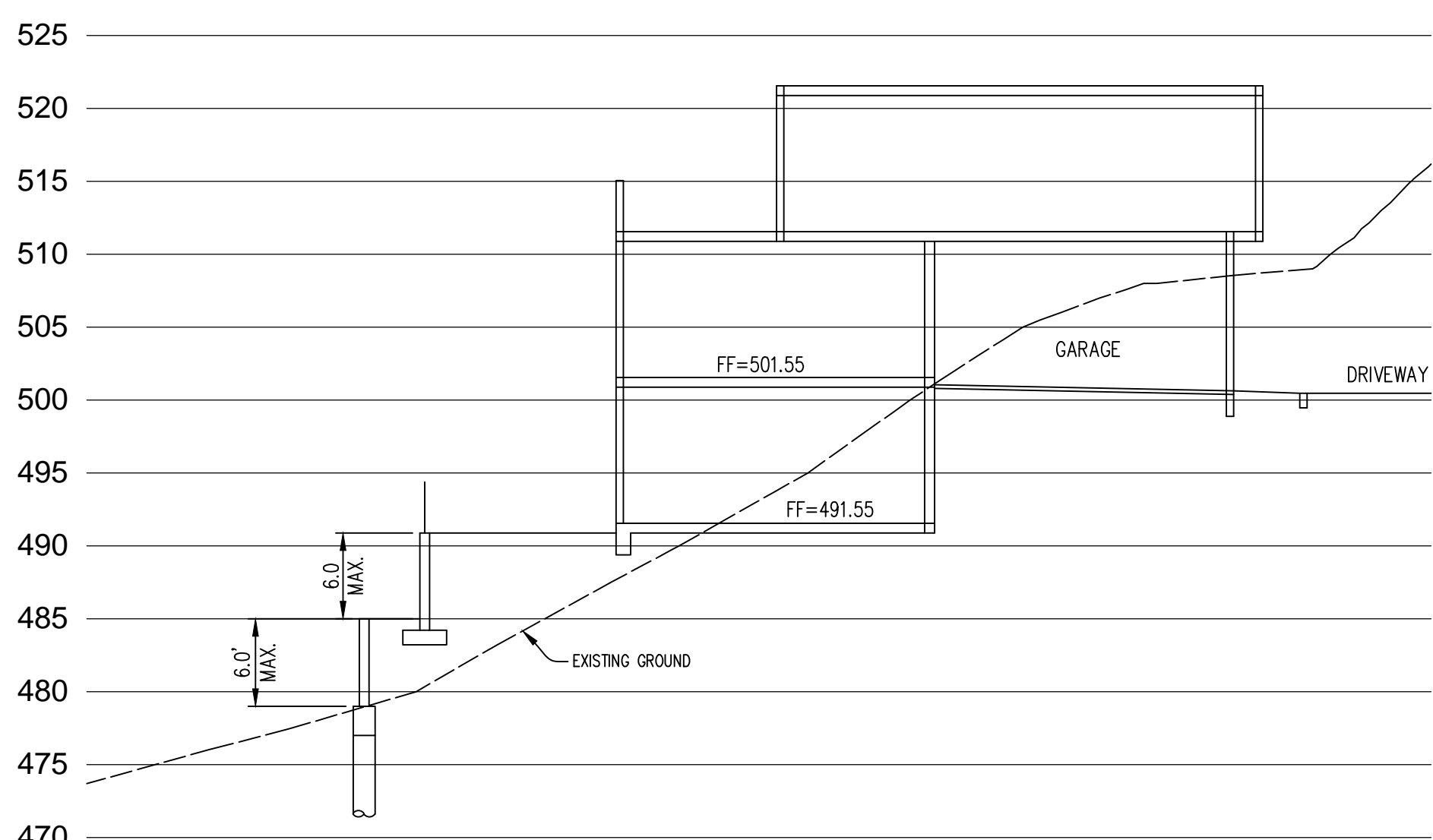
DESIGNER:	
W.M.W.	
CHECKED BY:	
A.B.	
DATE:	
7/10/2018	
SHEET	OF
1	



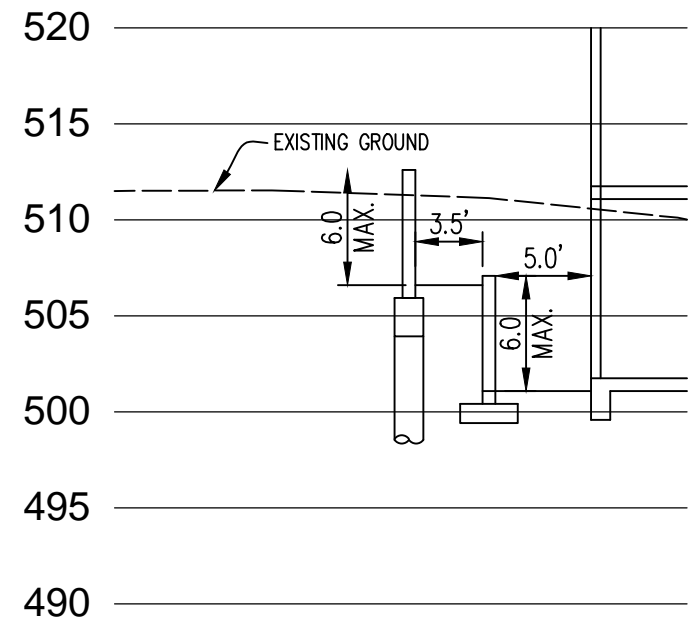
SECTION A-A
SCALE: 1"=10'



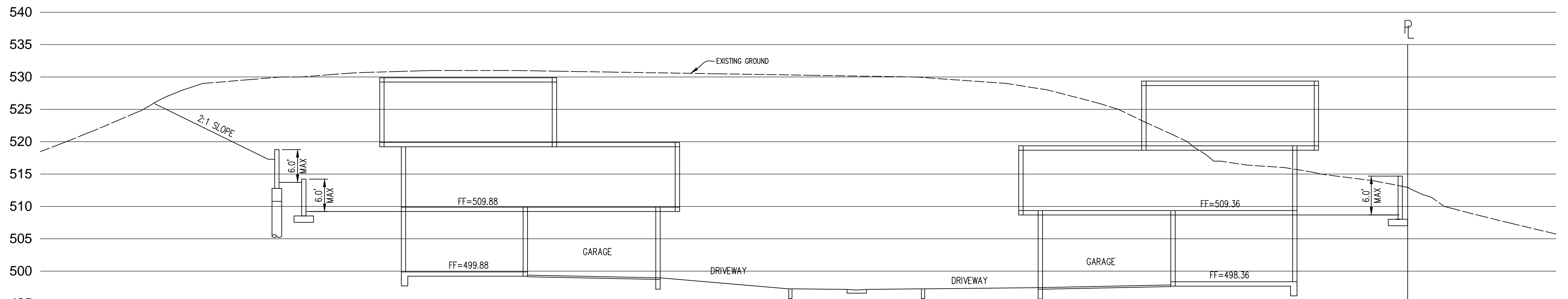
SECTION B-B
SCALE: 1"=10'



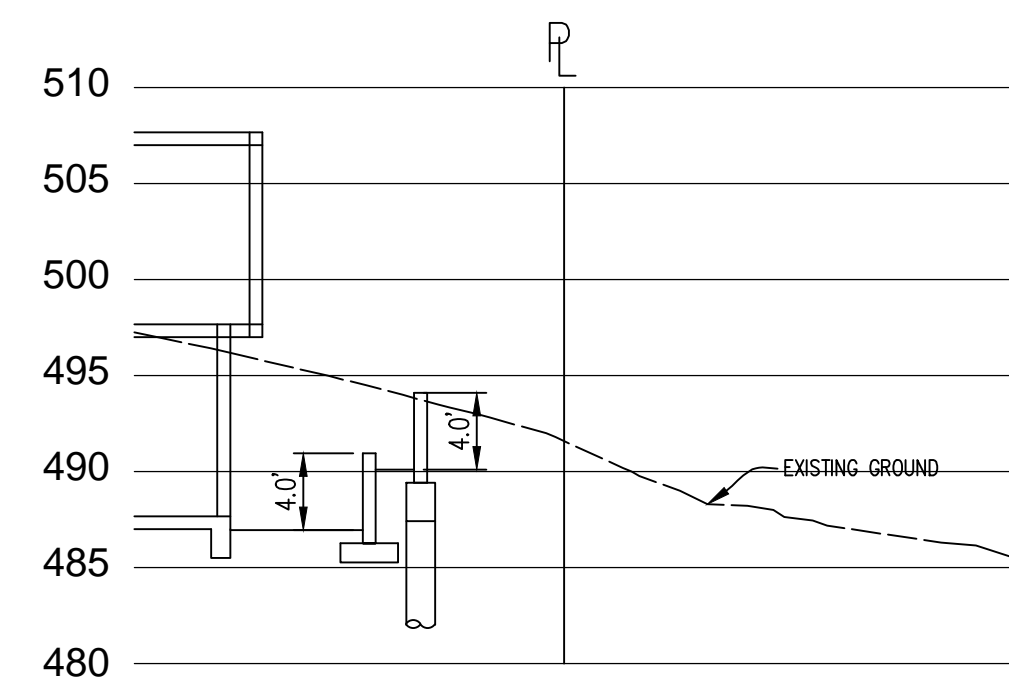
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SCALE: 1"=10'



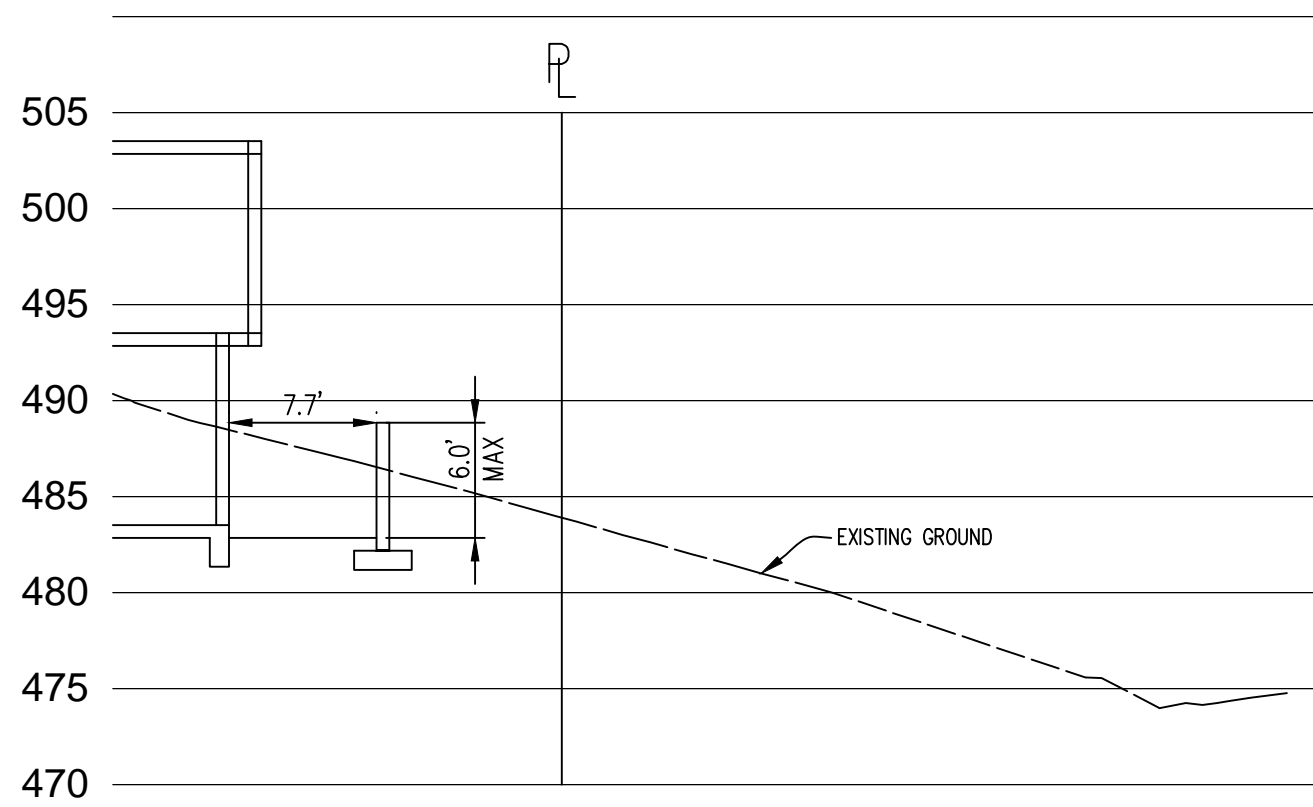
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SCALE: 1"=10'



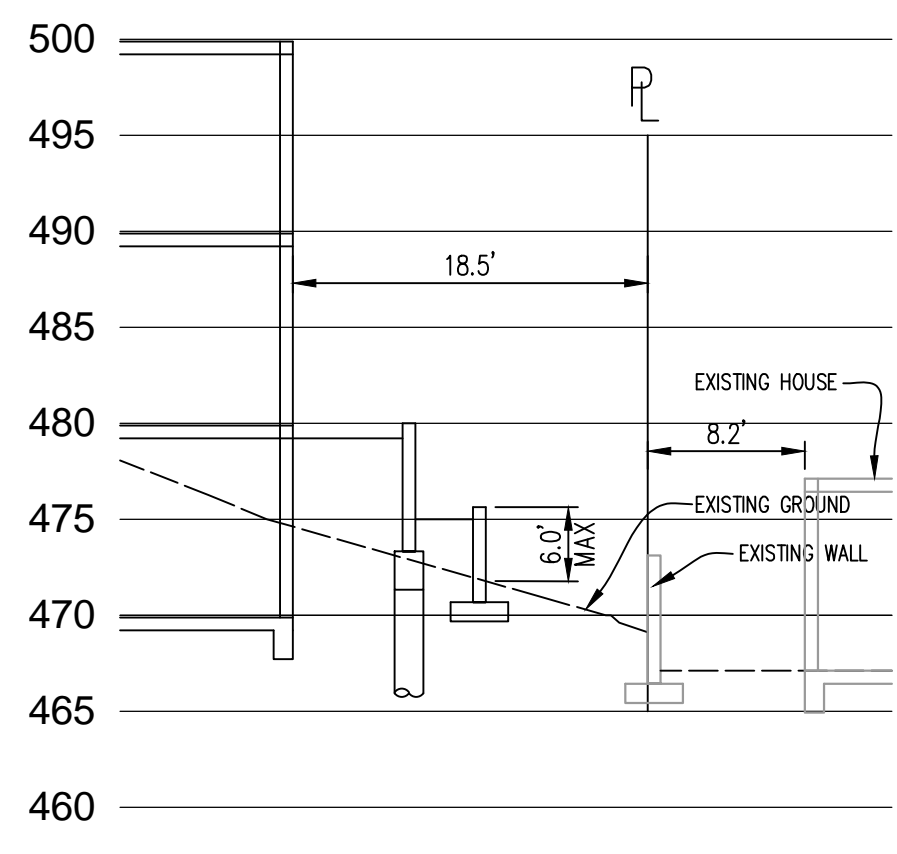
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SCALE: 1"=10'



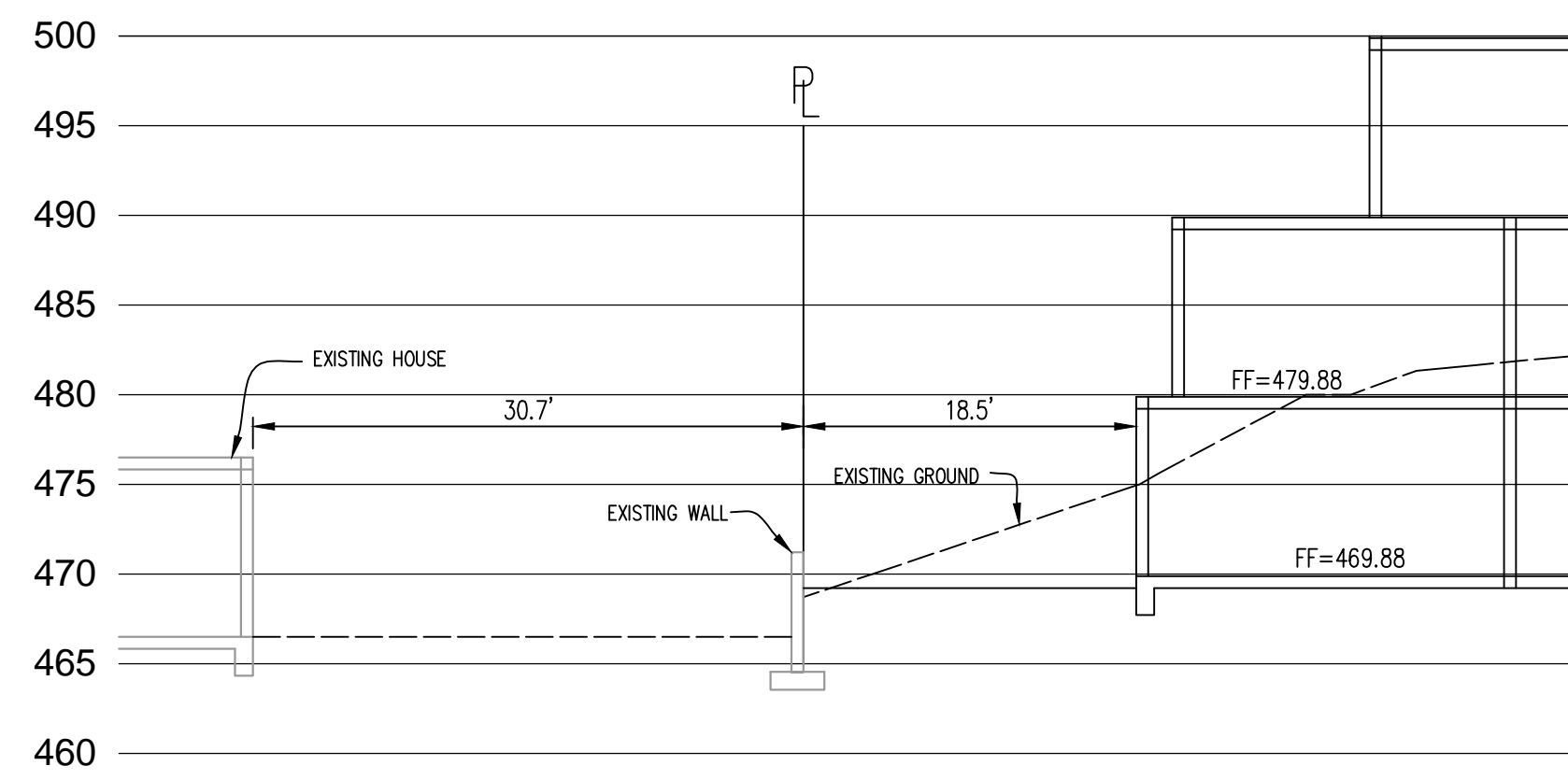
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SCALE: 1"=10'



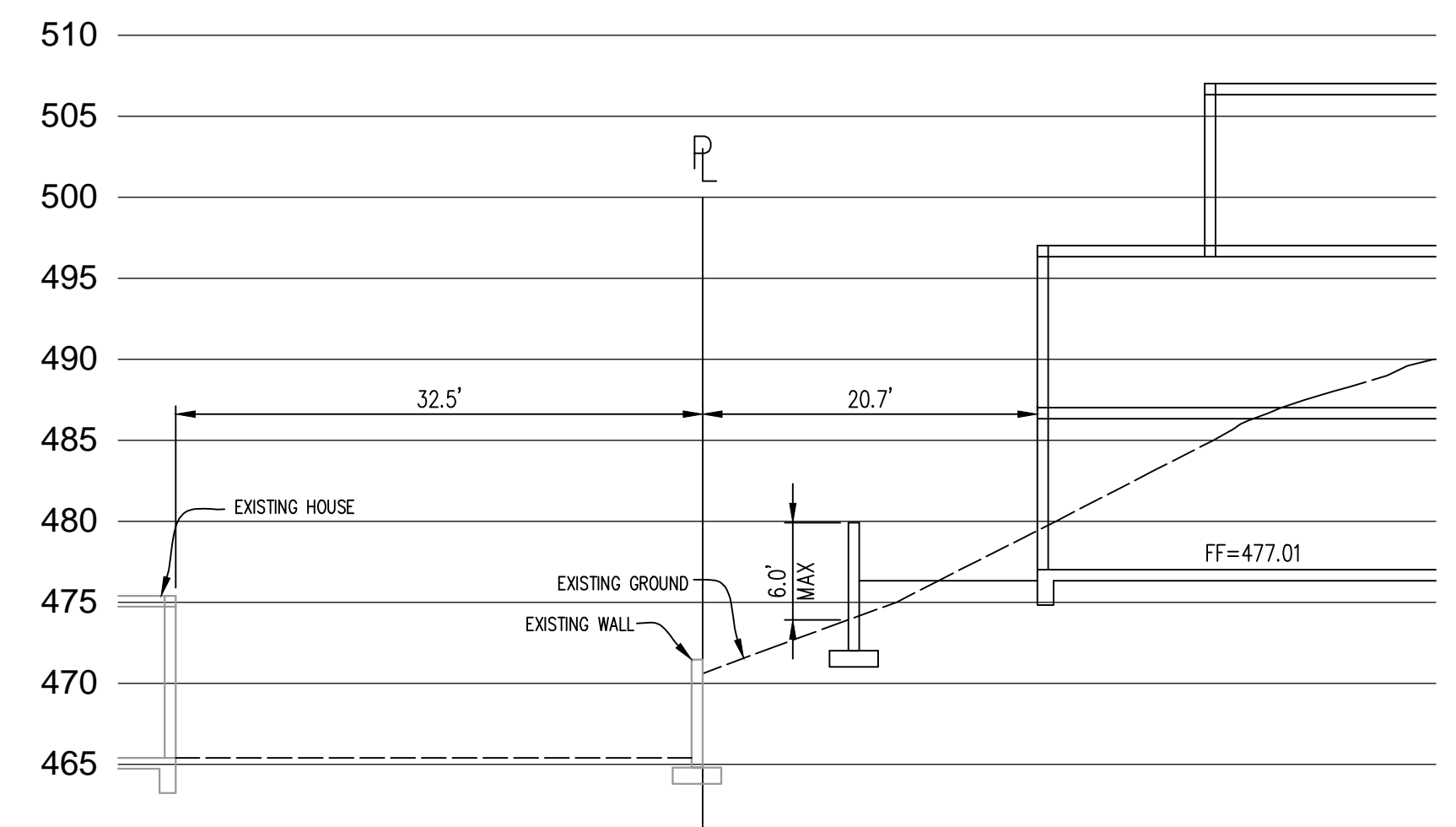
SECTION G-G
SCALE: 1"=10'



SECTION H-H
SCALE: 1"=10'



SECTION I-I
SCALE: 1"=10'



SECTION J-J
SCALE: 1"=10'

PREPARED FOR:

CLEARWATER COMMUNITIES, LLC
4685 MACARTHUR CT NEWPORT BEACH, CA 92660

GRADING & DRAINAGE PLAN
VTM TRACT 73531
2520 N. EASTERN AVE
LOS ANGELES, CA 90032

DEVELOPER'S ENGINEER:

FORMA ENGINEERING INC.

10814 Reseda Boulevard, Northridge, CA 91326
Phone: (818) 832-1710 • Fax: (818) 832-1740

William M. White
WILLIAM M. WHITE

R.C.E. 62111

7/10/2018
DATE



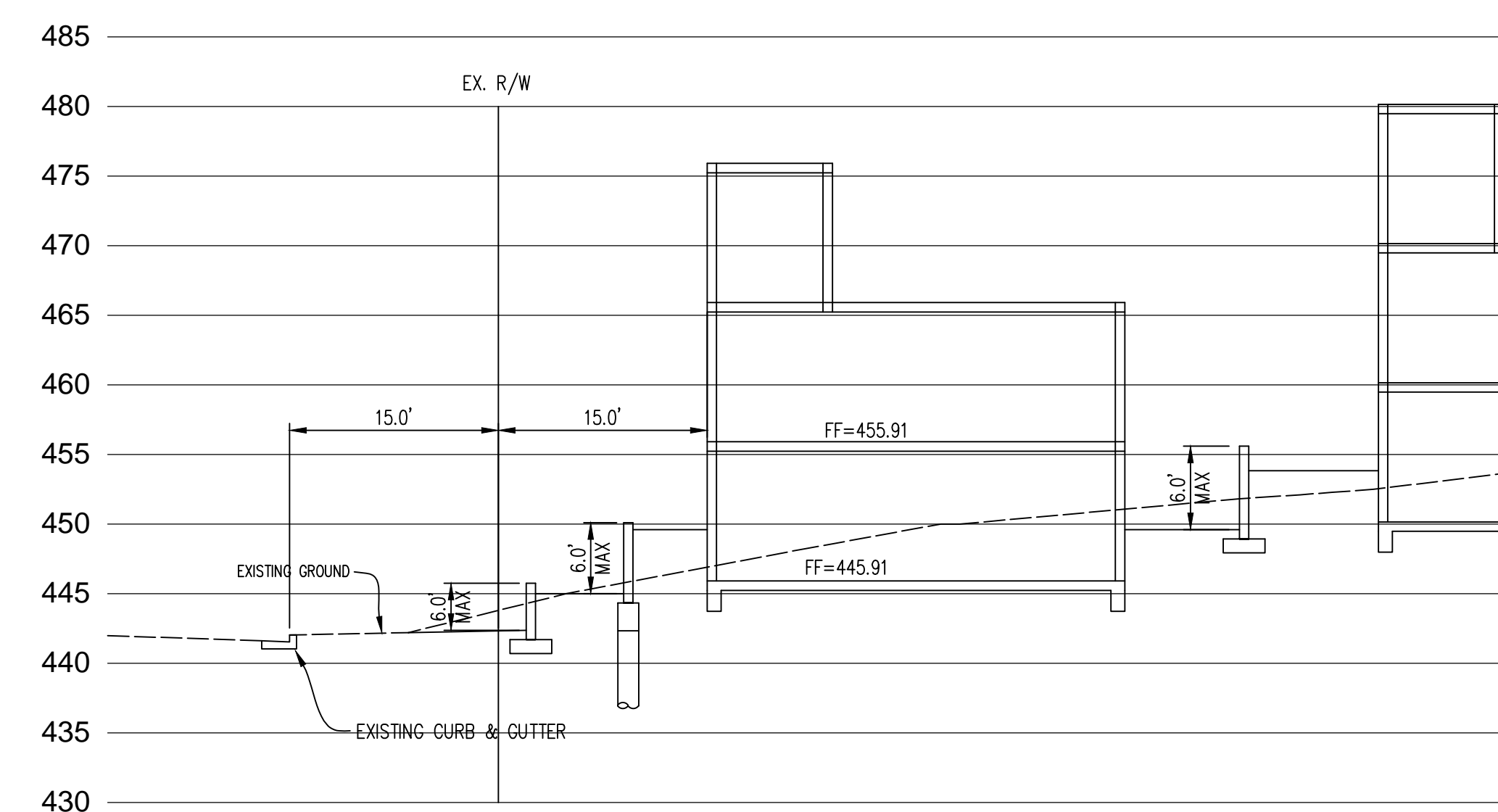
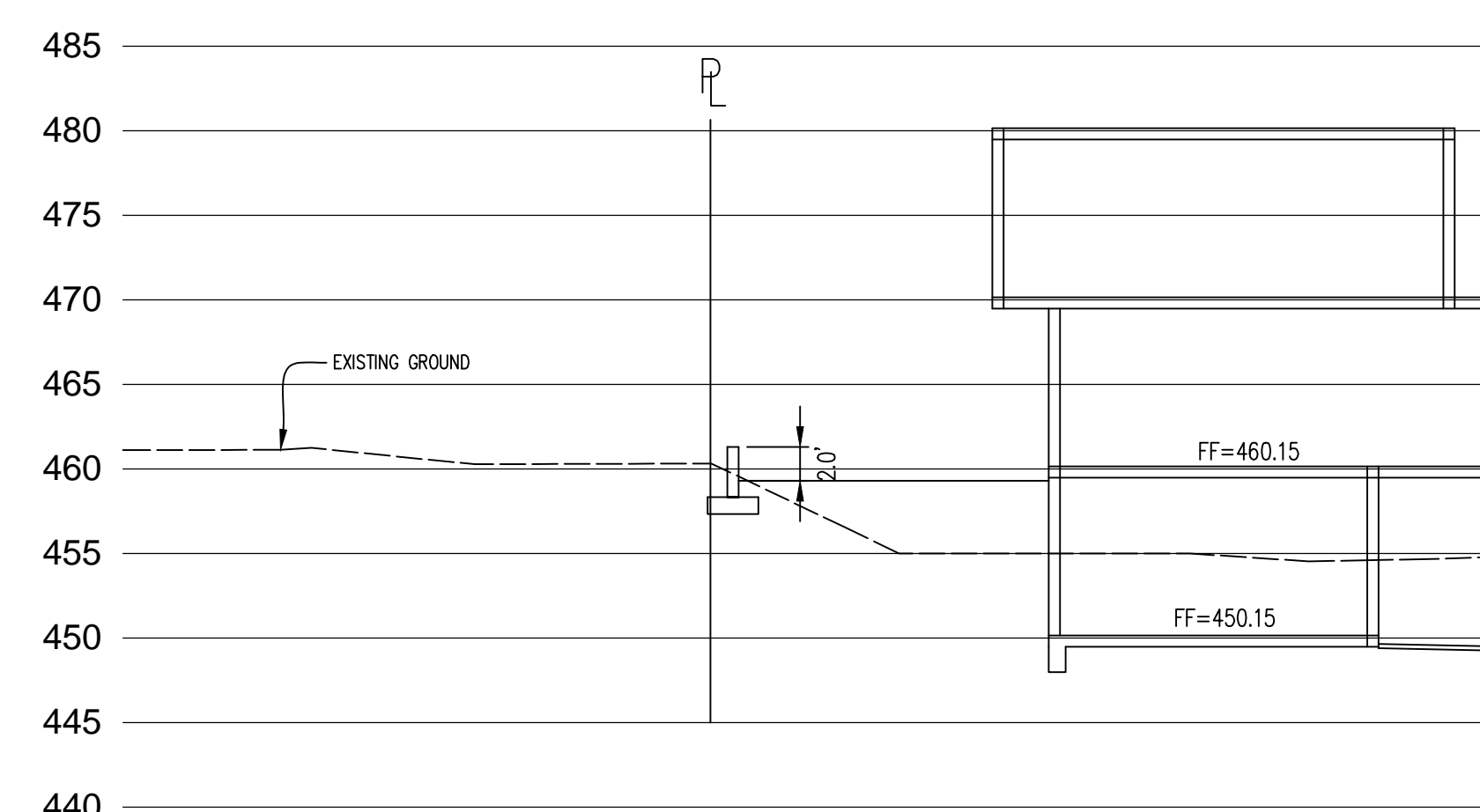
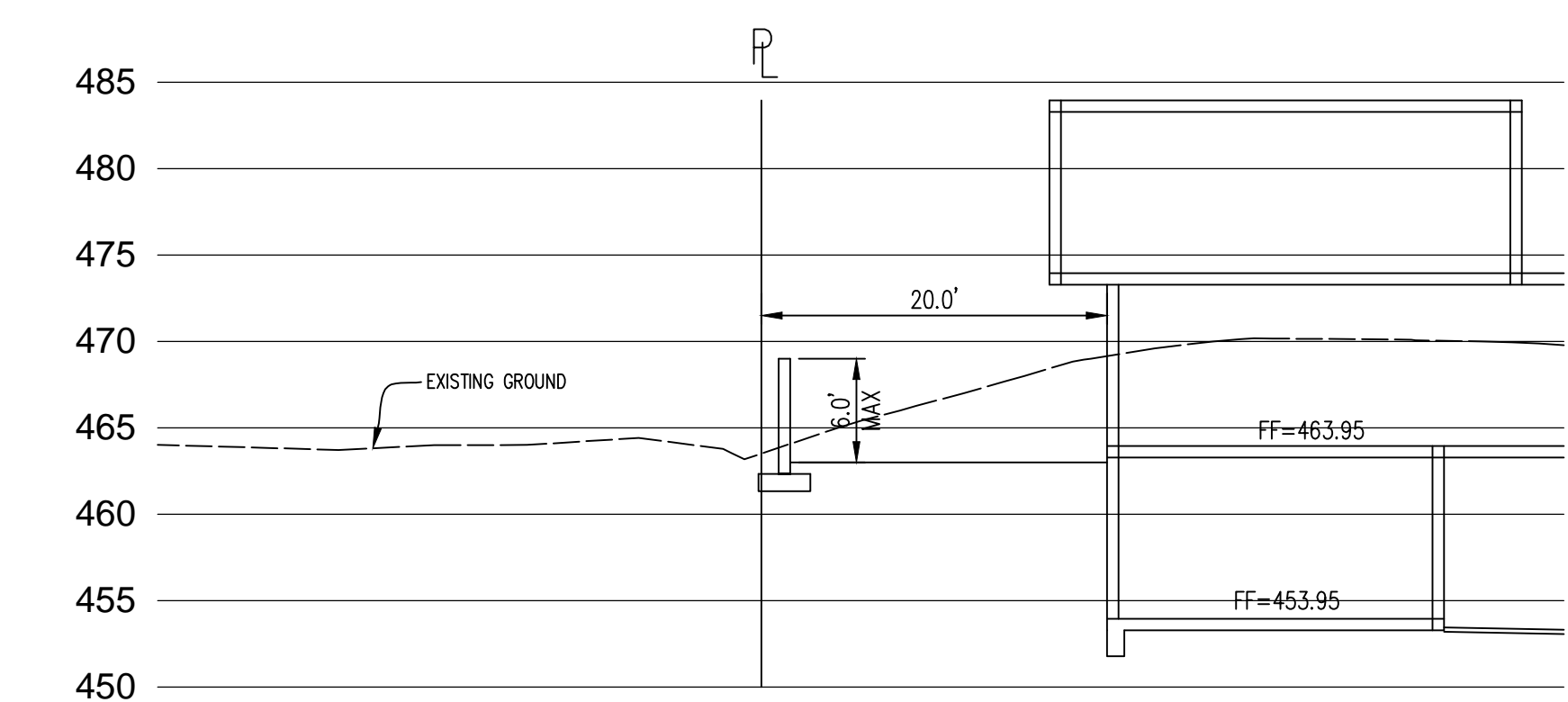
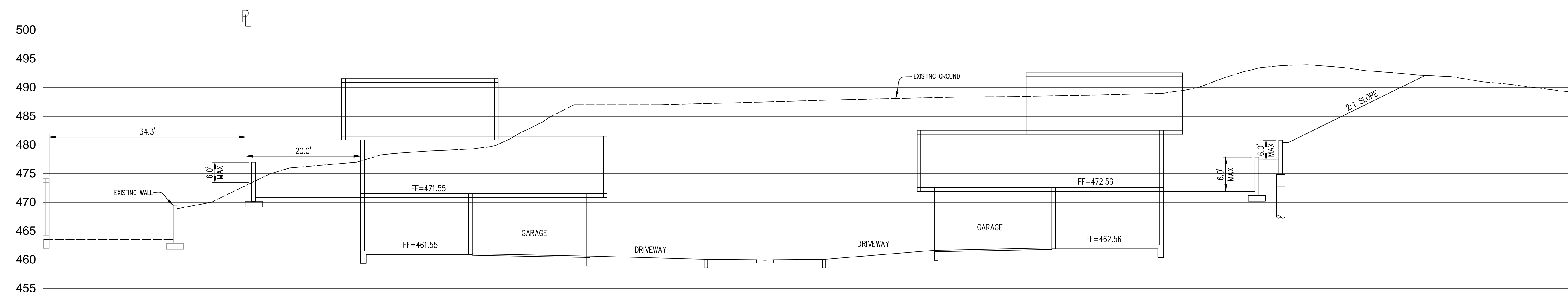
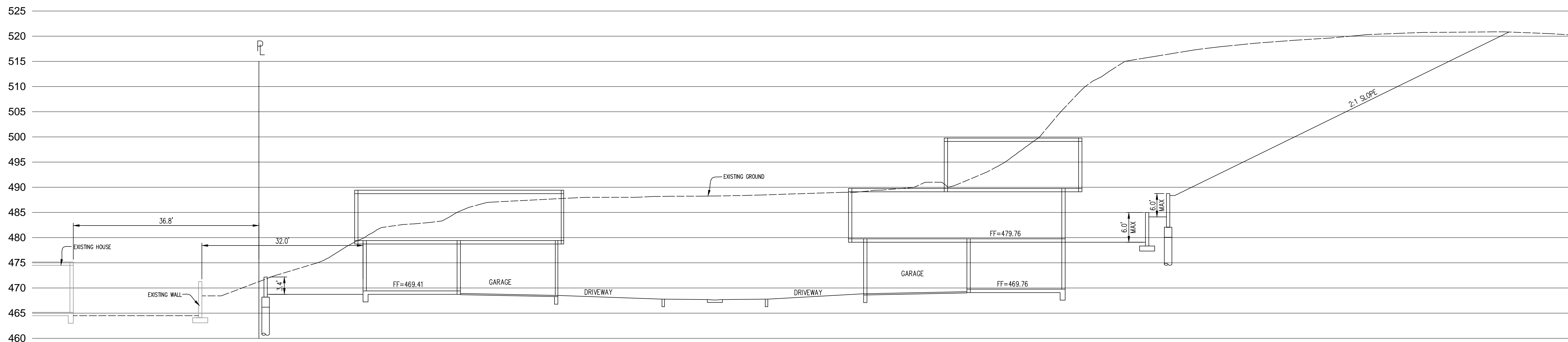
No.	DATE	REVISION

DESIGNER:
W.M.W.

CHECKED BY:
A.B.

DATE:
7/10/2018

SHEET 2 OF 3



PREPARED FOR:

CLEARWATER COMMUNITIES, LLC
4685 MACARTHUR CT NEWPORT BEACH, CA 92660

GRADING & DRAINAGE PLAN
VTM TRACT 73531
2520 N. EASTERN AVE
LOS ANGELES, CA 90032

DEVELOPER'S ENGINEER:

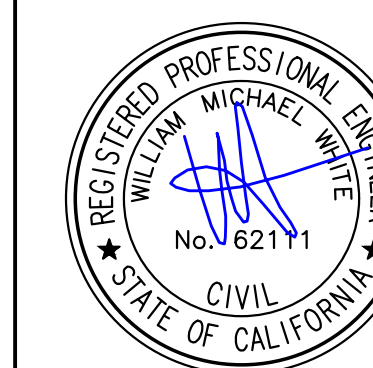
FORMA ENGINEERING INC.

10814 Reseda Boulevard, Northridge, CA 91326
Phone: (818) 832-1710 • Fax: (818) 832-1740

William M. White
WILLIAM M. WHITE

R.C.E. 62111

7/10/2018
DATE



No.	DATE	REVISION

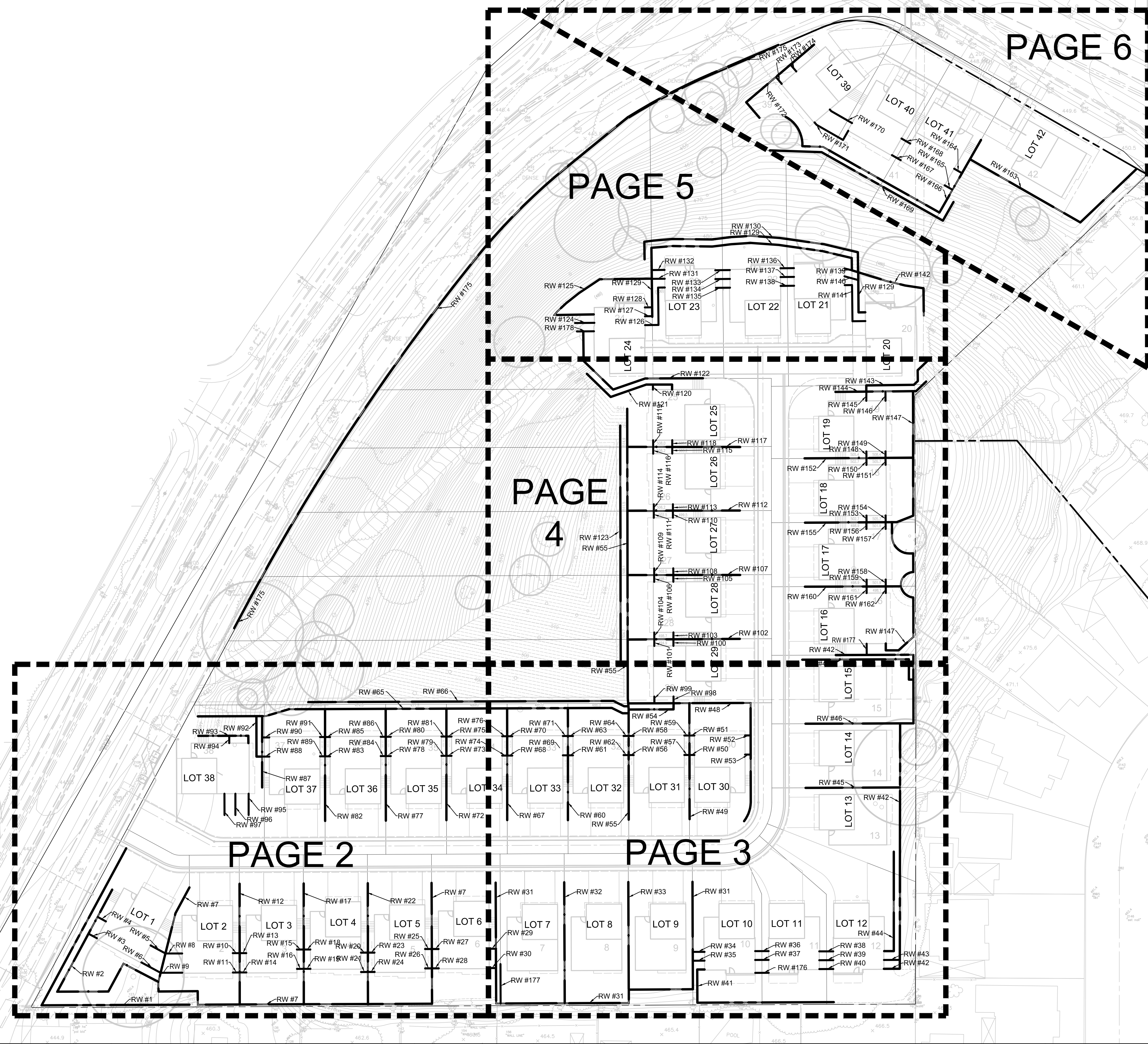
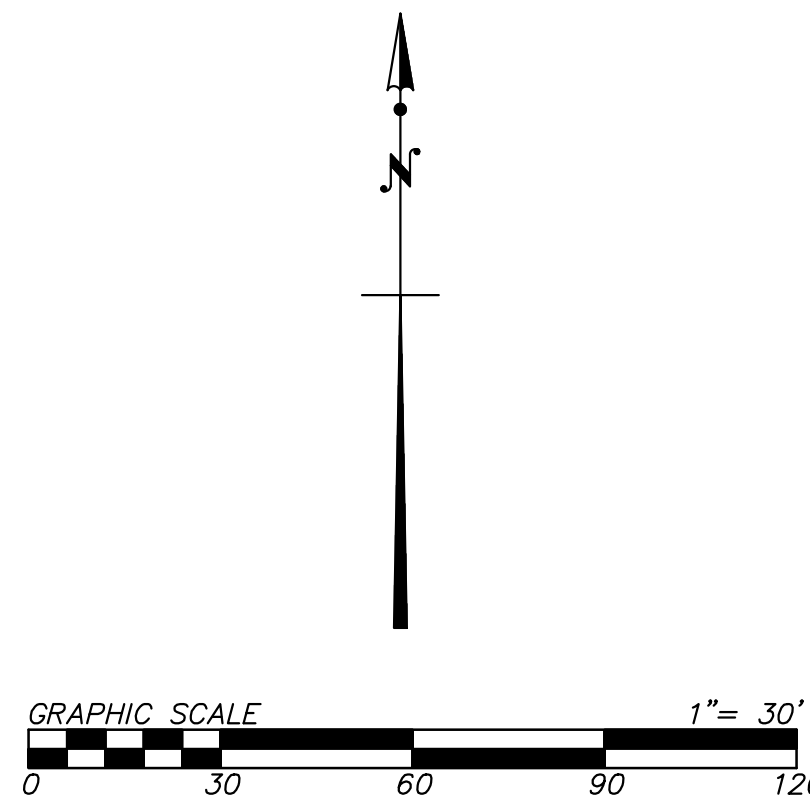
DESIGNER:
W.M.W.

CHECKED BY:
A.B.

DATE: 7/10/2018

SHEET 3 OF 3

SEE SHEET 7 FOR WALL DESCRIPTIONS



PREPARED FOR:

TTLIC LOS ANGELES – EL SERENO, LLC
2372 MORSE AVE, SUITE 618 IRVINE, CA 92614

EXHIBIT A–RETAINING WALLS
TRACT 73531
2520 N. EASTERN AVE
LOS ANGELES, CA 90032

DEVELOPER’S ENGINEER:

FORMA ENGINEERING INC.

400 San Fernando Mission Boulevard, Suite 200
San Fernando, CA 91340
Phone: (818) 832–1710 • Fax: (818) 832–1740

No.	DATE	REVISION

DESIGNER:

J.H.

CHECKED BY:

W.M.W.

DATE:

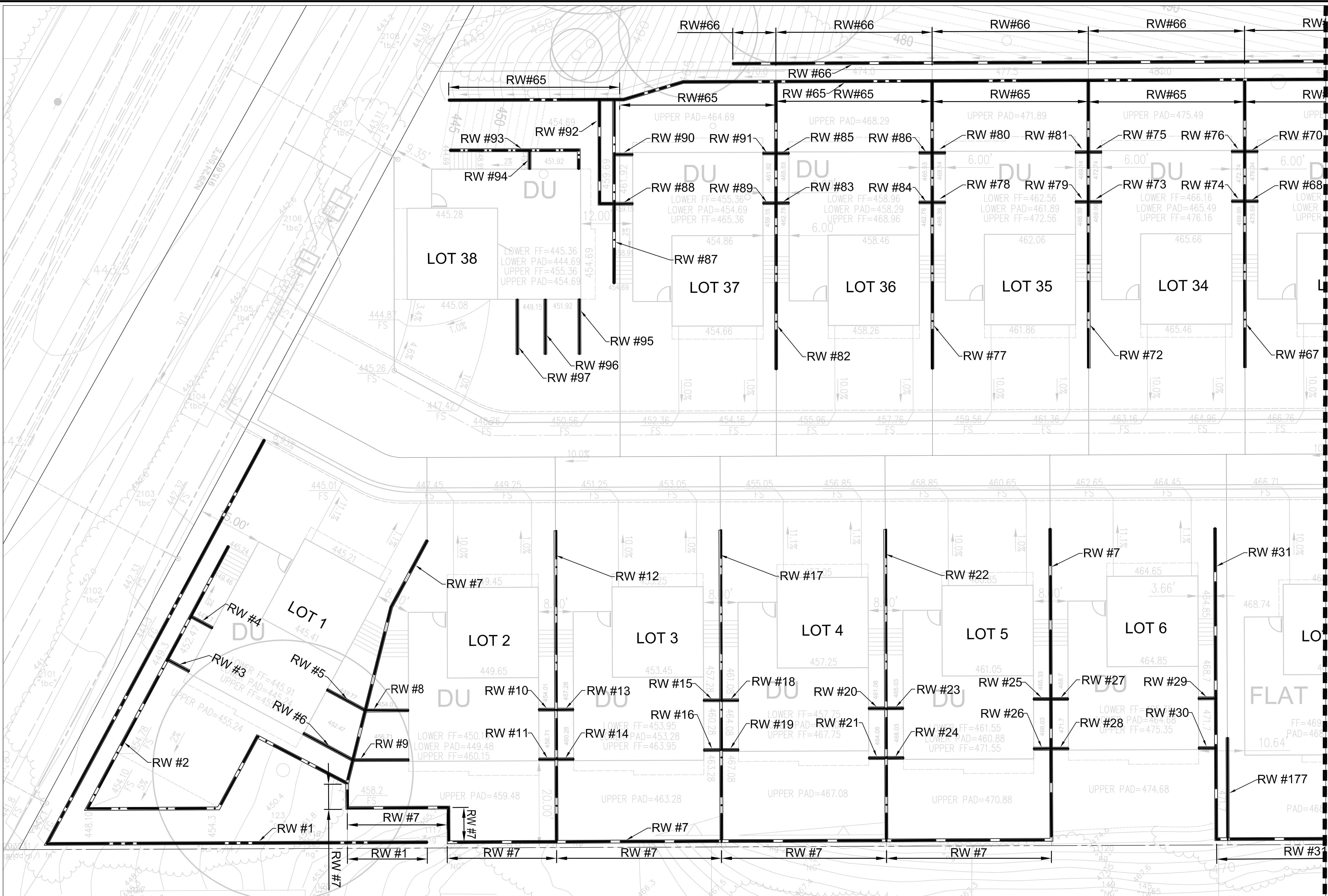
8/09/2019

SHEET

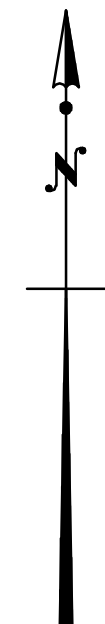
OF

1

7



MATCHLINE - SEE PAGE 3



SEE SHEET 7 FOR WALL DESCRIPTIONS

PREPARED FOR:

TTLIC LOS ANGELES – EL SERENO, LLC
2372 MORSE AVE, SUITE 618 IRVINE, CA 92614

EXHIBIT A–RETAINING WALLS
TRACT 73531
2520 N. EASTERN AVE
LOS ANGELES, CA 90032

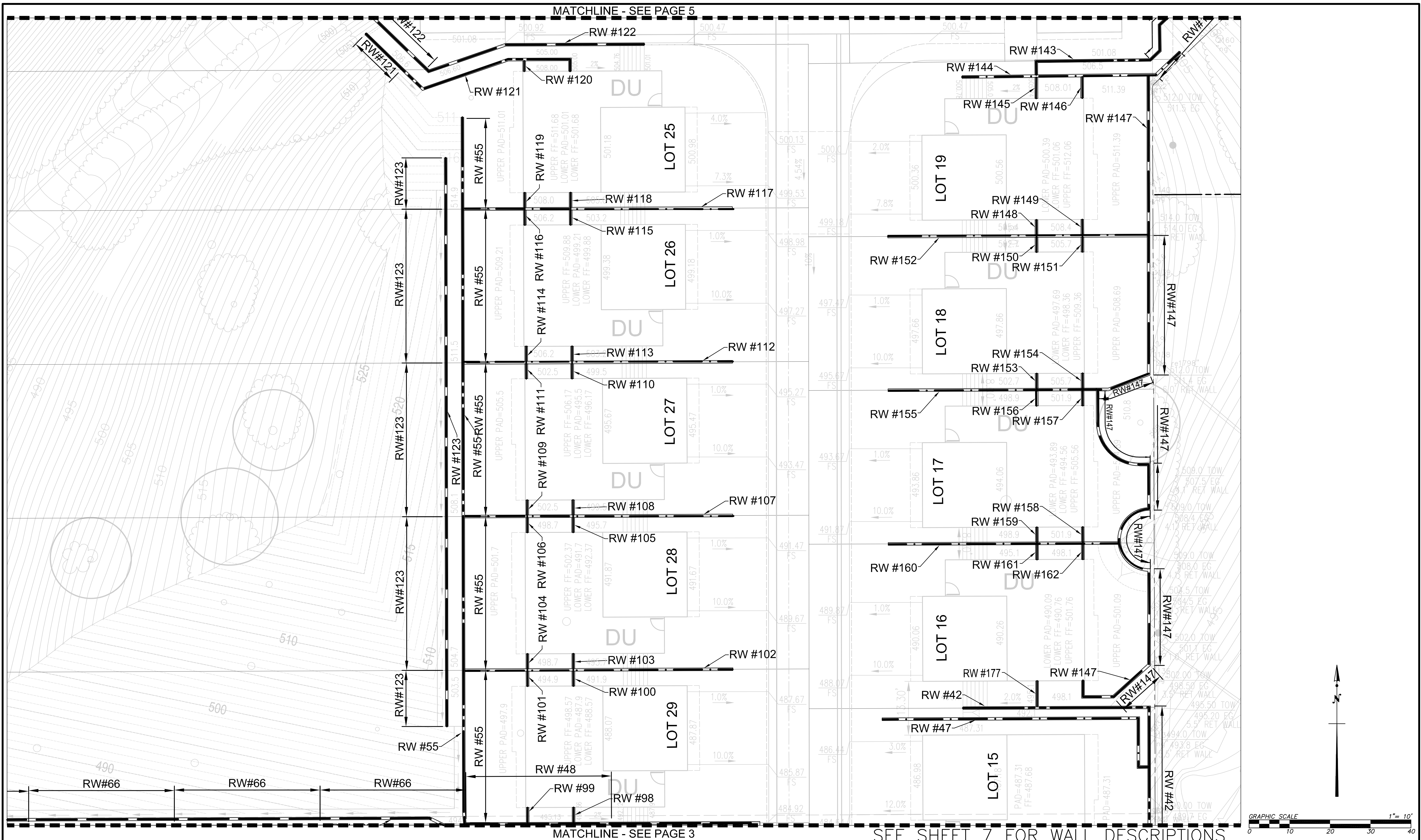
DEVELOPER'S ENGINEER:

FORMA ENGINEERING INC.

400 San Fernando Mission Boulevard, Suite 200
San Fernando, CA 91340
Phone: (818) 832-1710 • Fax: (818) 832-1740

No.	DATE	REVISION

DESIGNER:	J.H.
CHECKED BY:	W.M.W.
DATE:	8/09/2019
SHEET	OF
2	7



PREPARED FOR:

TTLIC LOS ANGELES – EL SERENO, LLC
2372 MORSE AVE, SUITE 618 IRVINE, CA 92614

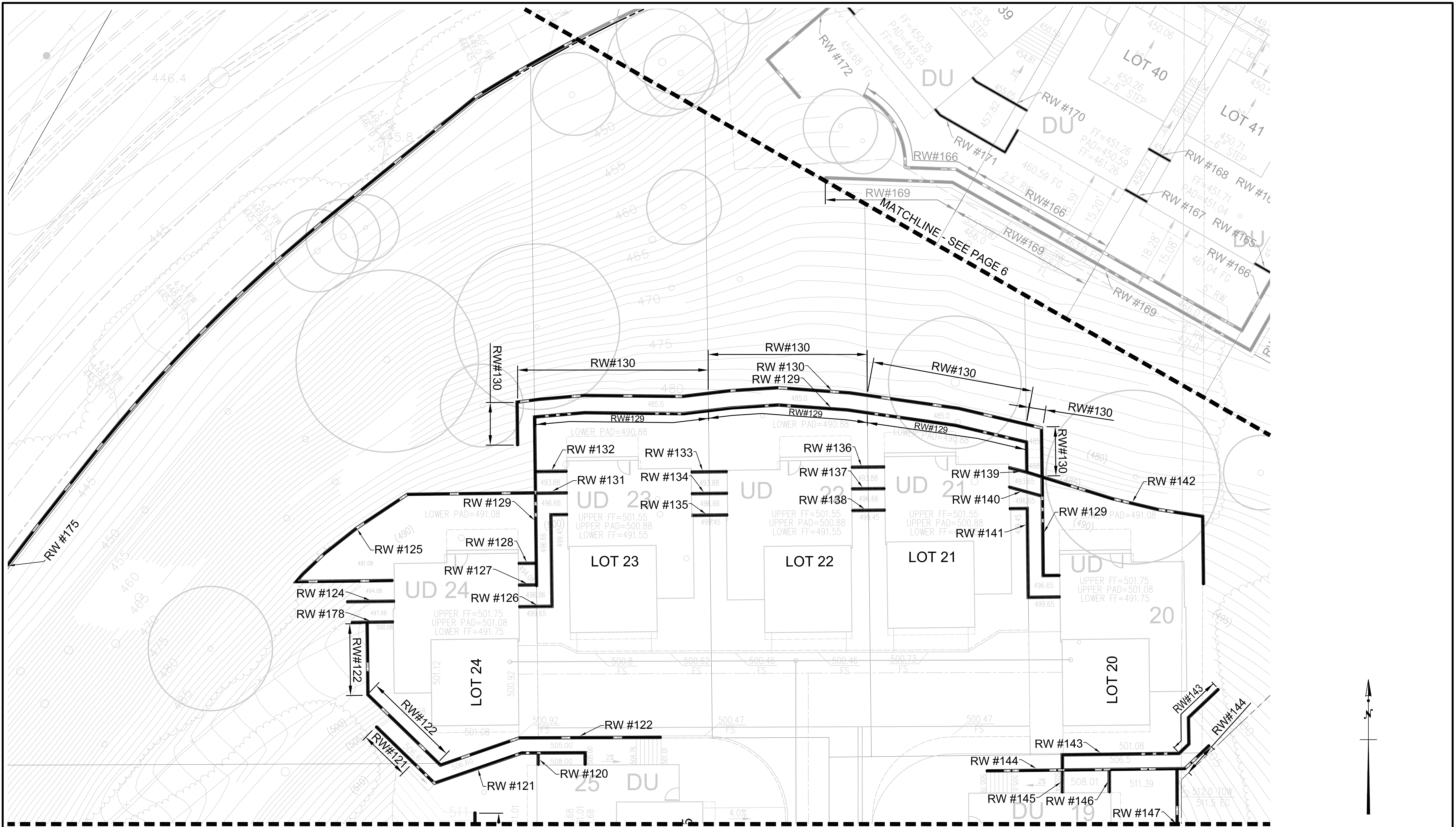
EXHIBIT A—RETAINING WALLS
TRACT 73531
2520 N. EASTERN AVE
LOS ANGELES, CA 90032

DEVELOPER'S ENGINEER:

FORMA ENGINEERING INC.

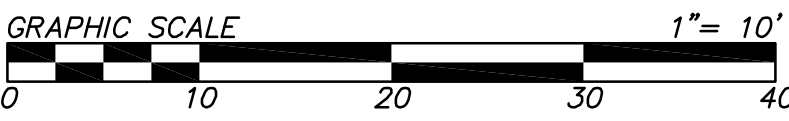
400 San Fernando Mission Boulevard, Suite 200
San Fernando, CA 91340
Phone: (818) 832-1710 • Fax: (818) 832-1740

No.	DATE	REVISION	DESIGNER: J.H.	
			CHECKED BY: W.M.W.	
			DATE: 8/09/2019	
			SHEET 4	OF 7



MATCHLINE - SEE PAGE 4

SEE SHEET 7 FOR WALL DESCRIPTIONS



PREPARED FOR: TTLIC LOS ANGELES — EL SERENO, LLC 2372 MORSE AVE, SUITE 618 IRVINE, CA 92614	EXHIBIT A—RETAINING WALLS TRACT 73531 2520 N. EASTERN AVE LOS ANGELES, CA 90032	DEVELOPER'S ENGINEER: <div>FORMA ENGINEERING INC.</div> 400 San Fernando Mission Boulevard, Suite 200 San Fernando, CA 91340 Phone: (818) 832-1710 • Fax: (818) 832-1740		No.	DATE	REVISION	DESIGNER: J.H.	
							CHECKED BY: W.M.W.	
							DATE: 8/09/2019	
							SHEET	OF
							5	7

Lot #	Wall #	Length	Max Ret. Height	NOTES
1	1	175	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	2	142	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE.
	3	6	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	4	6	3.0	
	5	11	3.0	
	6	13	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
2	1	19	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	7	122	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE. RETAINING CONDITION VARIES
	8	10	3.0	
	9	13	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	10	4	3.0	
	11	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
3	7	38	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE. RETAINING CONDITION VARIES
	12	72	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	13	4	3.0	
	14	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	15	4	3.0	
	16	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
4	7	38	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	17	72	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	18	4	3.0	
	19	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	20	4	3.0	
	21	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
5	7	38	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	22	72	5.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	23	4	3.0	
	24	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	25	4	3.0	
	26	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
6	7	72	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	27	4	3.0	
	28	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	29	4	3.0	
	30	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
7	31	72	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE. RETAINING CONDITION VARIES
	177	24	3.0	
8	31	46	2.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	32	72	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
9	31	61	5.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	33	64	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
10	31	42	5.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	34	7	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	35	7	3.0	
	36	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	37	4	3.0	
	176	4	3.0	
	41	63	5.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
11	36	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	37	4	3.0	
	176	4	3.0	
	38	5	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	39	5	3.0	
	40	5	3.0	
	41	39	2.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
12	38	4	3.0	
	39	4	3.0	
	40	4	3.0	
	41	5	2.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	42	65	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	43	10	3.0	
	44	52	5.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
13	42	53	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	44	13	5.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
14	42	38	5.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	45	56	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
15	42	49	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	46	57	5.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	47	78	5.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
16	42	46	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	147	58	5.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	177	7	3.0	
	161	4	3.0	
	162	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
17	147	51	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	156	4	3.0	
	157	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	158	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	159	4	3.0	
	160	57	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
18	147	46	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	150	4	3.0	
	151	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	153	4	3.0	
	154	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	155	52	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
19	143	29	6.0	
	144	54	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	145	6	3.0	
	146	6	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	147	40	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	148	4	3.0	
	149	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	152	65	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
20	129	32	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	130	16	6.0	
	140	4	3.0	

	141	8	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	142	57	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	143	20	6.0	
	144	3	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
21	129	47	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	130	40	6.0	
	136	4	3.0	
	137	4	3.0	
	138	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	139	5	3.0	
	140	5	3.0	
	141	26	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
22	129	39	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	130	39	6.0	
	133	5	3.0	
	134	5	3.0	
	135	5	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	136	4	3.0	
	137	4	3.0	
	138	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
23	126	30	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	129	43	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	130	43	6.0	
	131	8	3.0	
	132	8	3.0	
	133	5	3.0	
	134	5	3.0	
	135	5	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	175	13	6.0	
24	121	13	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	122	42	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	178	11	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	124	12	3.0	
	125	89	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	126	5	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	127	4	3.0	
	128	4	3.0	
	129	41	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	130	15	6.0	
	175	236	6.0	
25	55	23	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	117	67	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	118	4	3.0	
	119	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	120	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	121	48	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	122	55	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	123	13	6.0	
	175	42	6.0	
26	55	38	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	112	67	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	113	4	3.0	
	114	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	115	4	3.0	
	116	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	123	38	6.0	
	175	45	6.0	
27	55	38	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	107	4	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	108	4	3.0	
	109	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	110	4	3.0	
	111	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	123	38	6.0	
	175	44	6.0	
28	55	38	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	102	67	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	103	4	3.0	
	104	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	105	4	3.0	
	106	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	123	38	6.0	
	175	35	6.0	
29	48	36	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	55	38	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	90	4	3.0	
	90	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	100	4	3.0	
	101	4	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	123	14	6.0	
30	48	107	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	49	69	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	50	3	3.0	
	51	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	52	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	53	3	3.0	
31	54	30	6.0	
	55	69	4.0	
	56	3	3.0	
	57	3	3.0	
	58	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	59	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
32	60	66	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	61	3	3.0	
	62	3	3.0	
	63	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	64	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	65	37	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	66	37	6.0	
33	65	36	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	66	36	6.0	
	67	66	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	68	3	3.0	
	69	3	3.0	
	70	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	71	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
34	65	36	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	66	36	6.0	
	72	66	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	73	3	3.0	
	74	3	3.0	
	75	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	76	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
35	65	36	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	66	36	6.0	
	77	66	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	78	3	3.0	

	79	3	3.0	
	80	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	81	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
36	65	36	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	66	36	6.0	
	82	66	4.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	83	3	3.0	
	84	3	3.0	
	85	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	86	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
37	65	37	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	66	10	6.0	
	88	3	3.0	
	89	3	3.0	
	90	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	91	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
38	65	40	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	87	42	5.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	88	1	3.0	
	90	1	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	92	27	5.0	
	93	34	5.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	94	4	3.0	
	95	13	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	96	13	3.0	
	97	13	4.0	
39	166	39	6.0	
	169	35	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	170	13	3.0	
	171	17	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	172	65	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	173	5	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	174	5	3.0	
	175	72	6.0	
40	166	36	4.0	
	167	3	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	169	3	3.0	
	169	36	6.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
	170	3	3.0	
	171	10	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE
41	164	5	3.0	
	165	5	3.0	PLUS UP TO 6.0' OF SAFETY/PRIVACY FENCE</

EXHIBIT B

ORDINANCE NO. 180403

An ordinance amending Section 12.04 of the Los Angeles Municipal Code by amending the zoning map.

THE PEOPLE OF THE CITY OF LOS ANGELES DO ORDAIN AS FOLLOWS:

SECTION 1. Section 12.04 of the Los Angeles Municipal Code is hereby amended by changing the zone classifications on properties shown upon portions of the zoning maps titled "Zone Change Ordinance Map Northeast Los Angeles Hillside Zone Change" and the table for Section 1 below and incorporated herein by this reference, and made a part of Article 2, Chapter 1 of the Los Angeles Municipal Code.

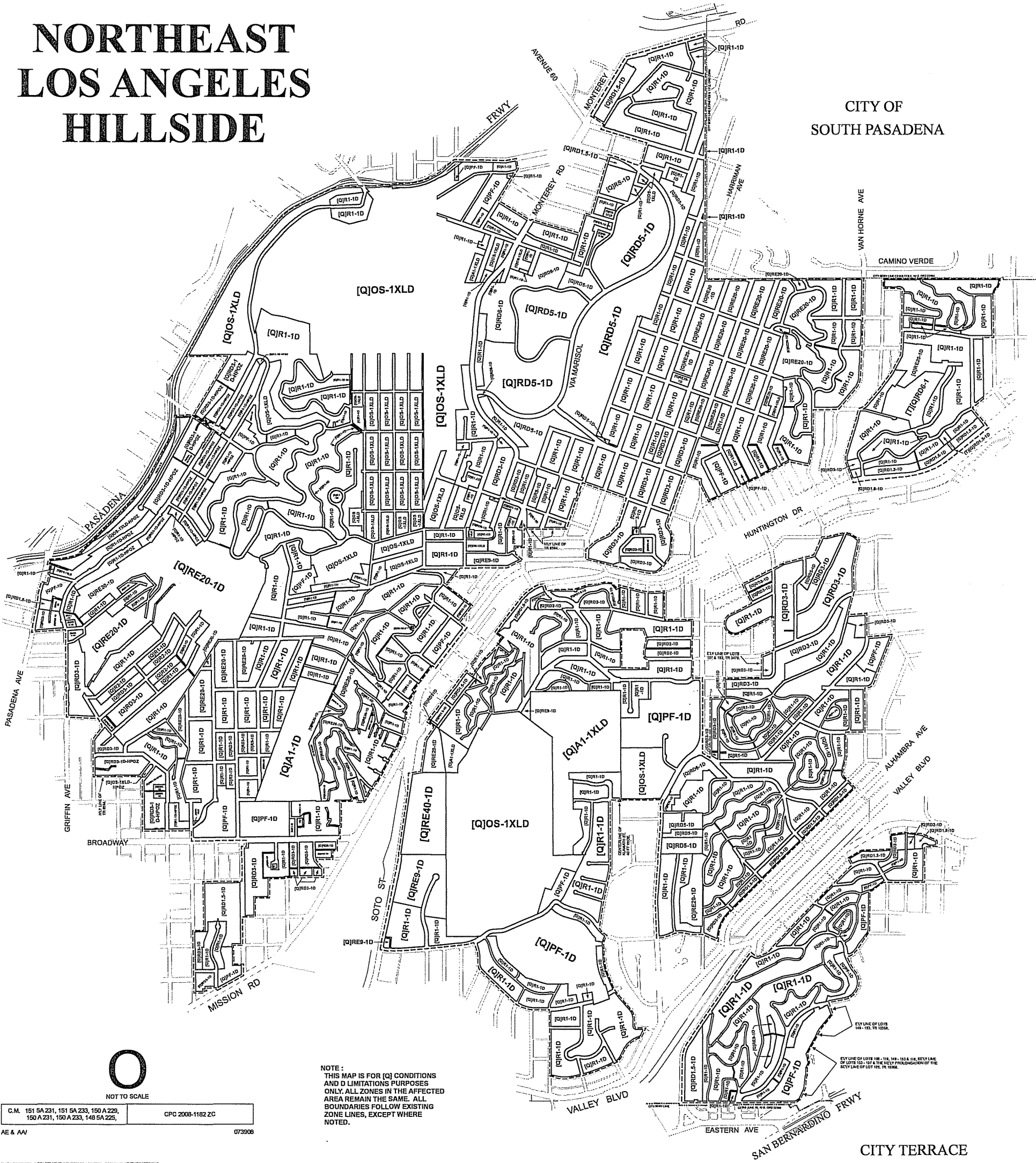
Table for Section 1

Existing Zone	New Zone
OS-1XL-HPOZ	[Q]OS-1XLD-HPOZ
OS-1XL	[Q]OS-1XLD
A1-1	[Q]A1-1D
A1-1XL	[Q]A1-1XLD
RE40-1	[Q]RE40-1D
RE20-1	[Q]RE20-1D
RE9-1	[Q]RE9-1D
RS-1	[Q]RS-1D
R1-1-HPOZ	[Q]R1-1D-HPOZ
R1-1	[Q]R1-1D
R2-1-HPOZ	[Q]R2-1D-HPOZ
R2-1	[Q]R2-1D
RD6-1	[Q]RD6-1D
[T][Q]RD6-1	[T][Q]RD6-1D
RD5-1	[Q]RD5-1D
[Q]RD5-1	[Q]RD5-1D
RD4-1-HPOZ	[Q]RD4-1D-HPOZ
RD3-1-HPOZ	[Q]RD3-1D-HPOZ
RD3-1	[Q]RD3-1D
RD2-1	[Q]RD2-1D
RD1.5-1	[Q]RD1.5-1D
(T)(Q)RD1.5-1	(T)[Q]RD1.5-1D
C1-1	[Q]C1-1
PF-1	[Q]PF-1D

SECTION 2. Pursuant to Section 12.32 G of the Los Angeles Municipal Code, and any amendment thereto, the following limitations are hereby imposed upon the use of that property described in Section 1 hereof which are subject to the Permanent "Q" Qualified Classification. **Clarifications of these Permanent "Q" Qualified Classifications should be processed through Zoning Administrator Adjustments, pursuant to Section 12.28 of the Los Angeles Municipal Code, whereby up to twenty percent increases in height and up to ten percent increases in residential floor area may be requested.** Where the zone symbols of the new zoning designation as shown in the

NORTHEAST LOS ANGELES HILLSIDE

CITY OF
SOUTH PASADENA



NOTE:
THIS MAP IS FOR [Q] CONDITIONS
AND D LIMITATIONS PURPOSES
ONLY. ALL ZONES IN THE AFFECTED
AREA REMAIN THE SAME. ALL
BOUNDARIES FOLLOW EXISTING
ZONE LINES, EXCEPT WHERE
NOTED.

0
NOT TO SCALE

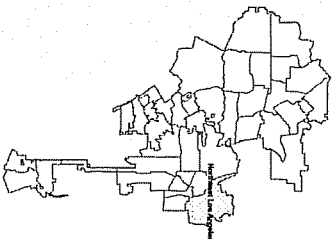
C.M. 151 5A 231, 151 5A 233, 150 A 229, 150 A 231, 150 A 233, 148 5A 225,	CPC 2008-1182 ZC
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AE & AV 073906

DATA SOURCES: DEPARTMENT OF CITY PLANNING - BUREAU OF ENGINEERING.

Northeast LA Hillside Work Program
Ridgelines

- Legend
- New Proposed Hillside Boundary
 - Ridgelines
 - Northeast LA Community Boundary



Disclaimer:
The City of Los Angeles is neither responsible nor liable for any inaccuracies, errors or omissions with respect to the material contained on this map. The map and all materials contained on it are furnished and furnished to the user as a reference only. The user shall be responsible for any inaccuracies, errors or omissions. The City of Los Angeles is not responsible for any special, indirect, incidental or consequential damages that may arise from the use of or reliance on the map. The map is provided for informational purposes only. The map is not a legal document. The map is provided by the City of Los Angeles as a public good.

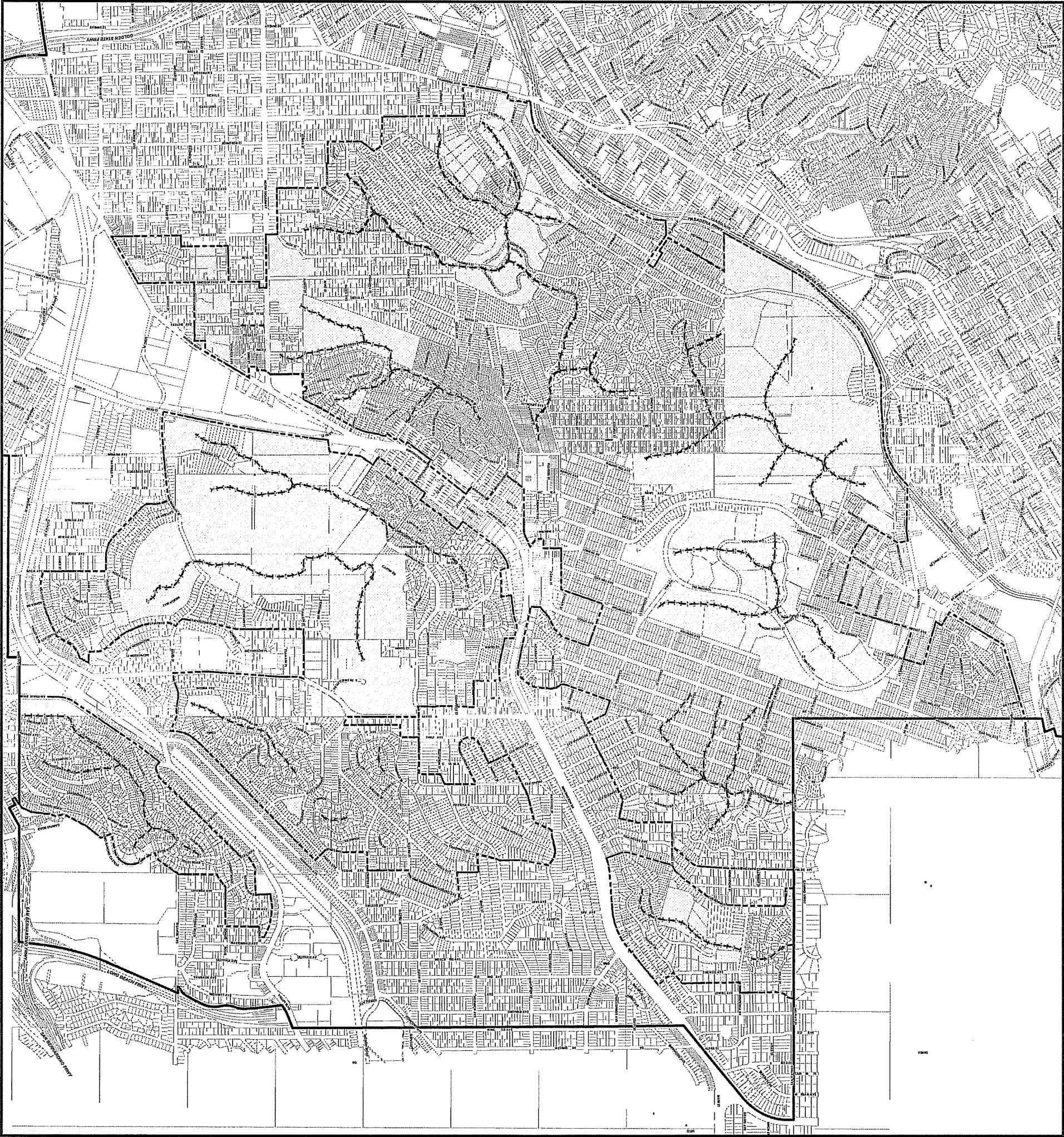


table for Section 1, are preceded by the symbol "Q" in brackets, the conditions and limitations imposed by the new "Q" Qualified Classification are set forth as follows:

A. All properties located within the Zone Change Ordinance Map shall be subject to the following [Q] Conditions:

1. Infrastructure

- a. Construction materials and equipment shall not be permitted to be stored in the public right-of-way in any manner that reduces roadway clearance to less than 20-feet in width. Storage of construction materials and equipment on public property requires a street use permit from the Bureau of Street Services.
- b. Construction vehicles shall be subject to the restrictions established by the Los Angeles Fire Department Red Flag - No Parking Program. Restricted parking signs shall be procured and installed along the project site at the owner/developer's expense when required by the LAFD and/or LADOT.

2. Building Design

- a. Second story setbacks or terraced structures and other design articulations are to be used to ensure that new development is compatible with existing neighborhood identity, character and scale.
- b. Building materials match architectural style of new development.
- c. Architectural design elements of the front and rear building elevations including articulation of facades, modulations of walls, shape, type details and the location of windows, doors, columns, balconies and garage doors vary from the adjacent/abutting buildings.
- d. Design of new structures shall meet one of the following standards:
 - (1) The total residential floor area of each story other than the base floor in a multi-story building does not exceed 75 percent of the base floor area, where "Base Floor" and "Floor Area, Residential" shall be defined as set forth in Section 12.03 of the Los Angeles Municipal Code for the purposes of this regulation; or
 - (2) The cumulative length of the exterior walls facing the front lot line, equal to a minimum of 25 percent of the building width shall be stepped-back a distance of at least 20 percent of the building depth from a plane parallel to the lot width established at the point of the building closest to the front lot line. When the front lot line is not straight, a line connecting the points where the side lot lines and the front lot line intersect shall be used. When through-lots have two front yards, the step-back shall be provided along both front lot lines; or

- (3) The buildings of the project shall consist of 3 or more building elements, each with its own associated roof form. A building element may also be a major horizontal mass, setback, or forward from the face of other masses.
- e. Structures within 50 linear feet of identified ridgelines, as shown on attached map marked 'Northeast LA Ridgelines', are limited to one story or 15 feet in height. The 50 linear feet must be labeled on all plans accordingly. No structure shall exceed a height in excess of 15 feet above the identified ridgeline. The precise ridgelines shall be verified and identified on the required topographic survey, prepared by a licensed surveyor.
- f. New hardscape areas, not including foundations, shall utilize permeable paving systems including, but not limited to pavers, porous asphalt, porous concrete, grasscrete and/or similar materials as approved by LADBS.

3. Retaining Walls

- a. The maximum total height of all freestanding retaining walls shall not exceed 12 feet, with no individual wall measuring higher than 6 feet on private property. Each freestanding retaining wall shall not exceed 75 feet in linear length, nor extend beyond one lot. Walls shall be separated by a minimum horizontal distance equal to the height of the highest wall. Freestanding garden walls 36" in height or less shall not be considered retaining walls for the purposes of this regulation.
- b. Require that all retaining walls provide a standard surface backdrain system and all drainage shall be conducted to the street in a non-erosive device, as required by approved Soils Report.

4. Landscaping

- a. Landscaping palette for required landscape plans shall be comprised of drought tolerant and/or native plant material that is fire retardant and controls erosion.
- b. Retaining walls and building understory areas shall be fully screened with plantings in a reasonable amount of time, as shown on approved landscape plan
- c. Landscape plans must be submitted to Bureau of Street Services Urban Forestry Division prior to DCP clearance. Upon satisfaction of the requirements set forth under LAMC Ordinance No. 177,404 (Protected Trees) deemed necessary by the Urban Forestry Division, an approval letter will be issued by the Urban Forestry Division and submitted with new development filings as part of submission packages.

- d. Landscaping plans must be submitted to Bureau of Street Services, Urban Forestry Division, and Department of City Planning for review and approval consistent with LAMC Ordinance No. 177,404 (Protected Trees).
- e. A signed "Certified Arborist's or Licensed Landscape Architect's Certificate of Compliance" must be filed with the Department of Building & Safety prior to issuance of a "Certificate of Compliance" to ensure that landscaping plans are fully implemented.

5. Environmental

- a. A Geotechnical Investigation Report that evaluates the proposed project's soil and grading shall be submitted to the LADBS Grading Division for review. An approved Soils & Grading report letter from LADBS – Grading Division shall be required prior to approval of a grading, foundation or building permit.
- b. Grading shall be done in accordance with the Planning Guidelines Landform Grading Manual adopted by the City Council.
- c. All new graded slopes shall be no steeper than 2:1 (rise:run), except when the Grading Division has determined that slopes may exceed 2:1 as part of an approved Soils Report.
- d. Grading shall be limited to a maximum of 500 cubic yards + numeric value equal to 5 percent of the total lot size, up to a maximum of 1,000 cubic yards total. Any deviations beyond these limits shall require a Zoning Administrator's approval under LAMC §12.27.
- e. New hardscape areas, not including foundations, shall utilize permeable paving systems including, but not limited to pavers, porous asphalt, porous concrete, grasscrete and/or similar materials as approved by LADBS.

B. In addition to those [Q] Conditions listed under Section 2A, the [Q] Conditions pursuant to Ordinances 166,216, 166,267, 179,344 shall be retained.

SECTION 3. Pursuant to Section 12.32 G of the Los Angeles Municipal Code, and any amendment thereto, the following limitations are hereby imposed upon the use of that property described in Section 1 hereof which are subject to the Permanent "D" Development Limitations. Where the zone symbols of the new zoning designation as shown in the table for Section 1, are followed by the symbol "D" in the suffix, the conditions and limitations imposed by the new "D" Development Limitation are set forth as follows:

A. All properties located within the Zone Change Ordinance Map shall be subject to the following [D] Development Limitations:

1. Height

- a. In addition to the height limitations contained in LAMC Section 12.21 A17(c), no building or structure shall exceed 30 feet in height from adjacent finished grade, measured as the vertical distance from the adjacent finished grade of the site to an imaginary plane located above and parallel to the finished grade; except that when the roof of the uppermost story of a building or structure or portion of the building or structure has a slope of less than 25 percent, the maximum height shall be 26 feet above adjacent finished grade.
- b. The finished floor elevation directly above an exposed underfloor area shall be limited to 6 feet above finished grade.
- c. Attached decks shall be limited such that no portion of the walking surface of a deck with visible underpinnings shall exceed a height of 6' above grade and decks shall be integrated into the architecture of the house, and not appear as an add-on to the primary building mass.

2. Maximum Residential Floor Area

Residential Floor Area for all lots shall be calculated as defined in Section 12.03 of the Los Angeles Municipal Code. Consistent with the definition contained therein, the first 400 square feet of covered parking area shall not be counted towards the maximum Residential Floor Area. In order to calculate the maximum Residential Floor Area permitted, a site survey showing 1-foot contours must be prepared by a licensed surveyor. The survey shall identify the total area of the lot, in square feet, according to the following slope intervals:

- Area A – Slope less than 15 percent;
- Area B – Slope at least 15 percent, but less than 30 percent;
- Area C – Slope at least 30 percent, but less than 45 percent;
- Area D – Slope at least 45 percent, but less than 60 percent;
- Area E – Slope at least 60 percent, but less than 100 percent;
- Area F – Slope greater than 100 percent.

For lots in the RD1.5 and RD2 Zones, the maximum Residential Floor Area contained in all buildings and accessory structures shall be determined by the following formula:

100% of Area A + 90% of Area B + 80% of Area C + 70% of Area D + 60% of Area E + 50% of Area F.

The maximum Residential Floor Area in the RD1.5 and RD2 Zones, however, shall not be limited below 3,000 square feet.

For lots in the R2, RD3, RD4, RD5, and RD6 Zones, the maximum Residential Floor Area contained in all buildings and accessory structures shall be determined by the following formula:

75% of Area A + 65% of Area B + 55% of Area C + 45% of Area D + 35% of Area E + 25% of Area F.

The maximum Residential Floor Area in the R2, RD3, RD4, RD5, and RD6 Zones, however, shall not be limited below 2,200 square feet.

For lots in the R1 Zone, the maximum Residential Floor Area contained in all buildings and accessory structures shall be determined by the following formula:

50% of Area A + 45% of Area B + 40% of Area C + 35% of Area D + 30% of Area E + 0% of Area F.

The maximum Residential Floor Area in the R1 Zone, however, shall not be limited below 20% of the total lot size, or 1,000 square feet, whichever is greater. **The maximum Residential Floor Area in the R1 Zone, for lots 5,000 square feet in size with the slope entirely within Area F, however, shall not be limited below 20% of the total lot size, or 1,100 square feet, whichever is greater.**

For lots in the RS Zone, the maximum Residential Floor Area contained in all buildings and accessory structures shall be determined by the following formula:

45% of Area A + 40% of Area B + 35% of Area C + 30% of Area D + 25% of Area E + 0% of Area F.

The maximum Residential Floor Area in the RS Zone, however, shall not be limited below 20% of the total lot size, or 1,000 square feet, whichever is greater.

For lots in the RE9 Zone, the maximum Residential Floor Area contained in all buildings and accessory structures shall be determined by the following formula:

40% of Area A + 35% of Area B + 30% of Area C + 25% of Area D + 20% of Area E + 0% of Area F.

The maximum Residential Floor Area in the RE9 Zone, however, shall not be limited below 20% of the total lot size, or 1,000 square feet, whichever is greater.

For lots in the RE20 Zone, the maximum Residential Floor Area contained in all buildings and accessory structures shall be determined by the following formula:

35% of Area A + 30% of Area B + 25% of Area C + 20% of Area D + 15% of Area E + 0% of Area F.

The maximum Residential Floor Area in the RE20 Zone, however, shall not be limited below 20% of the total lot size, or 1,000 square feet, whichever is greater.

For lots in the RE40 Zone, the maximum Residential Floor Area contained in all buildings and accessory structures shall be determined by the following formula:

35% of Area A + 30% of Area B + 25% of Area C + 20% of Area D + 15% of Area E + 0% of Area F.

The maximum Residential Floor Area in the RE40 Zone, however, shall not be limited below 20% of the total lot size, or 1,000 square feet, whichever is greater.

For lots in the A1 Zone, the maximum Residential Floor Area contained in all buildings and accessory structures shall be determined by the following formula:

25% of Area A + 20% of Area B + 15% of Area C + 10% of Area D + 5% of Area E + 0% of Area F.

The maximum Residential Floor Area in the A1 Zone, however, shall not be limited below 20% of the total lot size, or 1,000 square feet, whichever is greater.

For lots in the R1, RS, RE9, RE20, or RE40 Zone, if no portion of a lot exceeds a slope of 15 percent, an additional 20% of the maximum Residential Floor Area for that lot shall be allowed, consistent with Ordinance 179,883, effective June 29, 2008. Only one 20% bonus per property is allowed.

3. Residential Floor Area Bonus For Buildings Eligible for LEED® Certification

For lots in any Zone with a portion of the lot exceeding a slope of 15 percent, which are ineligible to receive the above 20% bonus in Residential Floor Area, an additional 20% of the maximum Residential Floor Area for that lot shall be allowed for the following:

New single family dwelling construction only, if the new construction is in substantial compliance with the requirements for the U.S. Green Building Council's (USGBC) Leadership in Energy and Environmental Design (LEED®) for Homes program at the "Certified" level or higher.

Prior to submitting an application to the Department of Building and Safety for a building permit, the applicant shall be required to obtain an

authorization to submit for plan check from the Department of Planning. In order to obtain this authorization, the applicant shall provide:

(1) Documentation that the project has been registered with the USGBC's LEED® for Homes Program, and that the required fees have been paid;

(2) A preliminary checklist from a USGBC-contracted LEED® for Homes Provider, which demonstrates that the project can be registered with the LEED® for Homes Program with a target of certification at the "Certified" or higher level;

(3) A signed declaration from the USGBC-contracted LEED® for Homes Provider stating that the plans and plan details have been reviewed, and confirms that the project can be registered with the LEED® for Homes Program with a target certification at the "Certified" or higher level; and

(4) A complete set of plans stamped and signed by a licensed architect or engineer that include a copy of the preliminary checklist and signed declaration identified in Subparagraphs (2) and (3) of this paragraph and identify the measures being provided for LEED® Certification. Each plan sheet must also be signed by a USGBC-contracted LEED® for Homes Provider verifying that the plans are consistent with the submitted preliminary checklist.

The Department of Building and Safety shall refer applicants to the Department of Planning prior to issuance of a building permit to obtain a clearance to verify the project compliance with the originally approved plans.

If changes are made to the project, the applicant shall be required to submit a revised set of plans, including the four requirements listed above, with all revisions necessary to make the project in substantial compliance with the requirements for LEED® Certification."

4. Verification of Existing Residential Floor Area

For additions with cumulative residential floor area of less than 500 square feet constructed after the adoption of this ordinance, or remodels of buildings built prior to the adoption of this ordinance, the existing residential floor area shall be the same as the building square footage shown on the most recent Los Angeles County Tax Assessor's records at the time the plans are submitted to the Department of Building and Safety and a plan check fee is paid. Except that residential floor area may be calculated as defined in Section 12.03 of this Code when a complete set of fully dimensioned plans with area calculations of all the structures on the lot, prepared by a licensed architect or engineer, is submitted by the applicant.

Any work that does not qualify as a remodel, as defined in the paragraph below, or additions that are 500 square feet or larger shall require a complete set of fully dimensioned plans with area calculations of all the structures on the lot prepared by a licensed architect or engineer.

For the purposes of implementing this subdivision, a remodel shall mean the alteration of an existing building or structure provided that at least 50 percent of the perimeter length of the contiguous exterior walls and 50 percent of the roof are retained.

SECTION 4. The City Clerk shall certify to the passage of this ordinance and have it published in accordance with Council policy, either in a daily newspaper circulated in the City of Los Angeles or by posting for ten days in three public places in the City of Los Angeles: one copy on the bulletin board located at the Main Street entrance to the Los Angeles City Hall; one copy on the bulletin board located at the Main Street entrance to the Los Angeles City Hall East; and one copy on the bulletin board located at the Temple Street entrance to the Los Angeles County Hall of Records. **If any provision of this Ordinance or its application to any property is held to be unconstitutional or otherwise invalidated by any court of competent jurisdiction, invalidity shall not affect other Ordinance provisions, clauses or applications which can be implemented without the invalid provision, clause or application, and to this end the provisions and clauses of this Ordinance are declared to be severable.**

I hereby certify that this ordinance was passed by the Council of the City of Los Angeles at its meeting of NOV 26 2008.

KAREN E. KALFAYAN, City Clerk

By [Signature] Deputy

Approved _____

DEC 10 2008

[Signature]
Mayor

Approved as to form and legality

ROCKARD J. DELGADILLO, City Attorney

Pursuant to Section 559 of the City Charter, the City Planning Commission on August 14, 2008, recommended that this Ordinance be adopted by the City Council.

By _____

City Attorney

November 19, 2008
see attached report

[Signature] AICP
S. Gail Goldberg, AICP
Director of Planning [Signature]

File No. 08-2599

EXHIBIT C

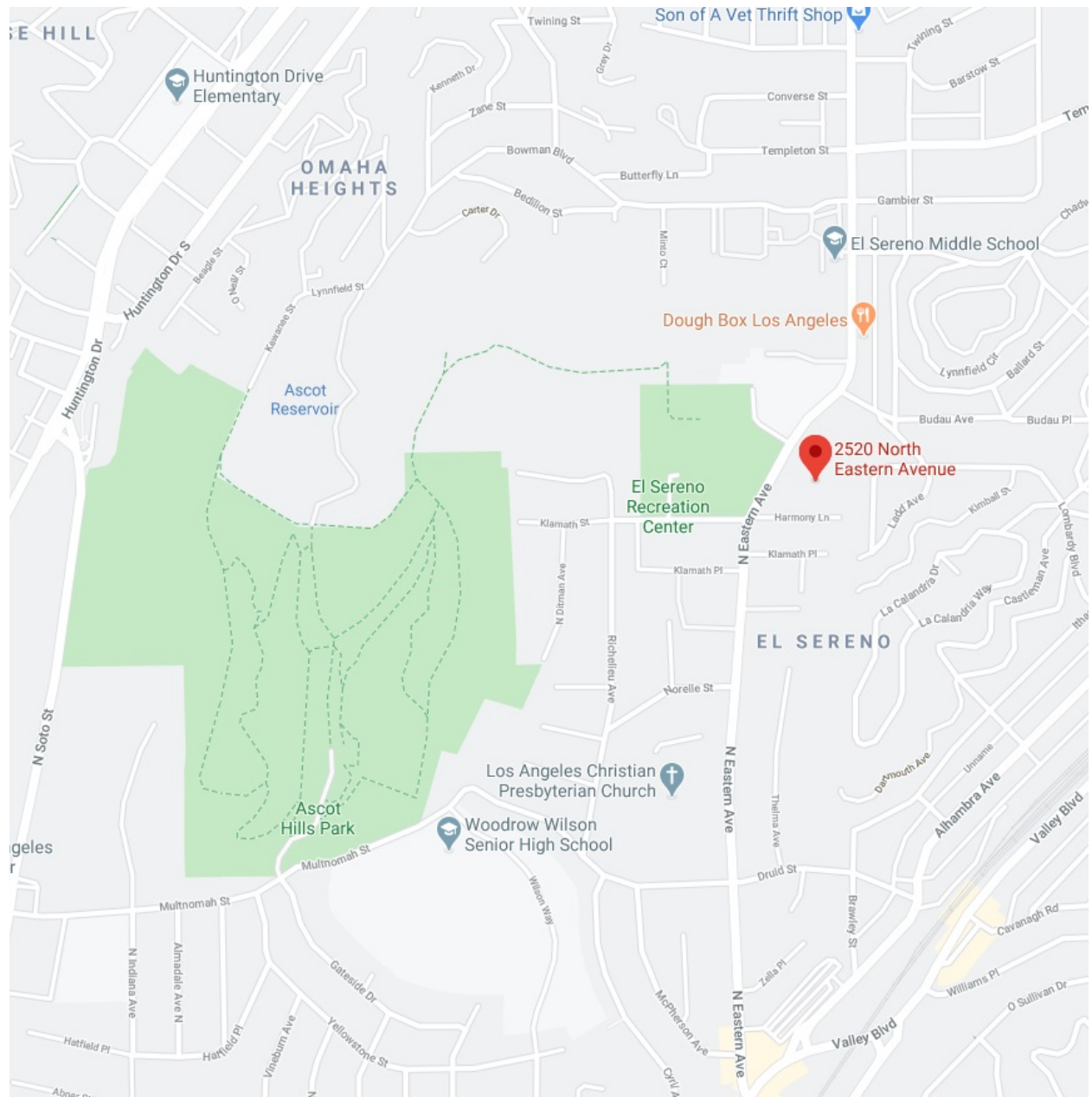


EXHIBIT D

CITY OF LOS ANGELES
OFFICE OF THE CITY CLERK
ROOM 395, CITY HALL
LOS ANGELES, CALIFORNIA 90012
CALIFORNIA ENVIRONMENTAL QUALITY ACT
PROPOSED MITIGATED NEGATIVE DECLARATION

EXHIBIT FILED City Clerk's Office	No. 17-054-PL	Certified by <i>SC</i>	Date 4-3-17
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LEAD CITY AGENCY

City of Los Angeles

COUNCIL DISTRICT

14

PROJECT TITLE

El Sereno

CASE NO.

ENV-2015-1918-MND, VTT-73531, APCE 2015-2048-ZC-ZAD

PROJECT LOCATION

2520, 2532, 2608, 2668, North Eastern Avenue and 2647, 2649, 2651 Lombardy Boulevard, Los Angeles, California, 90032

PROJECT DESCRIPTION

The Project site includes three contiguous infill lots totaling approximately 218,270 square feet, located at the south corner of Eastern Avenue and Lombardy Boulevard in the Northeast Los Angeles Community Plan Area of the City of Los Angeles. The land use designation for the Project site is Low Residential, and the Project site is zoned [Q]R1-1D and [Q]RD6-1D. The Project includes development of the Project site with 42 single-family residential homes. Each house would have 3-4 bedrooms and a two-car garage. The homes would range in size from approximately 1,729 square feet to 2,279 square feet. In order to implement the Project, the Project Applicant is requesting approval of the following discretionary actions from the City: 1) Vesting Tract Map (VTT) for Small Lot Purposes per LAMC Section 17.03 – Request is for a Vesting Tentative Tract Map to create forty-two (42) single-family lots in accordance with the Small Lot Subdivision Ordinance No. 176,354 in the Northeast Los Angeles Community Plan; 2) Tree Removal Permit – Request is authorization from the Board of Public Works or the Advisory Agency to allow for the removal of up to 39 protected trees pursuant to LAMC Section 17.05R1(b); 3) Vesting Zone Change (ZC) per LAMC Section 12.32 – Request to permit a change of zone from [Q]R1-1D and [Q]RD6-1D to (T)(Q)RD5-1D; 4) Zoning Administrator's Determination (ZAD) per LAMC Section 12.24 X.26 – Request is to allow fifty-four (54) walls varying in height from 3.5 feet to 6.0 feet in lieu of the maximum of two (2) 10-foot retaining walls otherwise required in LAMC Section 12.21 C.8(a); and 5) Haul Route Approval from the Board of Building and Safety Commission or Advisory Agency.

NAME AND ADDRESS OF APPLICANT IF OTHER THAN CITY AGENCY

Clearwater Communities, LLC
4685 MacArthur Court, Suite 375
Newport Beach, CA 92660

FINDING:

The City Planning Department of the City of Los Angeles has Proposed that a mitigated negative declaration be adopted for this project because the mitigation measure(s) outlined on the attached page(s) will reduce any potential significant adverse effects to a level of insignificance

(CONTINUED ON PAGE 2)

SEE ATTACHED SHEET(S) FOR ANY MITIGATION MEASURES IMPOSED.

Any written comments received during the public review period are attached together with the response of the Lead City Agency. The project decision-maker may adopt the mitigated negative declaration, amend it, or require preparation of an EIR. Any changes made should be supported by substantial evidence in the record and appropriate findings made.

THE INITIAL STUDY PREPARED FOR THIS PROJECT IS ATTACHED.

NAME OF PERSON PREPARING THIS FORM

TITLE

TELEPHONE NUMBER

Gregory S Shoop

City Planner

213-978-1213

ADDRESS

SIGNATURE (Official)

DATE

200 N. SPRING STREET, 6th FLOOR
LOS ANGELES, CA. 90012

Becky Lamb

APRIL 26, 2017

AESTHETICS

1-1: Non-Protected Trees

- Prior to issuance of any permit related to development of the Project, a plot plan shall be prepared for the Project, indicating the location, size, type, and general condition of all existing trees on the Project site and within the adjacent public right(s)-of-way.
- All significant (8-inch or greater trunk diameter, or cumulative trunk diameter if multi-trunked, as measured 54 inches above the ground) non-protected trees on the Project site proposed for removal shall be replaced at a 1:1 ratio with a minimum 24-inch box tree. Net new trees located within the parkway of the adjacent public-right(s)-of-way may be counted toward replacement tree requirements.
- Removal or planting of any tree in the public right-of-way shall require approval of the Board of Public Works. All trees in the public right-of-way shall be provided in the current standards of the Urban Forestry Division of the Department of Public Works, Bureau of Street Services.

1-2 Protected Trees

- All protected tree removals shall require approval from the Board of Public Works.
- A Tree Report shall be submitted to the Urban Forestry Division of the Bureau of Street Services, Department of Public Works, for review and approval prior to implementation of the Report's recommended measures.
- According to the City's Protected Tree Ordinance, a minimum of four protected trees (a minimum of 15 gallon in size) shall be planted for each protected tree that is removed.. The size of each replacement tree shall measure at least one inch or more in diameter at a point one foot above the base, and not less than seven feet in height, measured from the base.
- In consultation with the Division of Urban Forestry, twenty-five percent of the protected trees removed shall be replaced with 15-gallon *Juglans californica*.
- The location of the trees planted for the purposes of replacing a removed protected tree shall be clearly indicated on the required landscape plan, which shall also indicate the replacement tree species and further contain the phrase "Replacement Tree" in its description.

1-3 Previously Removed Trees

- The 8 qualifying previously removed protected trees shall be replaced at a ratio of one *Juglans californica* and three *Quercus* for each of the 8 trees, at a minimum of 15 gallon in size. The specific size and species of the trees to be planted as replacement for the protected trees being removed shall be determined by the Urban Forestry Division.

1-4 All Trees

- Protection Barrier: A protection barrier shall be installed around the construction area as shown on the map included in the Tree Preservation Report (refer to Appendix A). The barrier shall be 6-foot-high chain-link fencing. Twelve-inch-high silt fence shall be attached to the base of the fence with the bottom edge buried 1-2 inches. The barrier may be placed on the line shown on the map or closer to construction, but not further. The fencing shall be maintained in good repair throughout the duration of the Project, and

shall not be removed, relocated, or encroached upon without permission of the arborist involved.

- **Storage of Materials:** There shall be NO storage of materials or supplies of any kind inside the area of the protection fencing. Concrete and cement materials, block, sand and soil shall not be placed within the drip-line of any tree to remain.
- **Fuel Storage:** Fuel storage shall NOT be permitted within 150 feet of any tree to be preserved. Refueling, servicing and maintenance of equipment and machinery shall NOT be permitted within 150 feet of protected trees.
- **Debris and Waste Materials:** Debris and waste from construction or other activities shall NOT be permitted outside the construction area. Wash down of concrete or cement handling equipment, in particular, shall NOT be permitted within 150 feet of protected trees.
- **Planting near Trees Designated for Protection:** Any digging within designated protection zones shall be done using supersonic air directly as the digging medium, by means of a nozzle, whose nominal rated input pressure (available from manufacturer's literature) must not exceed 130 psig (pounds per square inch at gage) unless otherwise approved. Nozzles designed for input above 130 psig can damage fine roots. Air compressors rated between 100 to 125 psig recommended.
- **Grade Changes:** Any grade changes within the protection radius listed should be approved by a Registered Consulting Arborist before construction begins, and precautions taken to mitigate potential injuries. Grade changes can be particularly damaging to trees. Even as little as two inches of fill can cause the death of a tree. Lowering the grade can destroy major portions of a root system.
- **Damages:** Any tree damages or injuries should be reported to the project arborist as soon as possible. Severed roots shall be cut cleanly to healthy tissue, using proper pruning tools. Broken branches or limbs shall be pruned according to International Society of Arboriculture Pruning Guidelines and ANSI A-300 Pruning Standards.
- **Preventive Measures:** Pruning of the tree canopies and branches should be done at the direction of the project arborist to remove any dead or broken branches, and to provide any necessary clearances for the construction work or equipment.

AIR QUALITY

- 3-1:** All off-road construction equipment greater than 50 hp shall meet U.S. EPA Tier 4 emission standards, where available, to reduce NO_x, PM₁₀, and PM_{2.5} emissions at the Project site. In addition, all construction equipment shall be outfitted with Best Available Control Technology devices certified by CARB. Any emissions control device used by the contractor shall achieve emissions reductions that are no less than what could be achieved by a Level 3 diesel emissions control strategy for a similarly sized engine as defined by CARB regulations.
- 3-2:** Require the use of 2010 and newer diesel haul trucks (e.g., material delivery trucks and soil import/export) and if the Lead Agency determines that 2010 model year or newer diesel trucks cannot be obtained, the Lead Agency shall require trucks that meet U.S. EPA 2007 model year NO_x emissions requirements.
- 3-3:** At the time of mobilization of each applicable unit of equipment, a copy of each unit's certified tier

specification, BACT documentation, and CARB or SCAQMD operating permit shall be provided.

- 3-4:** Encourage construction contractors to apply for SCAQMD "SOON" funds. Incentives could be provided for those construction contractors who apply for SCAQMD "SOON" funds. The "SOON" program provides funds to accelerate clean up of off-road diesel vehicles, such as heavy duty construction equipment. More information on this program can be found at: <http://www.aqmd.gov/home/programs/business/business-detail?title=off-road-dieselengines&parent=vehicle-engine-upgrades>.
- 3-5:** Construction activities shall comply with SCAQMD Rule 403, including the following measures:
- Apply water to disturbed areas of the site three times a day
 - Require the use of a gravel apron or other equivalent methods to reduce mud and dirt trackout onto truck exit routes
 - Appoint a construction relations officer to act as a community liaison concerning on-site construction activity including resolution of issues related to PM generation.
 - Limit soil disturbance to the amounts analyzed in the Final MND.
 - All materials transported off-site shall be securely covered.
 - Apply non-toxic soil stabilizers according to manufacturers' specifications to all inactive construction areas (previously graded areas inactive for ten days or more).
 - Traffic speeds on all unpaved roads to be reduced to 15 mph or less.

BIOLOGICAL RESOURCES

- 4-1:** To avoid potential significant impacts to nesting birds, including migratory birds and raptors, one of the following shall be implemented by the Project Applicant:
- Conduct vegetation removal associated with construction from September 1st through January 31st, when birds are not nesting. Initiate grading activities prior to the breeding season (which is generally February 1st through August 31st) and keep disturbance activities constant throughout the breeding season to prevent birds from establishing nests in surrounding habitat (in order to avoid possible nest abandonment); if there is a lapse in activities of more than five days, pre-construction surveys shall be necessary as described in the bullet below.
- OR...
- Conduct pre-construction surveys for nesting birds if vegetation removal or grading is initiated during the nesting season. A qualified wildlife biologist shall conduct weekly pre-construction bird surveys no more than 30 days prior to initiation of grading to provide confirmation on the presence or absence of active nests in the vicinity (at least 300 to 500 feet around the individual construction site, as access allows). The last survey should be conducted no more than three days prior to the initiation of clearance/construction work. If active nests are encountered, clearing and construction in the vicinity of the nests shall be deferred until the young birds have fledged and there is no evidence of a second attempt at nesting. A minimum buffer of 300 feet (500 feet for raptor nests) or as determined by a qualified biologist shall be

maintained during construction depending on the species and location. The perimeter of the nest-setback zone shall be fenced or adequately demarcated with staked flagging at 20-foot intervals, and construction personnel and activities restricted from the area. Construction personnel should be instructed on the sensitivity of the area. A survey report by the qualified biologist documenting and verifying compliance with the mitigation and with applicable state and federal regulations protecting birds shall be submitted to the City and County, depending on within which jurisdiction the construction activity is occurring. The qualified biologist shall serve as a construction monitor during those periods when construction activities would occur near active nest areas to ensure that no inadvertent impacts on these nests would occur.

NOISE

- 12-1:** The Project shall comply with the City of Los Angeles Building Regulations Ordinance No. 178048, which requires a construction site notice to be provided that includes the following information: job site address, permit number, name and phone number of the contractor and owner or owner's agent, hours of construction allowed by code or any discretionary approval for the site, and City telephone numbers where violations can be reported. The notice shall be posted and maintained at the construction site prior to the start of construction and displayed in a location that is readily visible to the public.
- 12-2:** Two weeks prior to commencement of construction, notification shall be provided to the off-site residential and school uses within 500 feet of the Project site that discloses the construction schedule, including the types of activities and equipment that would be used throughout the duration of the construction period.
- 12-3:** Temporary sound barriers, capable of achieving a sound attenuation of at least 10 dBA (e.g., construction sound wall with sound blankets), and capable of blocking the line-of-sight to the adjacent residences shall be installed as feasible.
- 12-4:** Noise-generating construction equipment operated at the Project Site shall be equipped with effective state-of-the-art noise control devices, i.e., mufflers, lagging, solar power or electric plug-in on-site power generators and/or motor enclosures or other shielding equipment. All equipment shall be properly maintained to assure that no additional noise, due to worn or improperly maintained parts, would be generated.
- 12-5:** All construction areas for staging and warming-up equipment shall be located as far as possible from adjacent residences.
- 12-6:** Portable noise sheds for smaller, noisy equipment, such as air compressors, dewatering pumps, and generators shall be provided where feasible.
- 12-7:** A haul route for exporting cut materials from the site to a nearby landfill that access the San Bernardino and/or Long Beach Freeways should minimize travel on residential streets with sensitive receptors.

TRANSPORTATION/TRAFFIC

16-1: Hillside Construction Staging and Parking Plan

- Prior to the issuance of a grading or building permit, the applicant shall submit a Construction Staging and Parking Plan to the Department of Building and Safety and the Fire Department for review and approval. The plan shall identify where all construction materials, equipment, and vehicles will be stored through the construction phase of the project, as well as where contractor, subcontractor, and laborers will park their

vehicles so as to prevent blockage of two-way traffic on streets in the vicinity of the construction site. The Construction Staging and Parking Plan shall include, but not be limited to, the following:

- No construction equipment or material shall be permitted to be stored within the public right-of-way.
- If the property fronts on a designated Red Flag Street, on noticed "Red Flag" days, all the workers shall be shuttled from an off-site area, located on a non-Red Flag Street, to and from the site in order to keep roads open on Red Flag days.
- During the Excavation and Grading phases, all haul trucks shall be staged on the Project site. The drivers shall be required to follow the designated travel plan or approved Haul Route.
- Truck traffic directed to the project site for the purpose of delivering materials, construction-machinery, or removal of graded soil shall be limited to off-peak traffic hours, Monday through Friday only. No truck deliveries shall be permitted on Saturdays or Sundays.
- All deliveries during construction shall be coordinated so that all vendor/delivery vehicles will stage and make deliveries on the project site, and that a construction supervisor is present at such time.
- A radio operator shall be on-site to coordinate the movement of material and personnel, in order to keep the roads open for emergency vehicles, their apparatus, and neighbors.
- During all phases of construction, all construction vehicle parking and queuing related to the project shall be as required to the satisfaction of the Department of Building and Safety, and in substantial compliance with the Construction Staging and Parking Plan, except as may be modified by the Department of Building and Safety or the Fire Department.

16-2: Construction Activity Near Schools

- The Project developer and contractors shall maintain ongoing contact with administrators of the Farmdale Elementary School and the El Sereno Middle School. The administrative offices shall be contacted when demolition, grading and construction activity begin on the Project site so that students and their parents will know when such activities are to occur. The developer shall obtain school walk and bus routes to the schools from either the administrators or from the LAUSD's Transportation Branch (323) 342-1400 and guarantee that safe and convenient pedestrian and bus routes to the school be maintained.
- The developer shall install appropriate traffic signs around the site to ensure pedestrian and vehicle safety.
- There shall be no staging or parking of construction vehicles, including vehicles to transport workers on any of the streets adjacent to the school.
- Due to noise impacts on the schools, no construction vehicles or haul trucks shall be staged or idled on these streets during school hours.

16-3: Schools affected by Haul Route

- LADBS shall assign specific haul route hours of operation based upon Farmdale Elementary School and El Sereno Middle School hours of operation.

- Haul route scheduling shall be sequenced to minimize conflicts with pedestrians, school buses and cars at the arrival and dismissal times of the school day. Haul route trucks shall not be routed past the school during periods when school is in session especially when students are arriving or departing from the campus.

16-4: Good Neighbor Construction Practices

- Whenever possible, construction vehicles should be parked on site to prevent congestion on streets with limited parking.
- When temporarily blocking portions of streets for deliveries of construction materials, a flag person shall be provided to assist with pedestrian and vehicular traffic.
- Street closures shall not take place during peak traffic hours. Any street, sidewalk, or other improvement work shall be conducted in conformance with the latest Manual on Work Area Traffic Control.
- Care shall be taken not to overfill concrete trucks during deliveries. If spills occur, it shall be the responsibility of the concrete company to immediately provide clean up.
- Construction noise shall be kept to a minimum with consideration of the surrounding neighbors. Unnecessary noise such as music shall be kept below legal levels.
- Streets and sidewalks adjacent to construction sites shall be swept free of construction debris at all times.
- Care shall be taken to not interfere with trash pick-up by the Bureau of Sanitation. Construction and delivery vehicles shall be subject to trash pick-up parking restrictions.
- If building materials are to be stored in public right of ways, it shall be by permit from the Department of Public Works, Bureau of Street Services, Investigations and Enforcement Division and shall conform with all applicable rules.
- All construction/demolition activities shall comply with the construction hours in Section 41.40 of the LAMC.

- 16-5:** The Project Applicant shall plan construction and construction staging as to maintain pedestrian access to adjacent active land uses throughout all construction phases. This requires the Applicant to maintain adequate and safe pedestrian protection, including physical separation from workspace and vehicular traffic and overhead protection, due to sidewalk closure or blockage, at all times. Barriers, such as K-Rails, scaffolding, etc., shall be maintained at a height of 8 feet.

City of Los Angeles

Department of City Planning • Environmental Analysis Section
City Hall • 200 N. Spring Street, Room 750 • Los Angeles, CA 90012



MITIGATED NEGATIVE DECLARATION NORTHEAST LOS ANGELES COMMUNITY PLAN AREA

El Sereno Project

Case Number: ENV-2015-1918-MND, VTT-73531, APCE-2015-2048-ZC-ZAD

Project Location: 2520, 2532, 2608, 2668 North Eastern Avenue and 2647, 2649, 2651 Lombardy Boulevard, Los Angeles, California, 90032

Council District: 14

Project Description: The Project site includes three contiguous infill lots totaling approximately 218,270 square feet, located at the south corner of Eastern Avenue and Lombardy Boulevard in the Northeast Los Angeles Community Plan Area of the City of Los Angeles. The land use designation for the Project site is Low Residential, and the Project site is zoned [Q]R1-1D and [Q]RD6-1D. The Project includes development of the Project site with 42 single-family residential homes. Each house would have 3-4 bedrooms and a two-car garage. The homes would range in size from approximately 1,729 square feet to 2,279 square feet. In order to implement the Project, the Project Applicant is requesting approval of the following discretionary actions from the City: 1) Vesting Tract Map (VTT) for Small Lot Purposes per LAMC Section 17.03 – Request is for a Vesting Tentative Tract Map to create forty-two (42) single-family lots in accordance with the Small Lot Subdivision Ordinance No. 176,354 in the Northeast Los Angeles Community Plan; 2) Tree Removal Permit – Request is authorization from the Board of Public Works or the Advisory Agency to allow for the removal of up to 39 protected trees pursuant to LAMC Section 17.05R1(b); 3) Vesting Zone Change (ZC) per LAMC Section 12.32 – Request to permit a change of zone from [Q]R1-1D and [Q] RD6-1D to (T)(Q)RD5-1D; 4) Zoning Administrator's Determination (ZAD) per LAMC Section 12.24 X.26 – Request is to allow fifty-four (54) walls varying in height from 3.5 feet to 6.0 feet in lieu of the maximum of two (2) 10-foot retaining walls otherwise required in LAMC Section 12.21 C.8(a); and 5) Haul Route Approval from the Board of Building and Safety Commission or Advisory Agency.

APPLICANT:

Clearwater Communities, LLC
4685 MacArthur Court, Suite 375
Newport Beach, CA 92660

PREPARED BY:

CAJA Environmental Services
11990 San Vicente Boulevard
Los Angeles, CA 90049

ON BEHALF OF:

The City of Los Angeles
Department of City Planning
Environmental Analysis Section

March 2017

I. INTRODUCTION

Introduction

The subject of this Initial Study/Mitigated Negative Declaration (IS/MND) is the development of 42 single-family residential homes on three contiguous infill lots totaling approximately 218,270 square feet, located at the south corner of Eastern Avenue and Lombardy Boulevard in the Northeast Los Angeles Community Plan Area of the City of Los Angeles (the "City"). The Project Applicant is Clearwater Communities, LLC. A more detailed description of the Project is contained in Section II (Project Description). The City's Department of City Planning is the Lead Agency under the California Environmental Quality Act (CEQA).

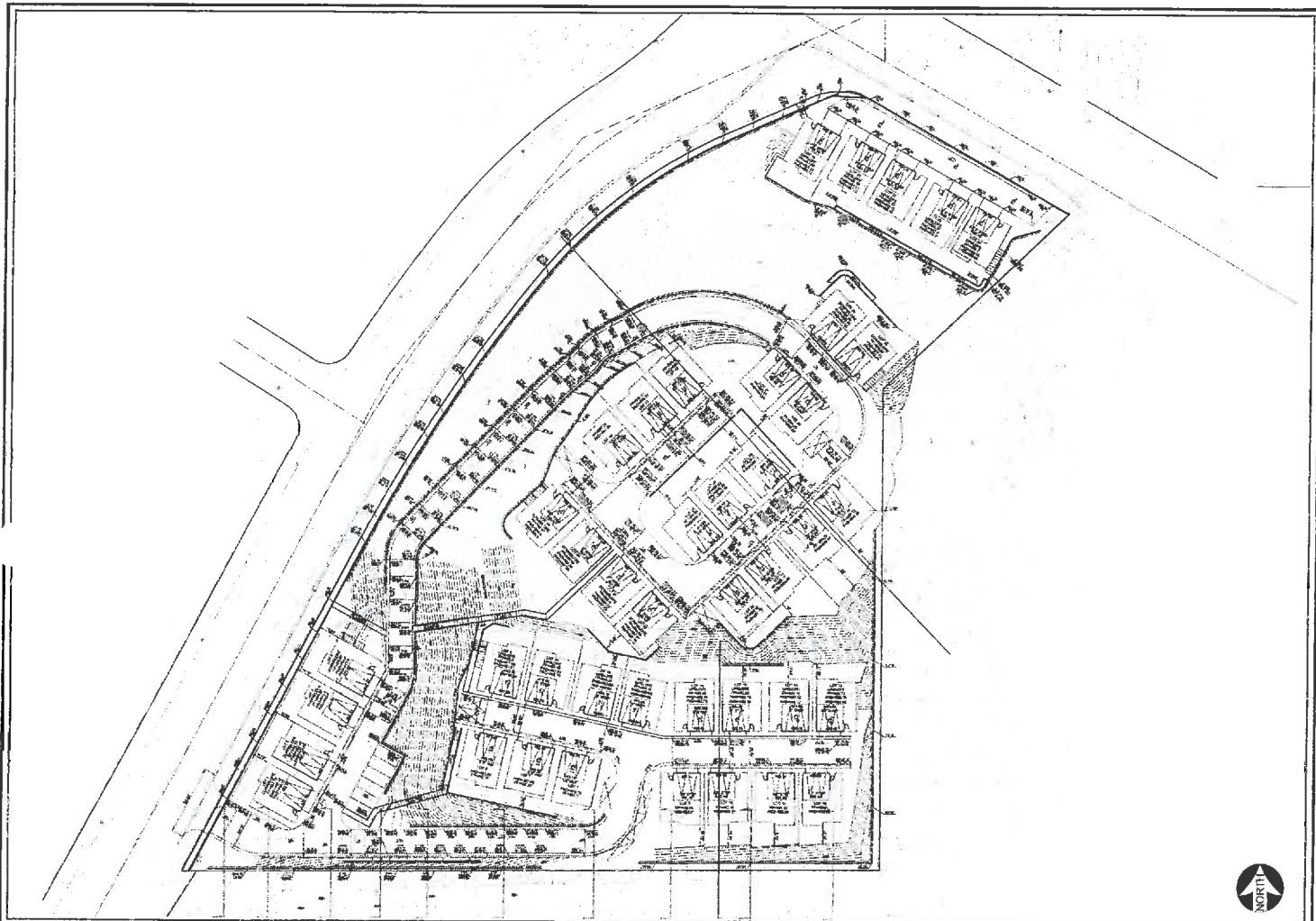
Background

In June 2016, the City prepared and circulated an IS/MND for development of the Project site with a previous version of the Project. The previous version of the Project included development of the same land uses (single-family residential) and the same number of dwelling units (42 homes), but in a different configuration than what is currently proposed. The previous version of the Project (shown on Figure I-1) would have required the removal of 68 protected trees (of the 102 protected trees on the Project site) and 64 non-protected trees (of the 72 non-protected trees on the site), for a total removal of 132 trees. Comments on the June IS/MND submitted to the City primarily raised concerns about removal of existing trees from the Project site, particularly removal of protected southern black walnut trees (*Juglans californica*). Other prevailing concerns expressed by commenters included the loss of walnut woodland habitat and traffic impacts.

As a result of these concerns, the Project Applicant revised the configuration of the Project to substantially reduce the need for removal of existing trees. The new Project configuration (refer to Figure II-7 in Section II, Project Description) would require the removal of 37 to 39 protected trees (a reduction of approximately 29 trees) and 59 non-protected trees (a reduction of 18 trees), which is a reduction of 6 total trees when compared to the previous version of the Project. As with the previous version of the Project, all trees removed from the Project site would be required by the City to be replaced at a ratio and size specified by the City (i.e., 4:1 for protected trees and 1:1 for non-protected trees).

The change in the configuration of the Project would result in an increase in cut soil at the Project site and export from the Project site than what was considered in the June IS/MND.

The City has updated the June IS/MND to reflect the Project's reconfiguration, tree removal reduction, and increase in soil export during the construction phase. Also, the IS/MND incorporates a Biological Resources Report prepared for the Project that addresses the issue of walnut woodlands, and a new Traffic Study that reflects existing traffic conditions within the Project site area.



Project Information

Project Title: El Sereno Project

Project Location: 2520, 2532, 2608, 2668 North Eastern Avenue and 2647, 2649, 2651 Lombardy Boulevard, Los Angeles, California, 90032

Lead Agency: City of Los Angeles Department of City Planning

Contact Person: Gregory Shoop, City Planner
GregShoop@lacity.org

Organization of Initial Study

This Draft Initial Study is organized into six sections as follows:

Introduction: This section provides introductory information such as the Project title, the Project Applicant, and the Lead Agency for the Project.

Project Description: This section provides a detailed description of the environmental setting and the Project, including Project characteristics and environmental setting.

Initial Study Checklist: This section contains the completed Initial Study Checklist.

Environmental Impact Analysis: Each environmental issue identified in the Initial Study Checklist contains an assessment and discussion of impacts associated with each subject area. When the evaluation identifies potentially significant effects, as identified in the Checklist, mitigation measures are provided to reduce such impacts to less-than-significant levels.

Preparers of Initial Study and Persons Consulted: This section provides a list of City personnel, other governmental agencies, and consultant team members that participated in the preparation of the Initial Study.

II. PROJECT DESCRIPTION

ENVIRONMENTAL SETTING

The Project site is located in the Northeast Los Angeles Community Plan Area of the City of Los Angeles (the “City”) (refer to Figures II-1 and II-2). Views of the Project site are shown on Figure II-3. Specifically, the Project site includes three contiguous lots totaling approximately 218,270 square feet, located at the south corner of Eastern Avenue and Lombardy Boulevard. The Project site is bound by North Eastern Avenue on the west/northwest, Lombardy Boulevard on the north, and single-family residential land uses on the northeast, east, and south. The topography of the Project site is hilly, with elevations ranging from approximately 440 to approximately 530 feet above sea level (asl). The Project site is an infill site that is not currently developed with any structures. The Project site’s assessor parcel numbers (APNs), zoning, land use designation, and parcel sizes are listed on Table II-1. As shown, the Project site is zoned [Q]R1-1D (Qualified Condition, One-Family Zone, Height District 1) and [Q]RD6-1D (Qualified Condition, Restricted Density Multiple Dwelling Zone, Height District 1). The land use designation for the Project site is Low Residential. The existing zoning and land use designation for the Project site are shown on Figures II-4 and II-5, respectively.

Existing land uses surrounding the Project site include single-family residential development to the north, east, and south; Farmdale Elementary School to the northwest; and El Sereno Recreation Center to the west. Views of the surrounding area are shown on Figure II-6. The existing zoning and land use designation of the area surrounding the Project site are shown on Figures II-4 and II-5, respectively.

PROJECT CHARACTERISTICS

The Project includes development of the Project site with 42 single-family residential homes, one home per parcel (refer to Figures II-7 through II-25). Each house would have 3-4 bedrooms and a 2-car garage. The homes would range in size from approximately 1,729 square feet to 2,279 square feet. Of the 102 protected trees located on the Project site, 37 to 39 protected trees would be removed. Of the 72 non-protected trees located on the Project site, 13 would be retained in place, and 59 would be removed. All trees would be replaced in accordance with City tree replacement requirements. The Project would include a cistern system that would capture rainwater to use for landscape irrigation. Development of the Project would require the export of approximately 78,000 cubic yards of soil.

Figure II-1 Vicinity Map

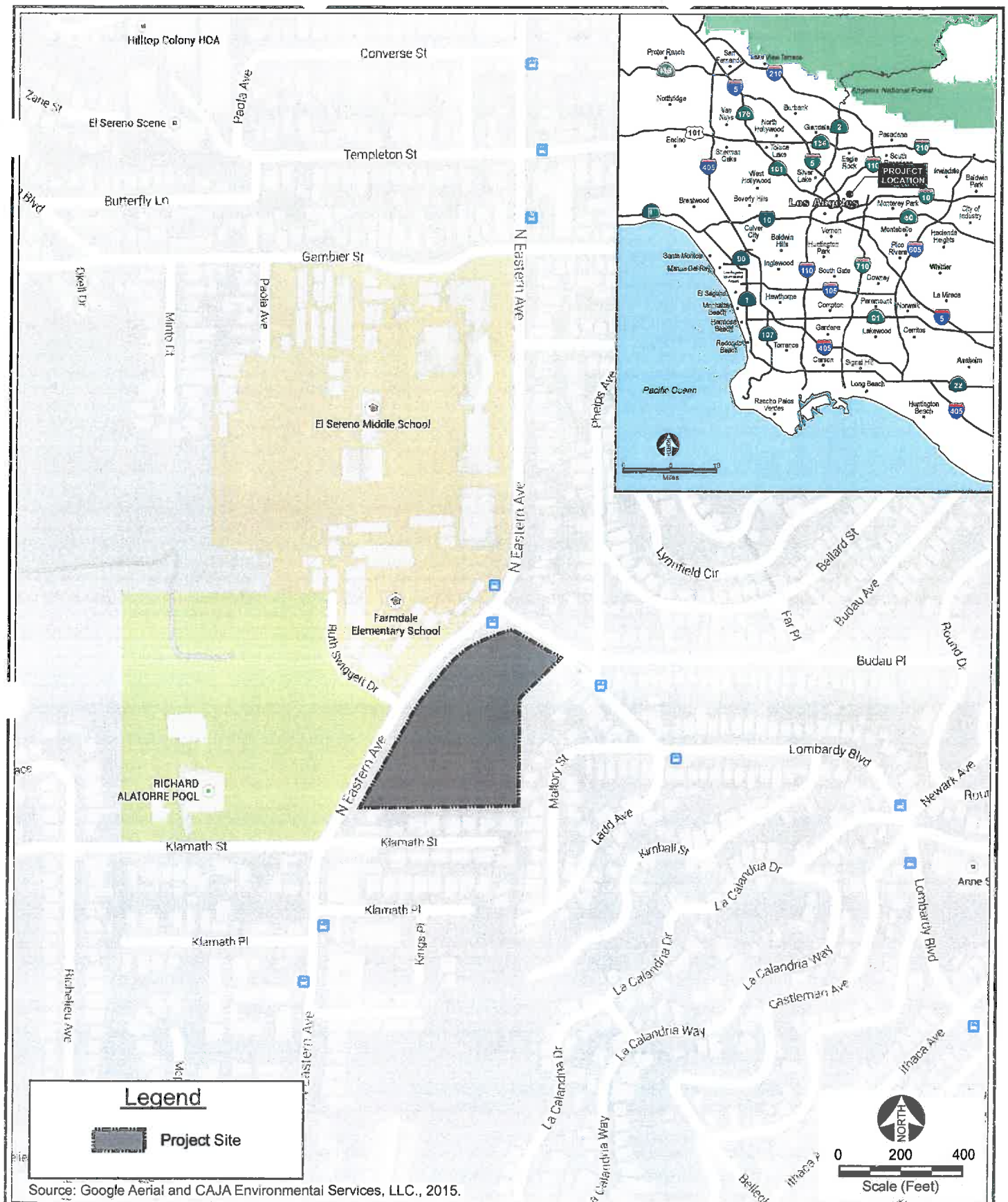


Figure II-1
Vicinity Map





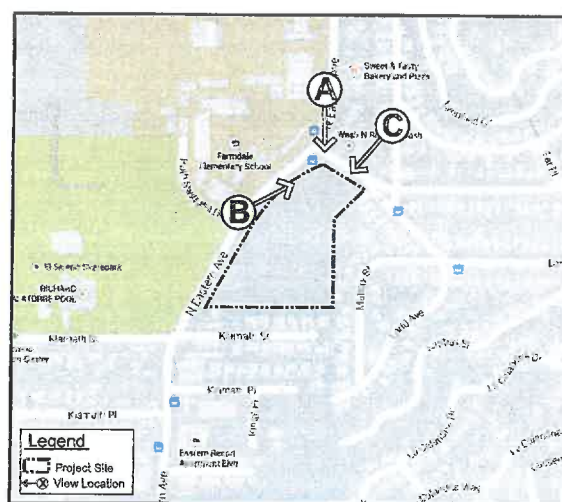
Photo A: View looking toward the south of the Project site.



Photo B: View looking toward the northeast of the Project site.



Photo C: View toward the southwest of the Project site.



View Location Map

Figure II-3
Views of the Project Site

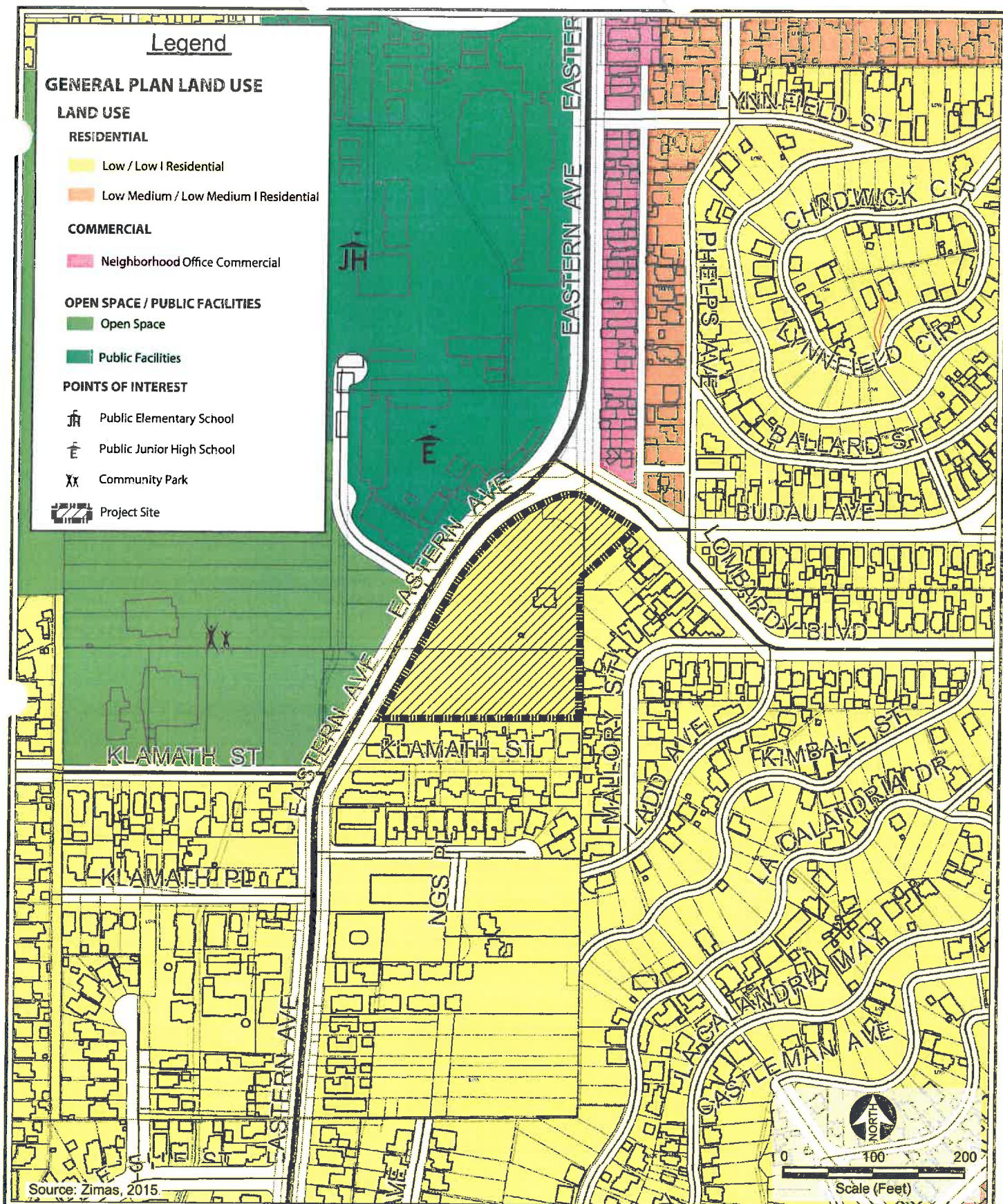




Photo A: View of the park located to the west of the Project site.



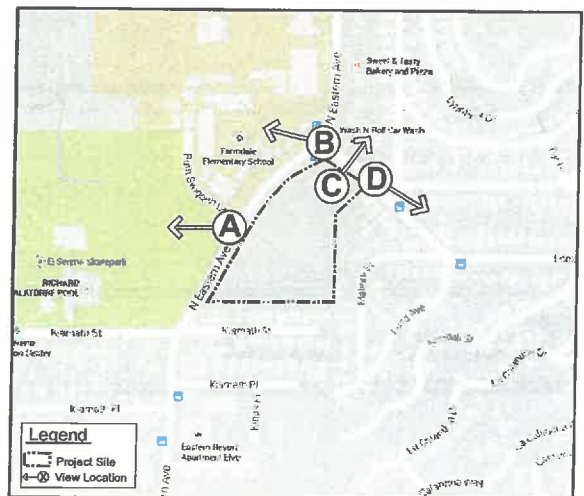
Photo B: View of the school located to the northwest of the Project site.



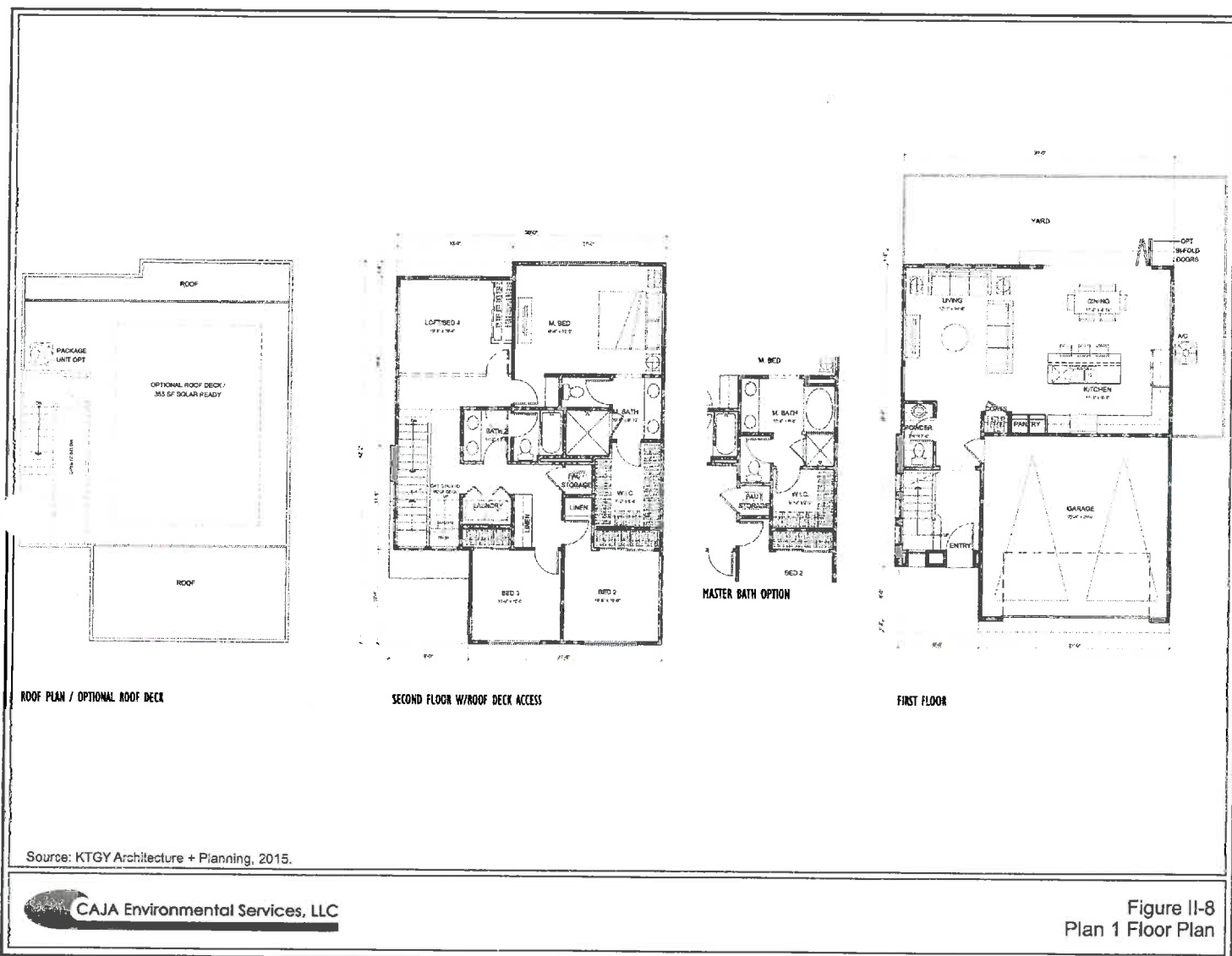
Photo C: View of the mix of land uses located to the north of the Project site.

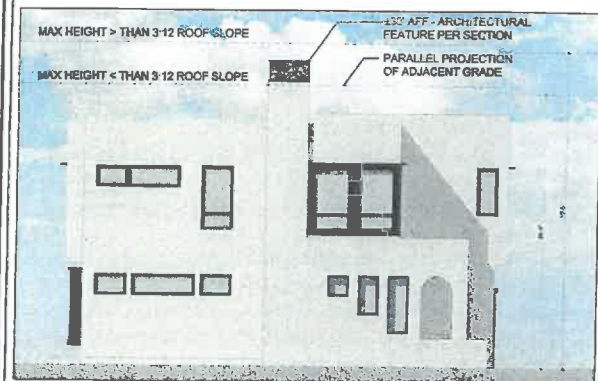


Photo D: View of the residential development located to the north and west of the Project site.

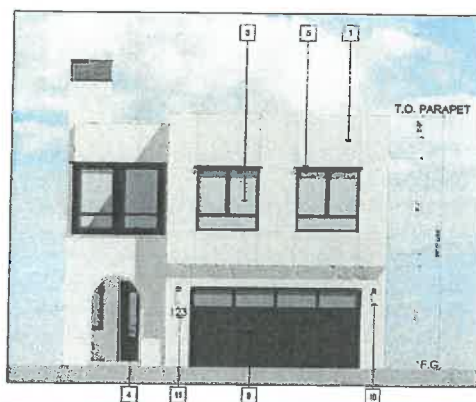


View Location Map

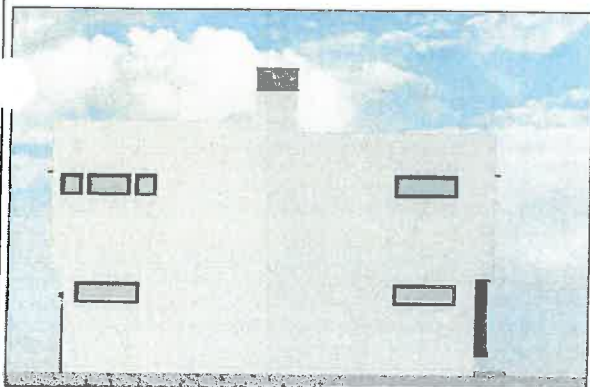




LEFT ELEVATION



FRONT ELEVATION



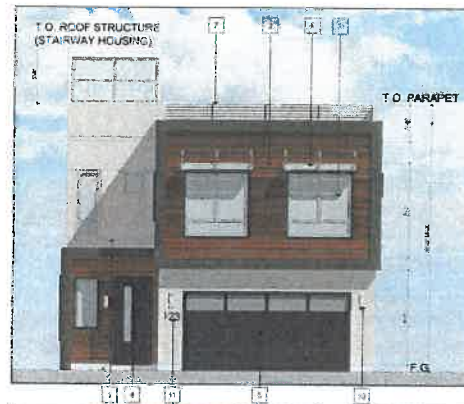
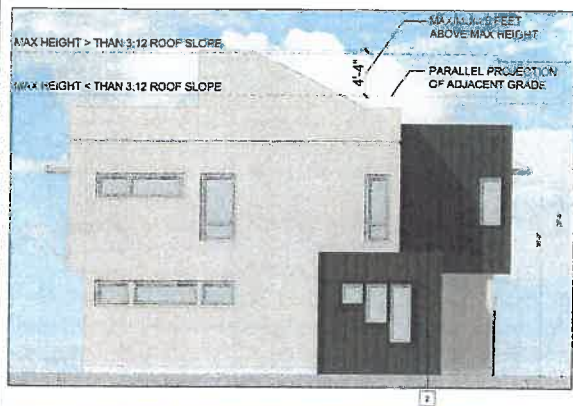
RIGHT ELEVATION



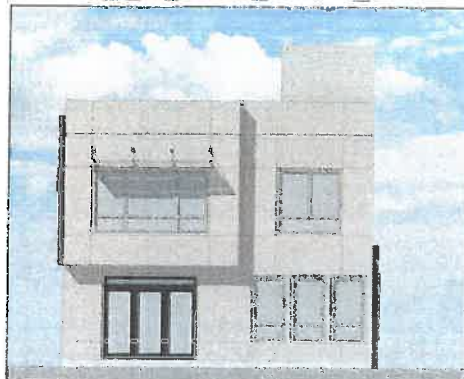
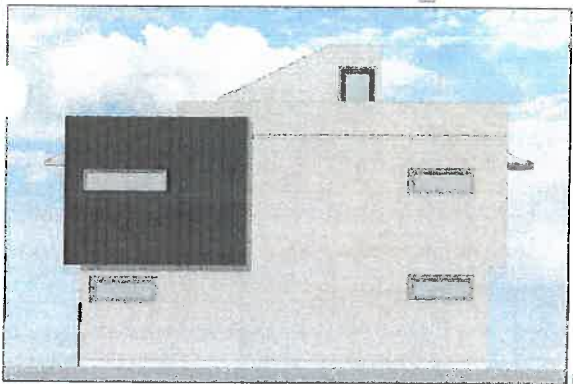
REAR ELEVATION

MATERIAL LEGEND	
1	EXPOSED LIGHT SAND FINISH
2	STANDARD GRADE CONCRETE BLOCK - (NOT LISTED)
3	WHITE BRICK
4	TRIMMED WHITE BRICK
5	ROOFING
6	WHITE BRICK
7	WHITE BRICK
8	WHITE BRICK
9	WHITE BRICK
10	WHITE BRICK
11	WHITE BRICK
12	WHITE BRICK
13	WHITE BRICK
14	WHITE BRICK
15	WHITE BRICK
16	WHITE BRICK
17	WHITE BRICK
18	WHITE BRICK
19	WHITE BRICK
20	WHITE BRICK

Source: KTG Architecture + Planning, 2015.



MATERIAL LEGEND	
1	BRICK (LEFT SIDE ONLY)
2	STONE (RIGHT SIDE ONLY)
3	WOOD (FLOORING)
4	WOOD (CEILING)
5	WOOD (WALLS)
6	WOOD (DOORS)
7	WOOD (WINDOWS)
8	WOOD (ROOFING)
9	WOOD (STAIRS)
10	WOOD (BENCHES)
11	WOOD (FURNITURE)
12	WOOD (LANDSCAPE)
13	WOOD (TREES)
14	WOOD (SHRUBS)
15	WOOD (GRASS)
16	WOOD (ROCKS)
17	WOOD (WATER)
18	WOOD (SKY)
19	WOOD (CLOUDS)
20	WOOD (MOON)
21	WOOD (SUN)
22	WOOD (STARS)
23	WOOD (PLANETS)
24	WOOD (GALAXIES)
25	WOOD (UNIVERSE)



Source: KTG Architecture + Planning, 2015.



Source: KTG Architecture + Planning, 2015.

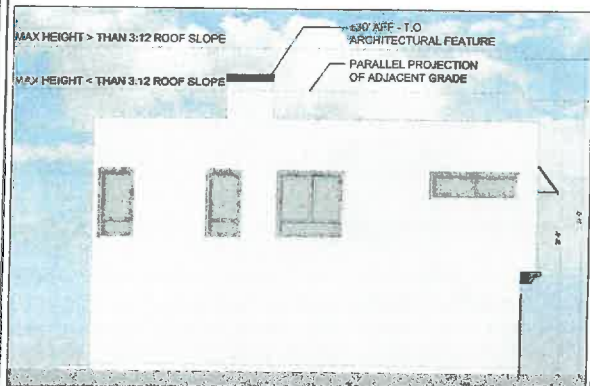


Figure II-11
Typical Street Scene (Lot 23-25)

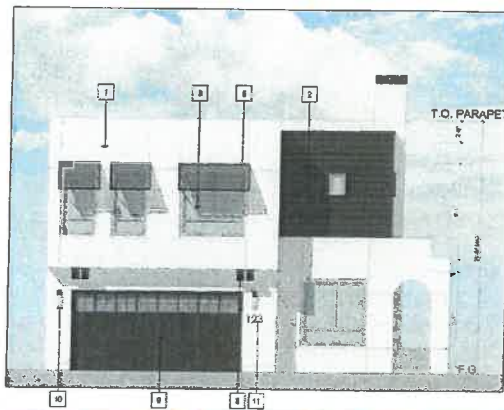


CAJA Environmental Services, LLC

Figure II-12
Plan 2 Floor Plan



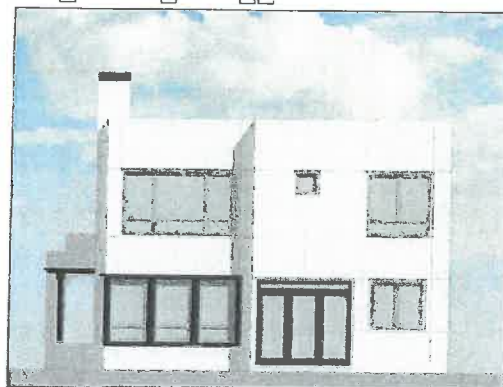
LEFT ELEVATION



FRONT ELEVATION



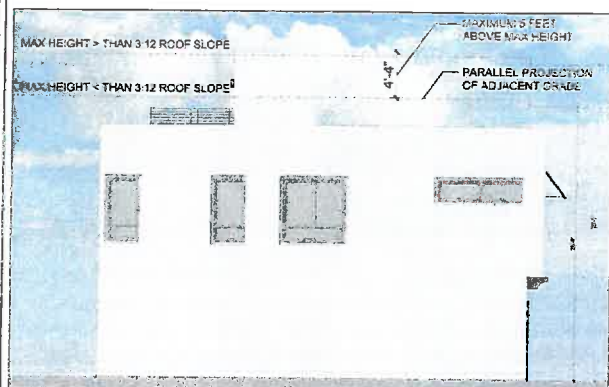
RIGHT ELEVATION



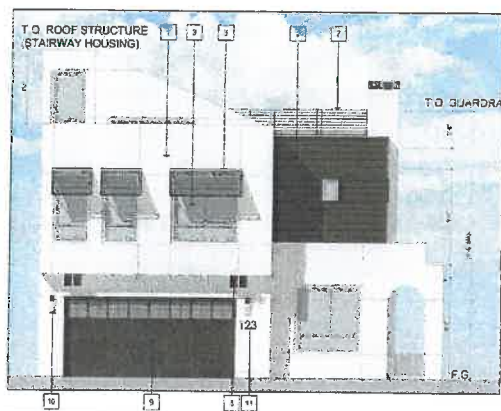
REAR ELEVATION

MATERIAL LEGEND	
1	STAINLESS STEEL PANELS
2	STAINLESS STEEL PANELS (CONT.)
3	WHITE STAINLESS
4	STAINLESS STEEL PANELS
5	STAINLESS STEEL PANELS (CONT.)
6	STAINLESS STEEL PANELS
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65	STAINLESS STEEL PANELS
66	STAINLESS STEEL PANELS
67	STAINLESS STEEL PANELS
68	STAINLESS STEEL PANELS
69	STAINLESS STEEL PANELS
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71	STAINLESS STEEL PANELS
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86	STAINLESS STEEL PANELS
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88	STAINLESS STEEL PANELS
89	STAINLESS STEEL PANELS
90	STAINLESS STEEL PANELS
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97	STAINLESS STEEL PANELS
98	STAINLESS STEEL PANELS
99	STAINLESS STEEL PANELS
100	STAINLESS STEEL PANELS

Source: KTG Architecture + Planning, 2015.

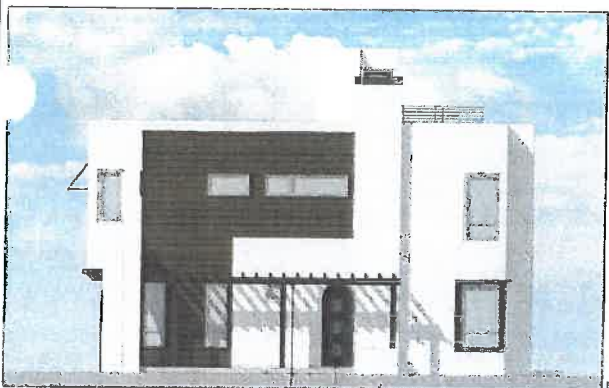


LEFT ELEVATION



FRONT ELEVATION

MATERIAL LEGEND	
1	BRICK LUMBER CLAD ROOF
2	BRICK LUMBER CLAD ROOF
3	WHITE SIDING
4	PAVED ASBESTOS ROOF
5	PAVED
6	METAL SIDING
7	METAL SIDING
8	OUTSIDING
9	METAL SIDING, DARK COLOR
10	BRICK LUMBER CLAD ROOF
11	BRICK LUMBER CLAD ROOF
12	BRICK LUMBER CLAD ROOF
13	BRICK LUMBER CLAD ROOF
14	BRICK LUMBER CLAD ROOF
15	BRICK LUMBER CLAD ROOF
16	BRICK LUMBER CLAD ROOF
17	BRICK LUMBER CLAD ROOF
18	BRICK LUMBER CLAD ROOF
19	BRICK LUMBER CLAD ROOF
20	BRICK LUMBER CLAD ROOF



RIGHT ELEVATION



REAR ELEVATION

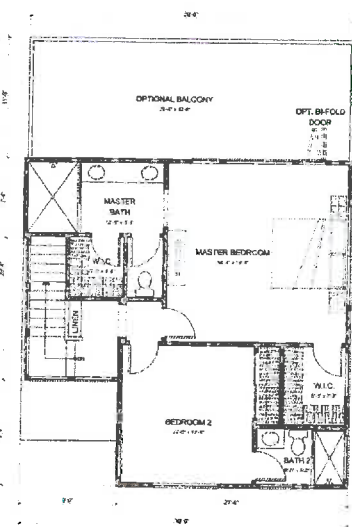
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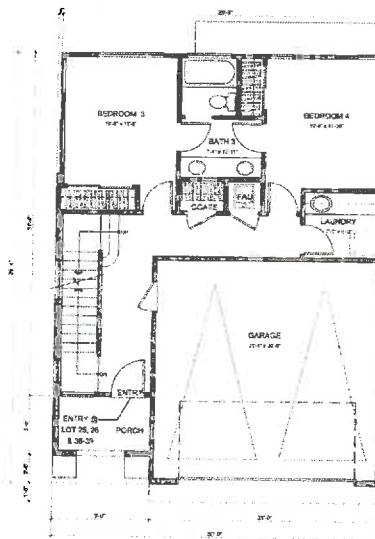
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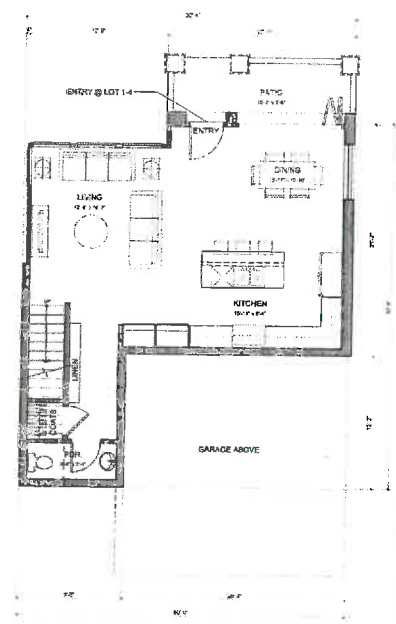
Figure II-15
Typical Perspective (Lot 17-19)



SECOND FLOOR



FIRST FLOOR

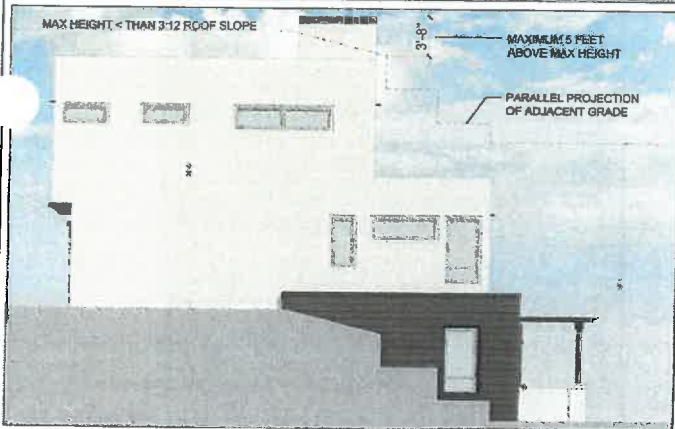


BASEMENT

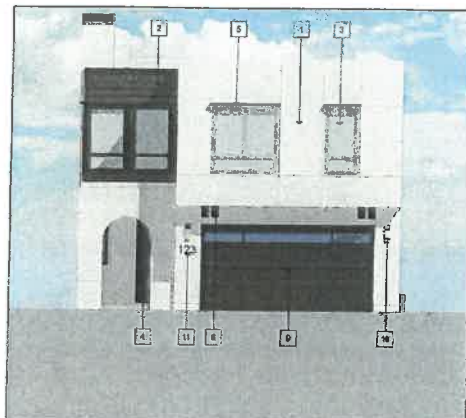
Source: KTG Architecture + Planning, 2015.



LEFT ELEVATION



RIGHT ELEVATION



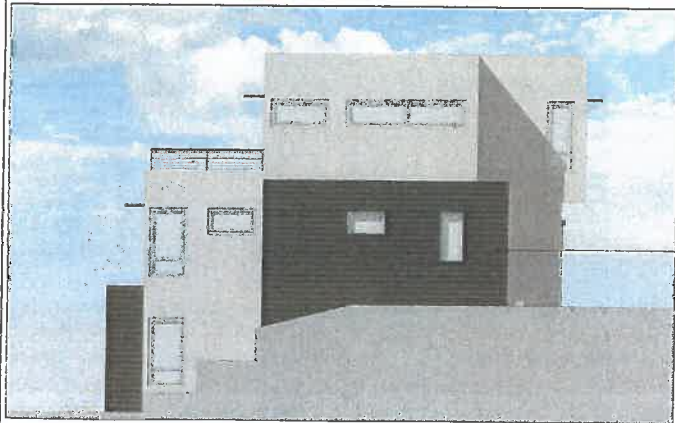
FRONT ELEVATION



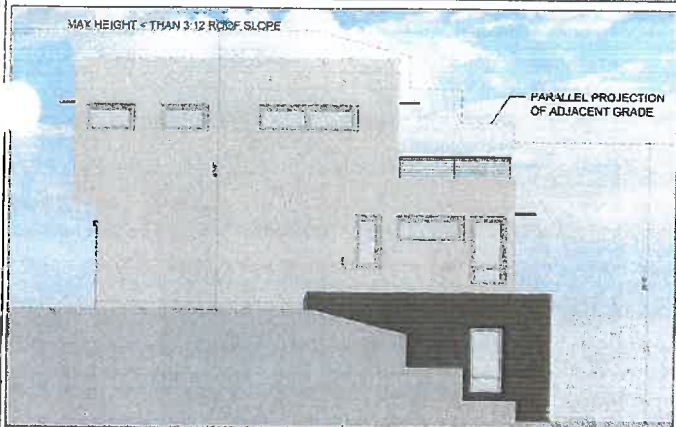
REAR ELEVATION

MATERIAL LEGEND	
1	STUCCO, LIGHT SAND PINK
2	STONE CHALK, WHITE CEMENT TONAL
3	WHITE SLATES
4	FRONTAL AND ENTRY DOOR
5	FRONTS
6	WHITE GRASS
7	WHITE GRASS
8	WHITE GRASS
9	WHITE GRASS
10	WHITE GRASS
11	WHITE GRASS
12	WHITE GRASS
13	WHITE GRASS
14	WHITE GRASS

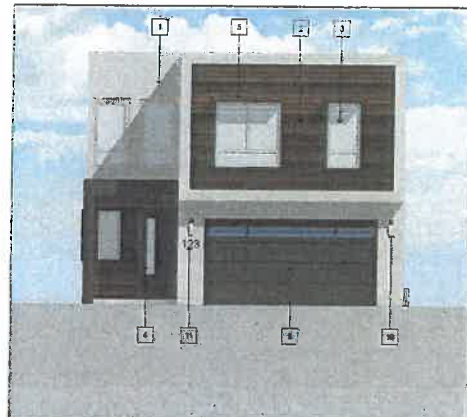
Source: KTG Architecture + Planning, 2015.



LEFT ELEVATION



RIGHT ELEVATION



FRONT ELEVATION

MATERIAL LEGEND	
1	STUCCO (LIGHT TONE/WHITE)
2	STUCCO (DARK TONE/BLACK)
3	STUCCO (MID TONE/GRAY)
4	STUCCO (DARK TONE/BLACK)
5	STUCCO (DARK TONE/BLACK)
6	STUCCO (DARK TONE/BLACK)
7	STUCCO (DARK TONE/BLACK)
8	STUCCO (DARK TONE/BLACK)
9	STUCCO (DARK TONE/BLACK)
10	STUCCO (DARK TONE/BLACK)



REAR ELEVATION

Source: KTG Architecture + Planning, 2015.

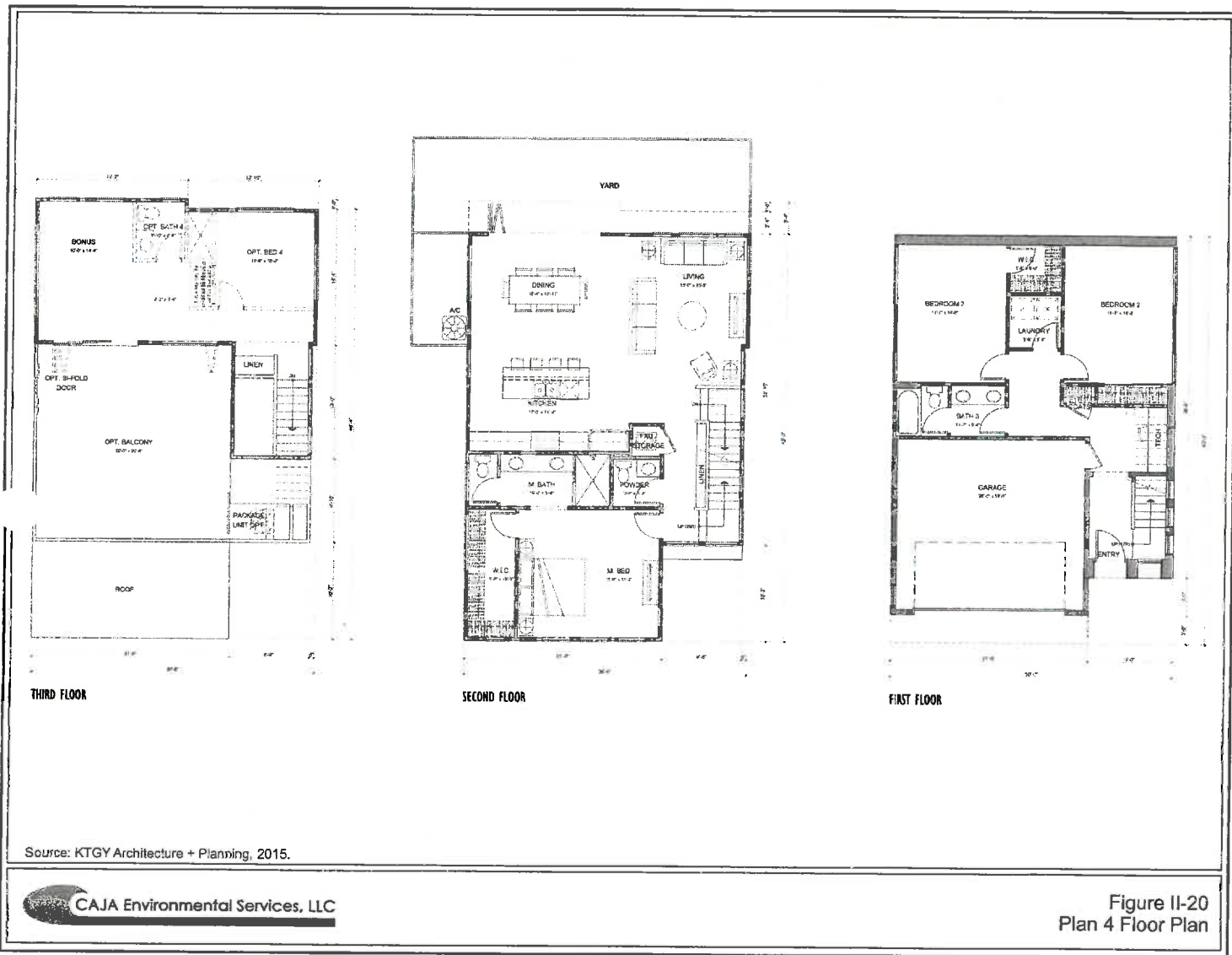


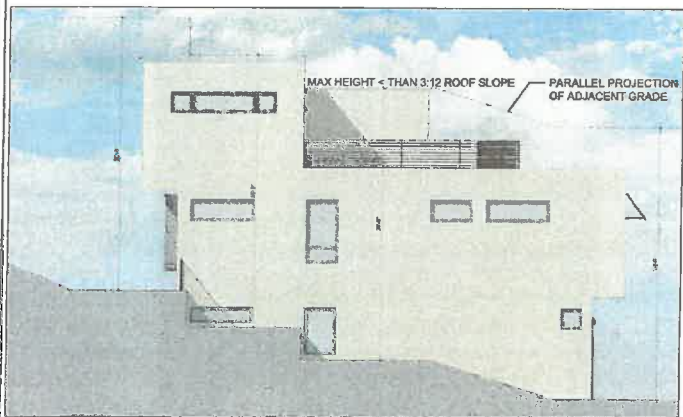
VIEW FROM N. EASTERN AVE.

Source: KTG Architecture + Planning, 2015.



Figure II-19
Typical Perspective (Lot 1-4)

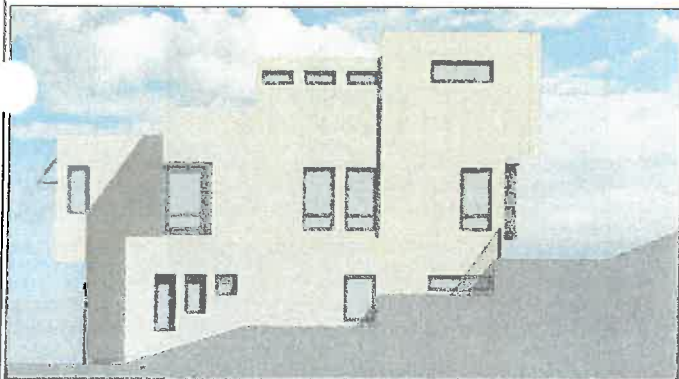




LEFT ELEVATION



FRONT ELEVATION



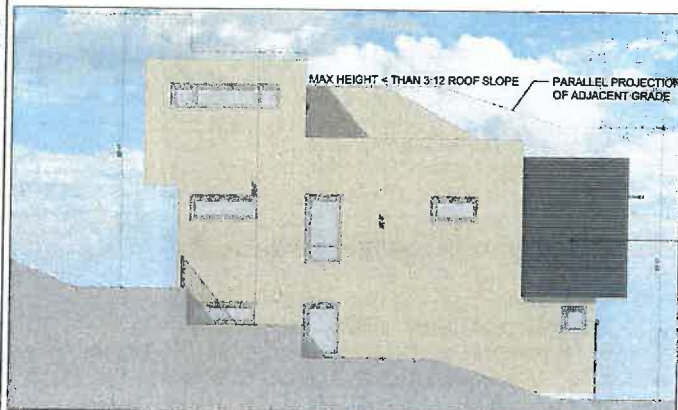
RIGHT ELEVATION



REAR ELEVATION

MATERIAL LEGEND	
1	BRICK (SEE SPEC)
2	STAINLESS STEEL CLAMP EDGE (SEE SPEC)
3	VERTICAL ALUMINUM
4	PERGOLA ENTRY DOOR
5	ROOFING
6	WALL CLADDING
7	WOOD CLADDING
8	OUTDOOR
9	WOOD CLADDING (SEE SPEC)
10	WOOD CLADDING (SEE SPEC)
11	WOOD CLADDING (SEE SPEC)
12	WOOD CLADDING (SEE SPEC)
13	WOOD CLADDING (SEE SPEC)
14	WOOD CLADDING (SEE SPEC)
15	WOOD CLADDING (SEE SPEC)
16	WOOD CLADDING (SEE SPEC)
17	WOOD CLADDING (SEE SPEC)
18	WOOD CLADDING (SEE SPEC)
19	WOOD CLADDING (SEE SPEC)
20	WOOD CLADDING (SEE SPEC)

Source: KTG Architecture + Planning, 2015.



LEFT ELEVATION



FRONT ELEVATION

MATERIAL LEGEND	
1	WHITE GRAY CONCRETE
2	GRAY GRAY CONCRETE
3	WHITE GRAY CONCRETE
4	WHITE GRAY CONCRETE
5	WHITE GRAY CONCRETE
6	WHITE GRAY CONCRETE
7	WHITE GRAY CONCRETE
8	WHITE GRAY CONCRETE
9	WHITE GRAY CONCRETE
10	WHITE GRAY CONCRETE
11	WHITE GRAY CONCRETE
12	WHITE GRAY CONCRETE
13	WHITE GRAY CONCRETE
14	WHITE GRAY CONCRETE
15	WHITE GRAY CONCRETE
16	WHITE GRAY CONCRETE
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18	WHITE GRAY CONCRETE
19	WHITE GRAY CONCRETE
20	WHITE GRAY CONCRETE

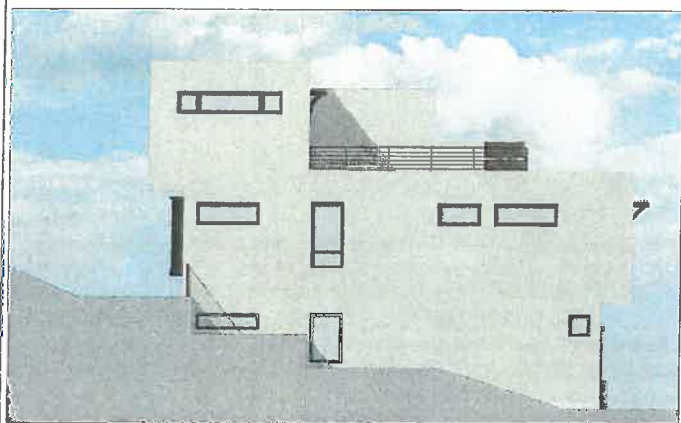


RIGHT ELEVATION



REAR ELEVATION

Source: KTG Architecture + Planning, 2015.

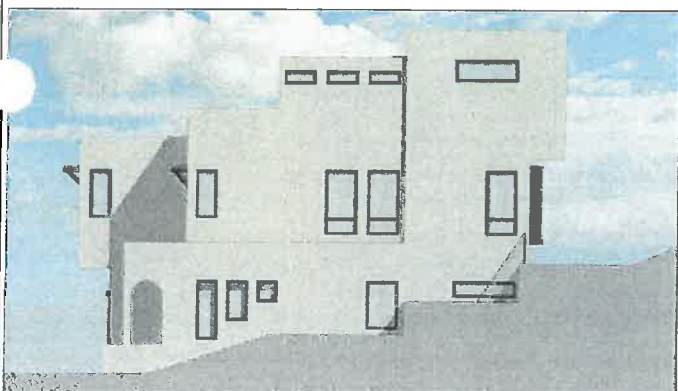


LEFT ELEVATION

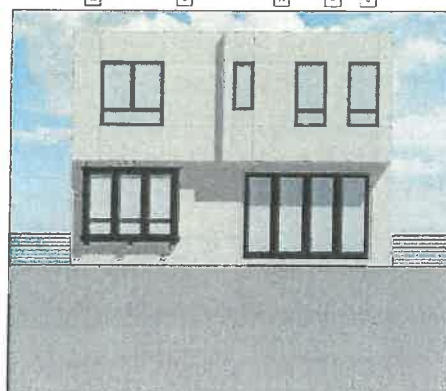


FRONT ELEVATION

MATERIAL LEGEND	
1	STREET LIGHT LAMP POUCH
2	STAIR CASE FLOOR (2ND FLOOR)
3	WALL GLASS
4	ROOFGLASS (2ND FLOOR)
5	WOOD
6	WALL BRICK
7	WALL BRICK
8	OUTDOOR
9	WALL BRICK (2ND FLOOR)
10	WOOD (2ND FLOOR)
11	WOOD (2ND FLOOR)
12	WOOD (2ND FLOOR)
13	WOOD (2ND FLOOR)
14	WOOD (2ND FLOOR)
15	WOOD (2ND FLOOR)
16	WOOD (2ND FLOOR)
17	WOOD (2ND FLOOR)
18	WOOD (2ND FLOOR)
19	WOOD (2ND FLOOR)
20	WOOD (2ND FLOOR)



RIGHT ELEVATION



REAR ELEVATION

Source: KTG Architecture + Planning, 2015.



CORNER OF LOMBARD BLVD. AND EASTERN AVE.

Source: KTG Architecture + Planning, 2015.



Figure II-24
Typical Perspective (Lot 38-42)



VIEW FROM LOMBARDY BLVD.

Source: KTG Architecture + Planning, 2015.



Figure II-25
Typical Perspective (Lot 39-42)

Table II-1
Project Site Information

Addresses	APN	Zoning Information	General Plan Land Use Designation
2520, 2532 North Eastern Avenue	5216008016	[Q]R1-1D [Q]RD6-1D	Low Residential
2608 North Eastern Avenue	5216008034	Z1-2129 East Los Angeles State Enterprise Zone	
2668 North Eastern Avenue	5216009037	ZI-2399 Northeast Los Angeles Hillsides	
2647, 2649, 2651 Lombardy Boulevard			
Source: Zone Information & Map Access System (ZIMAS): http://zimas.lacity.org , February 20, 2015.			

REQUESTED DISCRETIONARY ACTIONS

In order to implement the Project, the Project Applicant is requesting approval of the following discretionary actions from the City:

- **Vesting Tract Map (VTT) for Small Lot Purposes per LAMC Section 17.03** – Request is for the Advisory Agency to grant (1) a Vesting Tentative Tract Map to create 42 single-family lots in accordance with the Small Lot Subdivision Ordinance No. 176,354 in the Northeast Los Angeles Community Plan.
- **Tree Removal Permit** – Request is authorization from the Board of Public Works or the Advisory Agency to allow for the removal of up to 39 protected trees pursuant to LAMC Section 17.05R1(b).
- **Vesting Zone Change (ZC) per LAMC Section 12.32** – Request to permit a change of zone from [Q]R1-1D and [Q] RD6-1D to (T)(Q)RD5-1D.
- **Zoning Administrator's Determination (ZAD) per LAMC Section 12.24 X.26** – Request is to allow 54 walls varying in height from 3.5 feet to 6.0 feet in lieu of the maximum of 2 10-foot retaining walls otherwise required in LAMC Section 12.21 C.8(a).
- **Haul Route Approval** from the Board of Building and Safety Commission or Advisory Agency.

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IV. ENVIRONMENTAL IMPACT ANALYSIS

1. AESTHETICS

a) Would the project have a substantial adverse effect on a scenic vista?

Less Than Significant Impact. Views from the Project site and immediate area primarily include those of the mix of urban land uses that define the area, including single- and multi-family residential development, a school, a park, commercial land uses, and roadway and utility infrastructure. Intermittent views of the San Gabriel Mountains are available to the north of the Project area, but these views are limited by existing terrain and development. Scenic vistas are available from public trails atop the hill toward the west of the Project site associated with the Ascot Hills Park, which has a peak elevation of approximately 770 feet above sea level. However, the Project site is lower in elevation than this location. The roof of the house developed at the highest elevation on the Project site would reach approximately 543 feet above sea level. As such, the Project would not occlude scenic views available from the park. Therefore, Project impacts related to scenic vistas would be less than significant.

b) Would the project substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?

Less Than Significant With Mitigation Incorporated. No scenic highways are located in proximity to the Project site. No rock outcroppings or historic buildings are located on the Project site. A Tree Preservation Report was prepared for the Project that identified 102 protected trees measuring 4-inch trunk diameter or larger and 72 non-protected measuring 8-inch trunk diameter or larger (refer to Appendix A).¹ Of the 102 protected trees, 63 to 65 would be retained in place, depending on health and construction impacts. Thirty-seven to 39 protected trees would be removed.² Of the 72 non-protected trees, 13 would be retained in place, and 59 would be removed. However, as required by the City of Los Angeles (the "City") and as outlined in Mitigation Measures 1-1 and 1-2, the removed non-protected trees would be replaced on the Project site at a 1:1 ratio, and the removed protected trees would be replaced on the Project site at a 4:1 ratio, respectively. This 4:1 replacement mitigation requirement is double the minimum 2:1 replacement requirement set forth in the City's Protected Tree Ordinance (LAMC Section 17.05R[4][a]). Pursuant to the City's Protected Tree Ordinance the trees planted to replace the removed

¹ Tree Preservation Report, Arborgate Consulting, Inc., January 3, 2017. Refer to Appendix A.

² According to the Tree Survey, 37 protected trees must be removed. However, two additional protected trees (for a total of 39 protected trees) may also need to be removed due to their poor health prior to construction. Although all efforts to save these two trees will be made by the Applicant prior to construction, because the survival of these two trees cannot be assured at this time, the IS/MND conservatively assumes the removal of a maximum of 39 protected trees.

protected trees on a 4:1 ratio shall be (1) a protected variety, and (2) at least 15 gallons in size or larger measuring one inch or more in diameter at a point one foot above the base, and not less than seven feet in height. Additionally, prior to preparation of the Tree Survey, 8 qualifying protected trees were removed by a prior owner without the benefit of a tree removal permit. The replacement requirements for the 8 qualifying trees and the 37 to 39 protected trees that would be removed as part of the Project would be one *Juglans californica* and three *Quercus* for each of the removed protected trees. Therefore, with implementation of these mitigation measures, the Project would not result in any significant impacts related to trees.

c) Would the project substantially degrade the existing visual character or quality of the site and its surroundings?

Less than Significant Impact. The Project site area is characterized by a mix of urban/sub-urban land uses, including single-family residential to the northeast, east, and south; a park to the west; a school to the northwest; and commercial to the north along Eastern Avenue. The Project includes development of 42 single-family homes, similar to those adjacent to the Project site and within the area. The size, height, and massing of the homes would comply with all Los Angeles Municipal Code (the "LAMC") requirements. Also, the design of the proposed homes would be required to comply with the City's design requirements (i.e., height, building materials, landscaping, etc.). The homes would be developed to accommodate the topography of the Project site, stepping up or down the hillside where necessary to minimize the amount of grading needed at the site and the change to the existing topography of the site, and to maximize the preservation of existing protected tree species. The visual character of the Project would conform to the visual character of the surrounding area. Therefore, no Project impacts related to visual character would occur.

d) Would the project create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?

Less Than Significant Impact. The Project site is located in the Northeast Los Angeles Community Plan area of the City that is developed with urban land uses, including single- and multi-family residential development, a school, a park, commercial land uses, and roadway and utility infrastructure, all of which produce light and glare (e.g., indoor/outdoor lighting, windows, light-colored surfaces, etc.) typical of such urban uses in the City. The Project site is an infill site that is currently not developed with any structures and does not include any sources of light or glare.

The Project would include interior and exterior lighting that complies with the LAMC to minimize the effect of the new sources of lighting that would be introduced. The Project would not include sources of nighttime illumination that would adversely affect nighttime views in the area, and no spill-over lighting would occur. Specifically, LAMC Section 91.6205 requires that new lighting sources not exceed 1 foot-candle of new light spillover at residential property lines. Also, the Project would be required to use non-reflective glass, pursuant to LAMC Section 93.0117. For these reasons, the Project would not create a

new source of substantial light or glare which would adversely affect day or nighttime views in the area. Therefore, Project impacts related to light and glare would be less than significant.

Mitigation Measures (Aesthetics)

1-1: Non-Protected Trees

- Prior to issuance of any permit related to development of the Project, a plot plan shall be prepared for the Project, indicating the location, size, type, and general condition of all existing trees on the Project site and within the adjacent public right(s)-of-way.
- All significant (8-inch or greater trunk diameter, or cumulative trunk diameter if multi-trunked, as measured 54 inches above the ground) non-protected trees on the Project site proposed for removal shall be replaced at a 1:1 ratio with a minimum 24-inch box tree. Net new trees located within the parkway of the adjacent public-right(s)-of-way may be counted toward replacement tree requirements.
- Removal or planting of any tree in the public right-of-way shall require approval of the Board of Public Works. All trees in the public right-of-way shall be provided in the current standards of the Urban Forestry Division of the Department of Public Works, Bureau of Street Services.

1-2 Protected Trees

- All protected tree removals shall require approval from the Board of Public Works.
- A Tree Report shall be submitted to the Urban Forestry Division of the Bureau of Street Services, Department of Public Works, for review and approval prior to implementation of the Report's recommended measures.
- According to the City's Protected Tree Ordinance, a minimum of four protected trees (a minimum of 15 gallon in size) shall be planted for each protected tree that is removed.. The size of each replacement tree shall measure at least one inch or more in diameter at a point one foot above the base, and not less than seven feet in height, measured from the base.
- In consultation with the Division of Urban Forestry, twenty-five percent of the protected trees removed shall be replaced with 15-gallon *Juglans californica*.
- The location of the trees planted for the purposes of replacing a removed protected tree shall be clearly indicated on the required landscape plan, which shall also

indicate the replacement tree species and further contain the phrase "Replacement Tree" in its description.

1-3 *Previously Removed Trees*

- The 8 qualifying previously removed protected trees shall be replaced at a ratio of one *Juglans californica* and three *Quercus* for each of the 8 trees, at a minimum of 15 gallon in size. The specific size and species of the trees to be planted as replacement for the protected trees being removed shall be determined by the Urban Forestry Division.

1-4 *All Trees*

- **Protection Barrier:** A protection barrier shall be installed around the construction area as shown on the map included in the Tree Preservation Report (refer to Appendix A). The barrier shall be 6-foot-high chain-link fencing. Twelve-inch-high silt fence shall be attached to the base of the fence with the bottom edge buried 1-2 inches. The barrier may be placed on the line shown on the map or closer to construction, but not further. The fencing shall be maintained in good repair throughout the duration of the Project, and shall not be removed, relocated, or encroached upon without permission of the arborist involved.
- **Storage of Materials:** There shall be NO storage of materials or supplies of any kind inside the area of the protection fencing. Concrete and cement materials, block, sand and soil shall not be placed within the drip-line of any tree to remain.
- **Fuel Storage:** Fuel storage shall NOT be permitted within 150 feet of any tree to be preserved. Refueling, servicing and maintenance of equipment and machinery shall NOT be permitted within 150 feet of protected trees.
- **Debris and Waste Materials:** Debris and waste from construction or other activities shall NOT be permitted outside the construction area. Wash down of concrete or cement handling equipment, in particular, shall NOT be permitted within 150 feet of protected trees.
- **Planting near Trees Designated for Protection:** Any digging within designated protection zones shall be done using supersonic air directly as the digging medium, by means of a nozzle, whose nominal rated input pressure (available from manufacturer's literature) must not exceed 130 psig (pounds per square inch at gage) unless otherwise approved. Nozzles designed for input above 130 psig can damage fine roots. Air compressors rated between 100 to 125 psig are recommended.

- **Grade Changes:** Any grade changes within the protection radius listed should be approved by a Registered Consulting Arborist before construction begins, and precautions taken to mitigate potential injuries. Grade changes can be particularly damaging to trees. Even as little as two inches of fill can cause the death of a tree. Lowering the grade can destroy major portions of a root system.
- **Damages:** Any tree damages or injuries should be reported to the project arborist as soon as possible. Severed roots shall be cut cleanly to healthy tissue, using proper pruning tools. Broken branches or limbs shall be pruned according to International Society of Arboriculture Pruning Guidelines and ANSI A-300 Pruning Standards.
- **Preventive Measures:** Pruning of the tree canopies and branches should be done at the direction of the project arborist to remove any dead or broken branches, and to provide any necessary clearances for the construction work or equipment.

Cumulative Impacts

None of the related projects listed on Table IV-38 and shown on Figure IV-12 included later in this section are located within visual proximity of the Project. As such, the combination of the Project with the related projects would not result in any cumulative aesthetic impacts. The closest related project is located approximately one mile north of the Project site at 5479 East Huntington Drive. All new development in the City is required to comply with LAMC requirements referenced previously related to light and glare. Therefore, cumulative impacts related to aesthetics would be less than significant.

2. AGRICULTURE AND FORESTRY RESOURCES

a) Would the project convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?

No Impact. The Extent of Important Farmland Map Coverage maintained by the Division of Land Protection indicates that the Project site is not included in the Important Farmland category.³ Therefore, the Project would not convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland) to non-agricultural use, as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency. No impacts would occur.

³ State of California Department of Conservation, Division of Land Resource Protection, *Farmland Mapping and Monitoring Program, Los Angeles County Important Farmland, 1998.*

b) Would the project conflict with existing zoning for agricultural use, or a Williamson Act Contract?

No Impact. The Project site is not zoned for agricultural use, and the site is not under Williamson Act Contract.⁴ Thus, the Project would not conflict with existing zoning for agricultural use, or a Williamson Act Contract. Therefore, no impacts related to this issue would occur.

c) Would the project conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220[g]), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104 [g])?

No Impact. The Project site is not zoned as forest land or timberland. Therefore, no impacts related to this issue would occur.

d) Would the project result in the loss of forest land or conversion of forest land to non-forest use?

No Impact. The Project site does not contain any forest land. Therefore, no impacts related to this issue would occur.

e) Would the project involve other changes in the existing environment, which due to their location or nature, could result in conversion of Farmland, to non-agricultural use?

No Impact. The Project site and surrounding area are developed with single- and multi-family residential land uses. No agricultural uses are located on the Project site or within the area. Therefore, no impacts related to this issue would occur.

Cumulative Impacts

Neither the Project site nor any of the related projects' sites are used or designated as agricultural land or forest land. Therefore, no cumulative impacts related to agricultural resources would occur.

3. AIR QUALITY

The information below is based on the Air Quality modeling results prepared by DKA Planning (refer to Appendix B).

⁴ *Ibid.*

Pollutants and Effects

Criteria air pollutants are defined as pollutants for which the federal and state governments have established ambient air quality standards for outdoor concentrations. The federal and state standards have been set at levels above which concentrations could be harmful to human health and welfare. These standards are designed to protect the most sensitive persons from illness or discomfort. Pollutants of concern include carbon monoxide (CO), ozone (O₃), nitrogen dioxide (NO₂), sulfur dioxide (SO₂), particulate matter 2.5 microns or less in diameter (PM_{2.5}), particulate matter ten microns or less in diameter (PM₁₀), and lead (Pb). These pollutants are discussed below.

- Carbon Monoxide (CO) is a colorless and odorless gas formed by the incomplete combustion of fossil fuels. It is emitted almost exclusively from motor vehicles, power plants, refineries, industrial boilers, ships, aircraft, and trains. In urban areas, automobile exhaust accounts for the majority of emissions. CO is a non-reactive air pollutant that dissipates relatively quickly, so ambient concentrations generally follow the spatial and temporal distributions of vehicular traffic. Concentrations are influenced by local meteorological conditions, primarily wind speed, topography, and atmospheric stability. CO from motor vehicle exhaust can become locally concentrated when surface-based temperature inversions are combined with calm atmospheric conditions, a typical situation at dusk in urban areas between November and February.⁵ The highest concentrations occur during the colder months of the year when inversion conditions are more frequent. CO is a health concern because it competes with oxygen, often replacing it in the blood and reducing the blood's ability to transport oxygen to vital organs. Excess CO exposure can lead to dizziness, fatigue, and impair central nervous system functions.
- Ozone (O₃) is a colorless gas that is formed in the atmosphere when reactive organic gases (ROG) and nitrogen oxides (NO_x) react in the presence of ultraviolet sunlight. O₃ is not a primary pollutant; rather, it is a secondary pollutant formed by complex interactions of two pollutants directly emitted into the atmosphere. The primary sources of ROG and NO_x, the components of O₃, are automobile exhaust and industrial sources. Meteorology and terrain play major roles in O₃ formation. Ideal conditions occur during summer and early autumn, on days with low wind speeds or stagnant air, warm temperatures, and cloudless skies. The greatest source of smog-producing gases is the automobile. Short-term exposure (lasting for a few hours) to O₃ at levels typically observed in Southern California can result in breathing pattern changes, reduction of breathing capacity, increased susceptibility to infections, inflammation of the lung tissue, and some immunological changes.

⁵ Inversion is an atmospheric condition in which a layer of warm air traps cooler air near the surface of the earth, preventing the normal rising of surface air.

- Nitrogen Dioxide (NO₂) like O₃, is not directly emitted into the atmosphere but is formed by an atmospheric chemical reaction between nitric oxide (NO) and atmospheric oxygen. NO and NO₂ are collectively referred to as NO_x and are major contributors to O₃ formation. NO₂ also contributes to the formation of PM₁₀. High concentrations of NO₂ can cause breathing difficulties and result in a brownish-red cast to the atmosphere with reduced visibility. There is some indication of a relationship between NO₂ and chronic pulmonary fibrosis. Some increase of bronchitis in children (2-3 years old) has been observed at concentrations below 0.3 ppm.
- Sulfur Dioxide (SO₂) is a colorless, pungent gas formed primarily by the combustion of sulfur-containing fossil fuels. Main sources of SO₂ are coal and oil used in power plants and industries. Generally, the highest levels of SO₂ are found near large industrial complexes. In recent years, SO₂ concentrations have been reduced by the increasingly stringent controls placed on stationary source emissions of SO₂ and limits on the sulfur content of fuels. SO₂ is an irritant gas that attacks the throat and lungs. It can cause acute respiratory symptoms and diminished ventilator function in children. SO₂ can also yellow plant leaves and erode iron and steel.
- Particulate Matter (PM) consists of small liquid and solid particles floating in the air, including smoke, soot, dust, salts, acids, and metals and can form when gases emitted from industries and motor vehicles undergo chemical reactions in the atmosphere. Fine particulate matter, or PM_{2.5}, is roughly 1/28 the diameter of a human hair and results from fuel combustion (e.g. motor vehicles, power generation, industrial facilities), residential fireplaces, and wood stoves. In addition, PM_{2.5} can be formed in the atmosphere from gases such as SO₂, NO_x, and VOC. Inhalable particulate matter, or PM₁₀, is about 1/7 the thickness of a human hair. Major sources of PM₁₀ include crushing or grinding operations; dust stirred up by vehicles traveling on roads; wood burning stoves and fireplaces; dust from construction, landfills, and agriculture; wildfires and brush/waste burning; industrial sources; windblown dust from open lands; and atmospheric chemical and photochemical reactions.

PM_{2.5} and PM₁₀ pose a greater health risk than larger-size particles. When inhaled, they can penetrate the human respiratory system's natural defenses and damage the respiratory tract. PM_{2.5} and PM₁₀ can increase the number and severity of asthma attacks, cause or aggravate bronchitis and other lung diseases, and reduce the body's ability to fight infections. Very small particles of substances, such as lead, sulfates, and nitrates can cause lung damage directly. These substances can be absorbed into the blood stream and cause damage elsewhere in the body. These substances can transport absorbed gases, such as chlorides or ammonium, into the lungs and cause injury. Whereas PM₁₀ tends to collect in the upper portion of the respiratory system, PM_{2.5} is so tiny that it can penetrate deeper into the lungs and damage lung tissues. Suspended particulates also damage and discolor surfaces on which they settle, as well as produce haze and reduce regional visibility.

- Lead (Pb) in the atmosphere occurs as particulate matter. Sources of lead include leaded gasoline; the manufacturers of batteries, paint, ink, ceramics, and ammunition; and secondary lead smelters. Prior to 1978, mobile emissions were the primary source of atmospheric lead. Between 1978 and 1987, the

phase-out of leaded gasoline reduced the overall inventory of airborne lead by nearly 95 percent. With the phase-out of leaded gasoline, secondary lead smelters, battery recycling, and manufacturing facilities have become lead-emission sources of greater concern.

Prolonged exposure to atmospheric lead poses a serious threat to human health. Health effects associated with exposure to lead include gastrointestinal disturbances, anemia, kidney disease, and in severe cases, neuromuscular and neurological dysfunction. Of particular concern are low-level lead exposures during infancy and childhood. Such exposures are associated with decrements in neurobehavioral performance, including intelligence quotient performance, psychomotor performance, reaction time, and growth.

- Toxic Air Contaminants (TAC) are airborne pollutants that may increase a person's risk of developing cancer or other serious health effects. TACs include over 700 chemical compounds that are identified by State and federal agencies based on a review of available scientific evidence. In California, TACs are identified through a two-step process established in 1983 that includes risk identification and risk management.

Regulatory Setting

Federal

The United States Environmental Protection Agency (the "USEPA") is responsible for enforcing the Federal Clean Air Act (CAA), the legislation that governs air quality in the United States. The USEPA is also responsible for establishing the National Ambient Air Quality Standards (NAAQS). NAAQS are required under the 1977 CAA and subsequent amendments. The USEPA regulates emission sources that are under the exclusive authority of the federal government, such as aircraft, ships, and certain types of locomotives. USEPA has jurisdiction over emission sources outside state waters (e.g., beyond the outer continental shelf) and establishes emission standards, including those for vehicles sold in states other than California, where automobiles must meet stricter emission standards set by the California Air Resources Board (CARB).

As required by the CAA, NAAQS have been established for seven major air pollutants: CO, NO₂, O₃, PM_{2.5}, PM₁₀, SO₂, and Pb. The CAA requires the USEPA to designate areas as attainment, non-attainment, or maintenance for each criteria pollutant based on whether the NAAQS have been achieved. The federal standards are summarized on Table IV-1. The USEPA has classified the South Coast Air Basin as non-attainment for O₃, PM_{2.5}, and PM₁₀ and maintenance for CO and NO₂.

State

In addition to being subject to the requirements of CAA, air quality in California is also governed by more stringent regulations under the California Clean Air Act (CCAA). CARB, which became part of the California Environmental Protection Agency in 1991, is responsible for administering the CCAA and

establishing the California Ambient Air Quality Standards (CAAQS). The CCAA, as amended in 1992, requires all air districts in the State to achieve and maintain the CAAQS, which are generally more stringent than the federal standards and incorporate additional standards for sulfates, hydrogen sulfide, vinyl chloride, and visibility-reducing particles.

CARB has broad authority to regulate mobile air pollution sources, such as motor vehicles. It is responsible for setting emission standards for vehicles sold in California and for other emission sources, such as consumer products and certain off-road equipment. CARB established passenger vehicle fuel specifications, which became effective in March 1996. CARB oversees the functions of local air pollution control districts and air quality management districts, which, in turn, administer air quality activities at the regional and county levels. The state standards are summarized on Table IV-1.

The CCAA requires CARB to designate areas within California as either attainment or non-attainment for each criteria pollutant based on whether the CAAQS have been achieved. Under the CCAA, areas are designated as non-attainment for a pollutant if air quality data shows that a State standard for the pollutant was violated at least once during the previous three calendar years. Exceedances that are affected by highly irregular or infrequent events are not considered violations of a State standard and are not used as a basis for designating areas as non-attainment. Under the CCAA, the Los Angeles County portion of the South Coast Air Basin is designated as a non-attainment area for O₃, PM_{2.5}, and PM₁₀.⁶

Local

South Coast Air Quality Management District

The 1977 Lewis Air Quality Management Act merged four air pollution control district to create the SCAQMD to coordinate air quality planning efforts throughout Southern California. It is responsible for monitoring air quality, as well as planning, implementing, and enforcing programs designed to attain and maintain State and federal ambient air quality standards. Programs include air quality rules and regulations that regulate stationary sources, area sources, point sources, and certain mobile source emissions. The SCAQMD is also responsible for establishing stationary source permitting requirements and for ensuring that new, modified, or relocated stationary sources do not create net emission increases.

⁶ CARB, *Area Designation Maps*, available at <http://www.arb.ca.gov/desig/adm/adm.htm>, accessed August 17, 2013.

**Table IV-1
State and National Ambient Air Quality Standards and
Attainment Status for the South Coast Air Basin**

Pollutant	Averaging Period	California		Federal	
		Standards	Attainment Status	Standards	Attainment Status
Ozone (O ₃)	1-hour	0.09 ppm (180 µg/m ³)	Non-attainment	--	--
	8-hour	0.070 ppm (137 µg/m ³)	N/A ¹	0.075 ppm (147 µg/m ³)	Non-attainment
Respirable Particulate Matter (PM ₁₀)	24-hour	50 µg/m ³	Non-attainment	150 µg/m ³	Non-attainment
	Annual Arithmetic Mean	20 µg/m ³	Non-attainment	--	--
Fine Particulate Matter (PM _{2.5})	24-hour	--	--	35 µg/m ³	Non-attainment
	Annual Arithmetic Mean	12 µg/m ³	Non-attainment	15 µg/m ³	Non-attainment
Carbon Monoxide (CO)	8-hour	9.0 ppm (10 mg/m ³)	Attainment	9 ppm (10 mg/m ³)	Maintenance
	1-hour	20 ppm (23 mg/m ³)	Attainment	35 ppm (40 mg/m ³)	Maintenance
Nitrogen Dioxide (NO ₂)	Annual Arithmetic Mean	0.030 ppm (57 µg/m ³)	Non-attainment	53 ppb (100 µg/m ³)	Maintenance
	1-hour	0.18 ppm (338 µg/m ³)	Non-attainment	100 ppb (188 µg/m ³)	Maintenance
Sulfur Dioxide (SO ₂)	24-hour	0.04 ppm (105 µg/m ³)	Attainment	--	Attainment
	1-hour	0.25 ppm (655 µg/m ³)	Attainment	75 ppb (196 µg/m ³)	Attainment
Lead (Pb)	30-day average	1.5 µg/m ³	Non-attainment	--	--
	Calendar Quarter	--	--	0.15 µg/m ³	Attainment

¹N/A = CARB has not determined 8-hour O₃ attainment status
Source: CARB, Ambient Air Quality Standards, and attainment status, accessed October 20, 2014, (www.arb.ca.gov/desig/adm/adm.htm).

The SCAQMD monitors air quality over its jurisdiction of 10,743 square miles, including the South Coast Air Basin, which covers an area of 6,745 square miles and is bounded by the Pacific Ocean to the west; the San Gabriel, San Bernardino and San Jacinto mountains to the north and east; and the San Diego County line to the south. The Basin includes all of Orange County and the non-desert portions of Los Angeles, Riverside, and San Bernardino counties. The SCAQMD also regulates the Riverside County portion of the Salton Sea Air Basin and Mojave Desert Air Basin.

All areas designated as non-attainment under the CCAA are required to prepare plans showing how they will meet the air quality standards. The SCAQMD prepares the Air Quality Management Plan (AQMP) to address CAA and CCAA requirements by identifying policies and control measures. The Southern California Association of Governments (SCAG) assists by preparing the transportation portion of the AQMP. On December 7, 2012, the SCAQMD adopted its 2012 AQMP, which is now the legally enforceable plan for meeting the 24-hour PM_{2.5} strategy standard by 2014.

In addition to criteria pollutants, the SCAQMD also regulates air toxics. A cornerstone of its work was the development of the Multiple Air Toxics Exposure Study (MATES-III). The monitoring program measured more than 30 air pollutants, including both gases and particulates, and estimated the risk of cancer from breathing toxic air pollution throughout the region. MATES-III found that the cancer risk in the region from carcinogenic air pollutants ranges from about 870 in a million to 1,400 in a million, with an average regional risk of about 1,200 in a million. An addendum to the plan was completed in March 2004 that included an update on the implementation of the mobile and stationary source strategies.

In its role as the local air quality regulatory agency, the SCAQMD also provides guidance on how environmental analyses should be prepared. This includes recommended thresholds of significance for evaluating air quality impacts.

City of Los Angeles

The Project is located in the Northeast Los Angeles Community Plan Area. Air quality policies are governed by the City's General Plan, which includes an Air Quality Element. Adopted on November 24, 1992, the Element includes six key goals that relate directly or indirectly to air quality:

1. Good air quality in an environment of continued population growth and healthy economic structure.
2. Less reliance on single-occupant vehicles with fewer commute and non-work trips.
3. Efficient management of transportation facilities and system infrastructure using cost-effective system management and innovative demand management techniques.
4. Minimize impacts of existing land use patterns and future land use development on air quality by addressing the relationship between land use, transportation, and air quality.
5. Energy efficiency through land use and transportation planning, the use of renewable resources and less-polluting fuels and the implementation of conservation measures including passive measures such as site orientation and tree planting.
6. Citizen awareness of the linkages between personal behavior and air pollution and participation in efforts to reduce air pollution.

a) Would the project conflict with or obstruct implementation of the applicable air quality plan?

Less Than Significant Impact. In the case of projects proposed within the City or elsewhere in the South Coast Air Basin (the “Basin”), the applicable plan is the 2012 Air Quality Management Plan (AQMP), which is prepared by the South Coast Air Management District (SCAQMD). The SCAQMD is the agency principally responsible for comprehensive air pollution control in the Basin. To that end, the SCAQMD, a regional agency, works directly with the Southern California Association of Governments (SCAG), county transportation commissions, local governments, and cooperates actively with all state and federal government agencies. The SCAQMD develops rules and regulations, establishes permitting requirements, inspects emissions sources, and enforces such measures through educational programs or fines, when necessary.

The regional ozone attainment plan centers on accommodating population growth forecasts by SCAG. Specifically, SCAG’s growth forecasts from the 2016 Regional Transportation Plan (RTP)/Sustainable Communities Strategy (SCS) are largely built off local growth forecasts from local governments like the City of Los Angeles. The RTP/SCS accommodates up to 4,609,400 persons; 1,690,300 households; and 2,169,100 jobs in the City by 2040.

The Project site is currently [Q]R1-1D and [Q]RD6-1D. The Northeast Los Angeles Community Plan land use designation for the site is Low Residential. The type of land use proposed as part of the Project (single-family residential) are allowed under the existing zoning and land use designation for the Project site. As discussed in more detail in response to Checklist Question 13a, as shown on Table IV-26, the Project would represent a negligible percent of the estimated population and housing growth in the City. The Project’s residents and housing units would be within the forecasted population and housing SCAG and City estimates. Additionally, the Project would help achieve a portion of the household growth forecast for the City by adding housing to meet the need for housing identified in the City’s Regional Housing Needs Assessment (the “RHNA”), while also being consistent with regional policies to reduce urban sprawl, efficiently utilize existing infrastructure, reduce regional congestion, and improve air quality through the reduction of vehicle miles traveled (VMT) as called for in SCAG’s 2008 Regional Comprehensive Plan and 2012-2035 RTP and SCAQMD’s AQMP. The Project would not substantially induce housing growth beyond forecasted levels. Instead, the Project would accommodate a portion of forecasted housing demand currently forecasted for the City, including low-income housing. Thus, the Project would not represent a substantial or significant growth as compared to projected growth.

b) Would the project violate any air quality standard or contribute substantially to an existing or projected air quality violation?

Less Than Significant With Mitigation Incorporated. Both short-term impacts occurring during construction and long-term effects related to the ongoing operation of the Project are discussed. This analysis focuses on two levels of impacts: pollutant emissions and pollutant concentrations. “Emissions”

refer to the quantity of pollutants released into the air. “Concentrations” refer to the amount of pollutant material per volumetric unit of air, as measured in parts per million (ppm) or micrograms per cubic meter ($\mu\text{g}/\text{m}^3$).

Construction – Regional Emissions

Construction-related emissions were estimated using the SCAQMD’s CalEEMod 2013.2.2 model using assumptions from the Project’s developer, including the Project’s construction schedule of 26 months. Key assumptions include export of up to 78,000 cubic yards of soils; site preparation (two weeks), grading phase (3 months), building construction phase (2 months), paving (1 month), and architectural coatings (18 months).

As shown on Table IV-2, the construction of the Project would produce VOC, CO, SO_x , PM_{10} , and $\text{PM}_{2.5}$ emissions that do not exceed the SCAQMD’s regional thresholds. However, prior to mitigation, NO_x emissions during any concurrent grading and building construction processes would exceed the thresholds for this ozone precursor. As a result, prior to mitigation, construction of the Project could contribute substantially to an existing violation of air quality standards for regional pollutants (e.g., ozone). However, with implementation of Mitigation Measures 3-1 through 3-5, Project impacts related to regional construction emissions would be less than significant (refer to Table IV-4 shown after the list of Air Quality mitigation measures).

Table IV-2
Estimated Daily Construction Emissions – Unmitigated

Construction Phase Year	Pounds Per Day					
	VOC	NO_x	CO	SO_x	PM_{10}	$\text{PM}_{2.5}$
2017	16	189	134	<1	21	12
2018	18	201	151	<1	19	13
2019	4	20	21	<1	2	1
2020	4	18	21	<1	1	1
Maximum Regional Total	17	291	151	<1	21	13
Regional Significance Threshold	75	100	550	150	150	55
Exceed Threshold?	No	Yes	No	No	No	No
Maximum Localized Total	-	189	135	<1	15	11
Localized Significance Threshold	--	108	1,048	--	8	5
Exceed Threshold?	N/A	Yes	No	N/A	Yes	Yes
Source: DKA Planning, 2016 based on CalEEMod 2013.2.2 model runs. LST analyses based on 2-acre site with 25-meter distances to receptors in Central Los Angeles source receptor area. Refer to Appendix B.						

Construction – Local Emissions

In terms of local air quality, the Project would produce emissions that do not exceed the SCAQMD's recommended localized standards of significance for CO during the construction phase. However, construction activities could produce NO₂, PM₁₀ and PM_{2.5} emissions that would exceed localized thresholds recommended by the SCAQMD, primarily from vehicle exhaust and fugitive dust emissions from off-road construction vehicles during the potential concurrent grading and building construction phases (refer to Table IV-2). However, with implementation of Mitigation Measures 3-1 through 3-5, Project impacts related to localized construction emissions would be less than significant (refer to Table IV-4 shown after the list of Air Quality mitigation measures).

Operation – Regional Emissions

Table IV-3 shows the estimated daily emissions associated the operational phase of the Project. As shown, operation of the Project would not produce VOC, NO_x, CO, SO_x, PM_{2.5}, and PM₁₀ emissions in excess of SCAQMD's thresholds. Therefore, Project impacts related to operational pollutant emissions would be less than significant.

Table IV-3
Estimated Daily Project Operational Emissions

Emissions Source	Pounds Per Day					
	VOC	NO _x	CO	SO _x	PM ₁₀	PM _{2.5}
Area Source	2	<1	3	<1	<1	<1
Energy Source	<1	<1	<1	<1	<1	<1
Mobile Source	1	4	17	<1	3	1
Total Regional Emissions	4	5	21	<1	3	1
<i>Regional Significance Threshold</i>	55	55	550	150	150	55
Exceed Threshold?	No	No	No	No	No	No
Total Localized Emissions	3	<1	3	<1	<1	3
<i>Localized Significance Threshold</i>	-	108	1,048	-	2	-
Exceed Threshold?	N/A	No	No	N/A	No	N/A

Source: DKA Planning 2015 based on CalEEMod 2013.2.2 model runs. LST analysis based on 2-acre site with 25-meter distances to receptors in Central Los Angeles source receptor area.

- c) Would the project result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions, which exceed quantitative threshold for ozone precursors)?

Less Than Significant With Mitigation Incorporated. The SCAQMD's *CEQA Air Quality Handbook* identifies several methods to determine the cumulative significance of land use projects (i.e., whether the

contribution of a project's emissions is cumulatively considerable). However, the SCAQMD no longer recommends the use of these methodologies. Instead, the SCAQMD recommends that any construction-related emissions and operational emissions from individual development projects that exceed the project-specific mass daily emissions thresholds identified above also be considered cumulatively considerable.⁷ The SCAQMD neither recommends quantified analyses of the emissions generated by a set of cumulative development projects nor provides thresholds of significance to be used to assess the impacts associated with these emissions.

As discussed in response to Checklist Question 3b, with mitigation, the Project would not produce VOC, NO_x, CO, SO_x, PM_{2.5}, and PM₁₀ emissions in excess of SCAQMD's significance thresholds. As such, the Project's contribution to cumulative pollutant emissions would not be considerable.

d) Would the project expose sensitive receptors to substantial pollutant concentrations?

Less Than Significant Impact. SCAQMD recommends an evaluation of potential localized CO impacts when vehicle-to-capacity (V/C) ratios are increased by two percent or more at intersections with a level of service (LOS) of C or worse, and/or when the LOS for an intersection worsens from C to D or worse. Traffic volumes that meet these criteria have the potential to result in CO "hotspots." The Project includes development of 42 single-family residential homes, which would generate approximately 32 AM peak-hour trips, 42 PM peak-hour trips, and 400 daily trips. Project traffic would not change LOS at any of the intersections near the Project site (refer to Table IV-36 later in this section). Thus, Project traffic would not have the potential to result in CO hotspots. Additionally, as discussed in response to Checklist Question 3b, the Project would not produce VOC, NO_x, CO, SO_x, PM_{2.5}, and PM₁₀ emissions in excess of SCAQMD's significance thresholds. As such, the Project would not expose sensitive receptors to substantial pollutant concentrations. Therefore, Project impacts related to this issue would be less than significant.

e) Would the project create objectionable odors affecting a substantial number of people?

No Impact. The Project includes development of 42 single-family residential homes on the Project site and would not generate any odors. Therefore, the Project would not create objectionable odors affecting a substantial number of people.

Mitigation Measures (Air Quality)

To ensure that the Project would not result in any significant impacts related to construction emissions, the following mitigation measures are required (refer to Table IV-4):

⁷ *White Paper on Regulatory Options for Addressing Cumulative Impacts from Air Pollution Emissions, SCAQMD Board Meeting, September 5, 2003, Agenda No. 29, Appendix D, p. D-3.*

- 3-1: All off-road construction equipment greater than 50 hp shall meet U.S. EPA Tier 4 emission standards, where available, to reduce NO_x, PM₁₀, and PM_{2.5} emissions at the Project site. In addition, all construction equipment shall be outfitted with Best Available Control Technology devices certified by CARB. Any emissions control device used by the contractor shall achieve emissions reductions that are no less than what could be achieved by a Level 3 diesel emissions control strategy for a similarly sized engine as defined by CARB regulations.
- 3-2: Require the use of 2010 and newer diesel haul trucks (e.g., material delivery trucks and soil import/export) and if the Lead Agency determines that 2010 model year or newer diesel trucks cannot be obtained, the Lead Agency shall require trucks that meet U.S. EPA 2007 model year NO_x emissions requirements.
- 3-3: At the time of mobilization of each applicable unit of equipment, a copy of each unit's certified tier specification, BACT documentation, and CARB or SCAQMD operating permit shall be provided.
- 3-4: Encourage construction contractors to apply for SCAQMD "SOON" funds. Incentives could be provided for those construction contractors who apply for SCAQMD "SOON" funds. The "SOON" program provides funds to accelerate clean up of off-road diesel vehicles, such as heavy duty construction equipment. More information on this program can be found at: <http://www.aqmd.gov/home/programs/business/business-detail?title=off-road-dieselengines&parent=vehicle-engine-upgrades>.
- 3-5: Construction activities shall comply with SCAQMD Rule 403, including the following measures:
- Apply water to disturbed areas of the site three times a day
 - Require the use of a gravel apron or other equivalent methods to reduce mud and dirt trackout onto truck exit routes
 - Appoint a construction relations officer to act as a community liaison concerning on-site construction activity including resolution of issues related to PM generation.
 - Limit soil disturbance to the amounts analyzed in the Final MND.
 - All materials transported off-site shall be securely covered.
 - Apply non-toxic soil stabilizers according to manufacturers' specifications to all inactive construction areas (previously graded areas inactive for ten days or more).
 - Traffic speeds on all unpaved roads to be reduced to 15 mph or less.

Table IV-4
Estimated Daily Construction Emissions – Mitigated

Construction Phase Year	Pounds Per Day					
	VOC	NO _x	CO	SO _x	PM ₁₀	PM _{2.5}
2017	4	41	111	<1	7	4
2018	5	43	139	<1	5	3
2019	2	5	21	<1	<1	<1
2020	2	5	21	<1	<1	<1
Maximum Regional Total	5	43	139	<1	7	4
Regional Significance Threshold	75	100	550	150	150	55
Exceed Threshold?	No	No	No	No	No	No
Maximum Localized Total		13	112	<1	3	2
Localized Significance Threshold	--	108	1,048	--	8	5
Exceed Threshold?	N/A	No	No	N/A	No	No
<i>Source: DKA Planning, 2016 based on CalEEMod 2013.2.2 model runs. LST analyses based on 2-acre site with 25-meter distances to receptors in Central Los Angeles source receptor area. Refer to Appendix B.</i>						

Cumulative Impacts

As stated previously, SCAQMD recommends that any construction-related emissions and operational emissions from individual development projects that exceed the project-specific mass daily emissions thresholds identified above also would be considered cumulatively considerable. Individual projects that generate emissions not in excess of SCAQMD's significance thresholds would not contribute considerably to any potential cumulative impact. As discussed in response to Checklist Question 3b, with mitigation, the Project would not generate emissions in excess of SCAQMD's significance thresholds. As such, the Project would not contribute considerably to any potential cumulative impact. Therefore, cumulative impacts related to air quality would be less than significant.

4. BIOLOGICAL RESOURCES

a) **Would the project have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulation, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?**

Less Than Significant With Mitigation Incorporated. A Biological Resources Report was prepared for the Project by SWCA Environmental Consultants (SWCA) to determine the presence or absence of sensitive natural resources (including special status species) at the Project site (refer to Appendix C).

Methods

SWCA conducted a review of the Project site, reviewing aerial photographs and records of plant and wildlife occurrences in the U.S. Geological Survey Los Angeles 7.5-minute topographic quadrangle where the Project site is located. Plant and wildlife occurrences were queried for the quadrangle from the California Natural Diversity Database (CNDDDB) and the California Native Plant Society's Rare Plant Inventory.^{8,9} Both databases track special-status and sensitive species, including species that are listed under the State and Federal Endangered Species Acts; considered rare, sensitive, or of special concern to California Department of Fish and Wildlife (CDFW), the U.S. Fish and Wildlife Service, Bureau of Land Management (BLM), U.S. Forest Service (USFS); or are on other watch lists.

Before conducting a field survey SWCA biologists reviewed aerial photographs of the Project site and arborist's reports for the Project.^{10,11} Based on the aerial photographs showing both visible and infrared spectra, a preliminary map of plant communities and land cover was created in a Geographic Information System (GIS) using ESRI ArcMap version 10.3.4. The preliminary map was loaded onto a Trimble™ geographic positioning system (GPS) unit with sub-meter accuracy and modified in the field. The field survey of the Project site was conducted on July 22, 2016 by SWCA biologist Alex Beakes. During the survey, Mr. Beakes mapped the plant communities and land cover present using a minimum mapping unit size of 0.25 acre, identified plants at the site, recorded any evidence of wildlife use such as dens or trails, and took representative photographs.

Results

The Project site is characterized by hilly topography, which has a high point in the approximate center of the site. Elevation at the site ranges from approximately 450 to 525 feet above mean sea level. The western and northern edges of the site are bounded by sidewalk and a strip of vegetation between the sidewalk and two- and four-lane streets. An elementary school with buildings and sports fields is located across the street to the west and north of the site. Urban residential development abuts the Project site's

⁸ California Natural Diversity Data Base (CNDDDB). 2016. Data retrieved using Rarefind 5 [web application]. California Department of Fish and Game, Sacramento. Available at: <http://www.dfg.ca.gov/biogeodata/cnddb/mapsanddata.asp>. Accessed October 2016.

⁹ CNPS, Rare Plant Program. 2016. Inventory of Rare and Endangered Plants (online edition, v8-02). California Native Plant Society, Sacramento, CA. Website <http://www.rareplants.cnps.org>. Accessed October 2016/

¹⁰ Arborgate Consulting, Inc. 2015. Tree Preservation Report. Prepared for Patrick Donahue, Clearwater Communities. Prepared by Arborgate Consulting, Inc., Greg Applegate. March 2015.

¹¹ Arborgate Consulting, Inc. 2015. Addendum to Tree Preservation Report. Prepared for Patrick Donahue, Clearwater Communities. Prepared by Arborgate Consulting, Inc., Greg Applegate. October 2015.

eastern and southern edges. Current imagery shows areas at the hilltop and southern portion of the property that have previously been developed, although there are no buildings currently on the Project site. Historical imagery shows one building at the top of the hill, a second building at the southern foot of the hill, and access roads or driveways. Trees are visible in the images around the site perimeter, and concentrated in the north. An unauthorized encampment consisting of a single tent was located at the top of the hill.

Sensitive Species

A total of 27 plant species were recorded during the field survey, which were a mix of native species, plants often used in landscaping, and weedy non-native species. Only four native species were recorded (refer to Table IV-5). Numerous Southern California black walnut (*Juglans californica*) trees were recorded, around the site perimeter and concentrated on the north-facing slope in the northern portion of the site. This species is assigned a California Rare Plant Rank (CRPR) of 4.2 by the California Native Plant Society, meaning that it has a limited distribution or occurs infrequently through a broader area in California. Plants with a ranking of 1 or 2 meet the definition of rare, threatened, or endangered, and require evaluation under CEQA.

**Table IV-5
Plants Recorded at the Project Site**

Family	Scientific Name	Common Name	Life Form	Status
Aizoaceae	<i>Carpobrotus</i> sp.	ice plant	shrub	invasive
Anacardiaceae	<i>Malosma laurina</i>	laurel sumac	shrub	native
	<i>Schinus terebinthifolius</i>	Brazilian pepper tree	tree	invasive
Apocynaceae	<i>Vinca major</i>	periwinkle	perennial herb	invasive
Arecaceae	<i>Phoenix canariensis</i>	Canary island date palm	perennial herb	invasive
	<i>Washingtonia robusta</i>	Washington fan palm	tree	invasive
Asteraceae	<i>Centaurea melitensis</i>	Tocalote	annual herb	invasive
	<i>Helminthotheca echinoides</i>	bristly ox-tongue	annual herb	invasive
Bignoniaceae	<i>Jacaranda mimosifolia</i>	jacaranda	tree	non-native
Chenopodiaceae	<i>Chenopodium murale</i>	nettle leaf goosefoot	annual herb	non-native
	<i>Salsola tragus</i>	Russian thistle	annual herb	invasive
Euphorbiaceae	<i>Ricinus communis</i>	castor bean	tree	invasive
Fabaceae	<i>Acacia melanoxylon</i>	blackwood acacia	tree	invasive
Juglandaceae	<i>Junglans californica</i>	Southern California black walnut	tree	native
Malvaceae	<i>Malva parviflora</i>	cheeseweed	annual herb	non-native
Myrtaceae	<i>Eucalyptus globulus</i>	blue gum eucalyptus	tree	invasive
Oleaceae	<i>Fraxinus uhdei</i>	Shamel ash	tree	invasive
	<i>Olea europaea</i>	olive tree	tree	invasive
Plumbaginaceae	<i>Plumbago auriculata</i>	cape leadwort	shrub	non-native
Poaceae	<i>Avena</i> sp.	wild oat	grass	invasive
	<i>Bromus diandrus</i>	ripgut brome	grass	invasive
	<i>Pennisetum setaceum</i>	fountaingrass	grass	invasive
Polygonaceae	<i>Eriogonum fasciculatum</i>	California buckwheat	shrub	native
Rosaceae	<i>Heteromeles arbutifolia</i>	Toyon	tree	native
Simaroubaceae	<i>Ailanthus altissima</i>	Tree of heaven	tree	invasive
Solanaceae	<i>Nicotiana glauca</i>	tree tobacco	tree	invasive
Ulmaceae	<i>Ulmus parvifolia</i>	Chinese elm	shrub	non-native

Source: SWCA, 2016. Refer to Appendix C.

The database review returned 13 species of plants and 9 species of wildlife with recorded occurrences in the records search area (approximately 60 square miles; refer to Table IV-6). Of these, only one was considered to have the potential to occur at the site: hoary bat (*Lasiurus cinereus*). Hoary bat is included on the CDFW list of sensitive animals because it is listed as a Medium priority species by the Western Bat Working Group (WBWG). The Medium designation indicates “a level of concern that should warrant closer evaluation, more research, and conservation actions of both the species and possible threats. A lack of meaningful information is a major obstacle in adequately assessing these species’ status and should be considered a threat.”¹² The hoary bat is solitary and roosts primarily in foliage of both coniferous and deciduous trees, near the ends of branches, 10 to 40 feet above the ground. Many kinds of trees are used, and roost sites are usually hidden from above by canopy but open below. Roosts are usually at the edge of a clearing. Hoary bats frequently change roost sites and have low roost fidelity. Hoary bat is a migratory species, which in Southern California winters near the coast and breeds further inland. It has the potential to occur at the Project site during the winter season (August 1 to February 28). Trees at the Project site may provide roosting habitat for this species.

Discussion

Two species with local protections or considered sensitive were identified as occurring or having the potential to occur at the Project site: Southern California black walnut and hoary bat.

Southern California Black Walnut

Southern California black walnut trees are addressed in the Los Angeles Municipal Code (CRPR) and are ranked as a CRPR 4.2. The Southern California black walnut tree is designated as a protected tree in Section 96.303.5 of the LAMC. The CRPR rank of 4.2 means that the species has limited distribution; no specific protections are afforded by this ranking. As discussed in response to Checklist Question 1(b), 102 Southern California black walnuts are located on the Project site, and up to 39 of these trees would be removed as part of the Project. However, as outlined previously in Mitigation Measure 1-2, the Project Applicant would be required to replace the protected trees on the Project site at a 4:1 ratio. Implementation of Mitigation Measure 1-2 and 1-4 would ensure that Project impacts related to protected trees would be less than significant.

¹² The Western Bat Working Group (WBWG) is comprised of agencies, organizations and individuals interested in bat research, management and conservation from the 13 western states and provinces. The goals are (1) to facilitate communication among interested parties and reduce risks of species decline or extinction; (2) to provide a mechanism by which current information on bat ecology, distribution and research techniques can be readily accessed; and (3) to develop a forum to discuss conservation strategies, provide technical assistance and encourage education programs. Species are ranked as High, Medium, or Low Priority in each of 10 regions in western North America. The CNDDDB tracks bat species that are at least Low- Medium Priority in California. More information is available at: <http://www.wbwg.org>.

Table IV-6
Sensitive Species With Recorded Occurrences in the
Los Angeles U.S. Geological Survey 7.5-Minute Topographic Quadrangle

Name	Special Status	General Habitat Microhabitat	Potential to Occur at the Project
Plants			
Catalina mariposa lily <i>Calochortus catalinae</i>	CRPR 4.2	Chaparral, cismontane woodland, coastal scrub, valley and foothill grassland.	None. No suitable habitat is present.
Davidson's saltscale <i>Atriplex serenana</i> var. <i>davidsonii</i>	CRPR 1B.2	Coastal bluff scrub, coastal scrub. Alkaline soil.	None. No suitable habitat is present. Likely extirpated from L.A. County.
Greata's aster <i>Symphyotrichum greatae</i>	CRPR 1B.3	Broadleaved upland forest, chaparral, cismontane woodland, lower montane coniferous woodland, riparian woodland. Mesic canyons.	None. No suitable habitat is present.
Hubby's phacelia <i>Phacelia hubbii</i>	CRPR 4.2	Chaparral, coastal scrub, valley and foothill grassland. Gravelly and rocky areas; talus slopes.	None. No suitable habitat is present.
Los Angeles sunflower <i>Helianthus nuttallii</i> ssp. <i>parishii</i>	CRPR 1A	Freshwater marsh, marsh and swamp, salt marsh, wetlands.	None. No suitable habitat is present.
Mesa horkelia <i>Horkelia cuneata</i> var. <i>puberula</i>	CRPR 1B.1	Chaparral, cismontane woodland, coastal scrub. Sandy or gravelly sites.	None. No suitable habitat is present.
Monkey-flower savory <i>Clinopodium mimuloides</i>	CRPR 4.2	Chaparral, North Coast coniferous forest. Stream banks, mesic sites.	None. No suitable habitat is present.
Parish's gooseberry <i>Ribes divaricatum</i> var. <i>parishii</i>	CRPR 1A	Riparian woodlands. Willow swales in riparian habitats.	None. No suitable habitat is present.
Plummer's mariposa-lily <i>Calochortus plummerae</i>	CRPR 4.2	Chaparral, cismontane woodland, coastal scrub, lower montane coniferous forest, valley and foothill grassland. Occurs on rocky and sandy sites, usually of granitic or alluvial material. Can be very common after fire.	None. No suitable habitat is present.
Prostrate vernal pool navarretia <i>Navarretia prostrata</i>	CRPR 1B.1	Coastal scrub, meadow and seep, valley and foothill grassland, vernal pools, wetlands. Alkaline soils in grassland, or in vernal	None. No suitable habitat is present.

Table IV-6
Sensitive Species With Recorded Occurrences in the
Los Angeles U.S. Geological Survey 7.5-Minute Topographic Quadrangle

Name	Special Status	General Habitat Microhabitat	Potential to Occur at the Project
		pools. Mesic, alkaline sites.	
Robinson's pepper-grass <i>Lepidium virginicum</i> var. <i>robinsonii</i>	CRPR 4.3	Chaparral, coastal scrub. Dry soils, shrubland.	None. No suitable habitat is present.
Round-leaved filaree <i>California macrophylla</i>	CRPR 1B.2	Cismontane woodland, valley and foothill grassland. Clay soils.	None. No suitable habitat is present.
Vernal barley <i>Hordeum intercedens</i>	CRPR 3.2	Coastal dunes, coastal scrub, valley and foothill grassland, vernal pools. Saline flats and depressions.	None. No suitable habitat is present.
Reptiles and Amphibians			
Coast horned lizard <i>Phrynosoma blainvillii</i>	SSC	Coastal sage scrub and chaparral in arid and semiarid climates. Prefers friable, rocky, or shallow sandy soils.	None. No suitable habitat is present.
Birds			
Bank swallow <i>Riparia riparia</i>	ST	Riparian scrub, riparian woodland. Nests in steep sand, dirt, or gravel banks, along the edge of inland water, along coast, in gravel pits, or road embankments.	None. No suitable habitat is present.
Burrowing owl <i>Athene cunicularia</i>	SSC	Open, dry annual or perennial grasslands, deserts and scrublands characterized by low-growing vegetation. Subterranean nester, dependent upon burrowing mammals, most notably the California ground squirrel.	None. No suitable habitat is present.
Least Bell's vireo <i>Vireo bellii pusillus</i>	FE / SE	Riparian forest, riparian scrub, riparian woodland. Nests placed along margins of bushes or on twigs projecting into pathways, usually willow, mulefat, mesquite	None. No suitable nesting or foraging habitat present.
Southwestern willow flycatcher <i>Empidonax traillii extimus</i>	FE / SE	Riparian woodland. Breeds in relatively dense riparian tree and shrub communities associated with rivers, swamps, and other wetlands including lakes and reservoirs; habitat patches must be at least	None. No suitable nesting or foraging habitat present.

Table IV-6
Sensitive Species With Recorded Occurrences in the
Los Angeles U.S. Geological Survey 7.5-Minute Topographic Quadrangle

Name	Special Status	General Habitat Microhabitat	Potential to Occur at the Project
		0.25 acres in size and at least 30 feet wide.	
Mammals			
American badger <i>Taxidea taxus</i>	SSC	Found in arid, open habitats, particularly grasslands, savannahs, mountain meadows, and desert scrub openings. Needs friable soils for digging and open, uncultivated ground; occurs at low to moderate slopes; has been associated with Joshua tree woodland and pinyon-juniper habitats.	None. No suitable habitat is present.
Big free-tailed bat <i>Nyctinomops macrotis</i>	SSC	Rocky terrain; bare rock/talus/scree, cliff, desert, woodland – hardwood. Roosts in rock crevices (vertical or horizontal) in cliffs; also in buildings, caves, and occasionally tree holes.	Low. If cavities are present in trees on-site, there is a very low potential for this species to be present.
Hoary bat <i>Lasiurus cinereus</i>	None	Forages over a wide range of habitats, but prefers open habitats with access to trees for roosting, and water. Ranges throughout most of California. Primarily roosts in trees and foliage.	Moderate. Trees on site may provide some roost sites. The Arroyo Seco wash, approximately 2 miles to the northwest, is the closest substantial semi-natural water source.
Western mastiff bat <i>Eumops perotis californicus</i>	SSC	Found in the southwestern United States, generally away from human development; this species can utilize a variety of habitat types including chaparral, oak woodland, pine forests, agricultural areas and desert washes. Roosts primarily in vertical rock crevices on cliffs; common in open habitats when foraging.	None. No suitable roosting habitat is present.
<p>Key: FC = Federal candidate for ESA listing, FE = Federally endangered, None = species included for other reasons (see the Special Animals List at http://www.dfg.ca.gov/wildlife/nongame/list.html), SE = State endangered, SSC = Species of Special Concern, ST = State threatened. CRPR = California Rare Plant Rank. CRPR Rankings: IA. : Presumed extinct in California</p>			

Table IV-6
Sensitive Species With Recorded Occurrences in the
Los Angeles U.S. Geological Survey 7.5-Minute Topographic Quadrangle

Name	Special Status	General Habitat Microhabitat	Potential to Occur at the Project
<i>1B. : Rare, threatened, or endangered in California and elsewhere.</i> <i>2. : Rare, threatened, or endangered in California, but more common elsewhere.</i> <i>3. : Needs review (information lacking).</i> <i>4. : Limited distribution (Watch List).</i> <i> 1: Seriously threatened in California.</i> <i> 2: Fairly threatened in California.</i> <i> 3: Not very threatened in California.</i>			
Source: SWCA, 2016. Refer to Appendix C.			

Hoary Bat

Hoary bat is ranked as a Medium priority species by the WBWG and tracked by the CNDDDB because there is limited information available on this species. The bat's ranking provides no specific protections. This species has the potential to occur at the Project site between August 1st and February 28th; between March 1st and July 31st, the bat moves inland and northward away from the Project site area to its breeding grounds. The removal of individual trees at the Project site would not substantially change available habitat for this species, because of the wide range of trees available for roosting that would remain at the Project site and in the surrounding area. Significant impacts to individual hoary bats could occur if individual bats are roosting on the trees when the trees are cut. Removing large trees with potential roosting habitat when hoary bats are absent (March 1st through July 31st) would avoid this impact. If large trees are removed between August 1st and February 28th, a nocturnal survey for roosting bats should be conducted by a qualified bat biologist. If evidence of bats is present, then removal of occupied roost trees shall not occur until the biologist determines that the roost is no longer in use through repeated nocturnal surveys. Requirements to avoid roosting bats are included as Mitigation Measure 4-1, implementation of which would ensure that the Project would not result in any significant impacts related to the hoary bat.

Nesting Birds

The Project site does contain 174 trees, 98 of which would be removed as part of the Project. Depending on the time of year that the Project site is developed, nesting birds (which are protected by law) could be using the trees on the Project site. As such, the Project Applicant would be required to implement Mitigation Measure 4-2 to ensure that no significant impacts related to nesting birds would occur. Therefore, impacts related to nesting birds would be less than significant.

b) Would the project have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

Less Than Significant Impact. As discussed previously in response to Checklist Question 4(a), a Biological Resources Report was prepared for the Project by SWCA to determine the presence or absence of sensitive natural resources (including sensitive natural communities) at the Project site.

Sensitive Natural Communities

Several classification systems have been used to describe natural communities in California; the current most widely accepted system, and the one recommended by CDFW is *A Manual of California Vegetation* (MCV), the most current version of which is maintained online.¹³ The MCV provides descriptions of more than 400 distinct vegetation alliances, which are plant communities dominated by native plant species. Membership rules for over 90 different woodland alliances are detailed, as well as numerous shrubland alliances and herbaceous alliances.

Since the inception of the State's Natural Heritage program in 1979, CDFW has maintained a list of special status (i.e. sensitive) natural communities in the state.¹⁴ Sensitive natural communities are those with a ranking of 1, 2, or 3, which is assigned based on rarity inside state borders, where 1 is the rarest. The CDFW maintains a crosswalk to translate different vegetation community mapping methods to the corresponding MCV alliance. The CDFW describes special status natural communities as follows:

Special status natural communities are communities that are of limited distribution statewide or within a county or region and are often vulnerable to environmental effects of projects. These communities may or may not contain special status species or their habitat. The most current version of the Department's List of California Terrestrial Natural Communities indicates which natural communities are of special status given the current state of the California classification."¹⁵

¹³ CNPS. 2016. *A Manual of California Vegetation, Online Edition*. <http://www.cnps.org/cnps/vegetation/>. Accessed on July 28, 2016. California Native Plant Society, Sacramento, CA.

¹⁴ CDFW. 2016. *Natural Communities*. <https://www.wildlife.ca.gov/Data/VegCAMP/Natural-Communities>. Accessed on July 28, 2016.

¹⁵ CDFW. 2009. *Guidelines for Assessing the Effects of Proposed Projects on Rare, Threatened and Endangered Plants and Natural Communities*. California Department of Fish and Wildlife. Available from: <http://nrm.dfg.ca.gov/FileHandler.ashx?DocumentID=107494&inline>.

The MCV also includes a few types of communities called semi-natural stands that are dominated by non-native species; these are included in the MCV because they have potential value as habitat for native species of plants and wildlife. Most vegetation communities dominated by non-native species are not included in the MCV because their potential habitat values are limited.

Water Bodies under State or Federal Jurisdiction

There are no water bodies at the Project site. As such, no further discussion of this issue is required.

Vegetation Communities and Land Cover

Vegetation at the Project site is generally characterized as consisting of a mix of native plant species, landscape plants, and weedy non-native plants, some of which are invasive species (refer to Table IV-5). No buildings were present. Substantial amounts of trash were observed (and mapped) throughout the Project site, especially on the graded pad at the southern end of the site.

There were no areas dominated by native species where Southern California black walnut constituted more than 50 percent relative cover in the canopy. Therefore, no areas met the definition of *Juglans californica* Woodland Alliance as described in the MCV, which is considered a sensitive natural community by CDFW. No areas of the Project site were dominated by native species. Thus, the Project site was mapped as various other land cover types, including Bare Soil/Paved, Mixed Woodland, Non-native Grassland, and Debris (refer to Figure IV-1).

The Bare Soil/Paved area consisted of graded areas where building foundations and driveways were built in the past, including unpaved areas, degraded asphalt, and degraded concreted. The Mixed Woodland consisted of areas of trees of various species, including Southern California black walnut and several non-native species (refer to Table IV-5). The Non-native Grassland was comprised of a mix of non-native weedy and invasive grasses that commonly grow in unmaintained areas, with scattered small shrubs and other herbaceous plants. The Debris was comprised of shingles, boards, chairs, tires, and other refuse.

The vegetation mapped on the Project site does not meet the criteria for a sensitive natural community, nor are the native trees within mixed woodlands considered special status species under state or federal regulations. Therefore, Project impacts related to sensitive natural communities would be less than significant.



Source: SWCA Environmental Consultants, 2016.

c) Would the project have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?

No Impact. The Project site is an infill site that is not currently developed with any structures and does not contain any wetlands or other areas subject to the jurisdiction of the U.S. Army Corps of Engineers, CDFW, or State Water Resources Control Board under the Clean Water Act (refer to response to Checklist Question 4[c]). Therefore, no impacts related to this issue would occur.

d) Would the project interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?

No Impact. The term “wildlife movement corridor” implies a continuous, unidirectional movement of individual animals. Although wildlife movement corridors may sometimes be used in this way, the most important functions of a wildlife movement corridor are the long-term dispersal of genetic material between population centers and the maintenance of balanced, viable populations in these areas. The term “habitat linkage”—described as “an undisturbed habitat parcel that connects two or more reserve parcels (generally public land holdings) with habitat suitable for movement of mobile terrestrial organisms between the reserve parcels”—better characterizes this concept. Habitat linkages are best conceived as large “plains” of habitat rather than as narrow travel routes, which offer the greatest possible potential of facilitating short- and long-term wildlife movement between parcels. The habitat linkages serve to both permit movement between isolated populations and maintain an integrated, functioning landscape-wide ecosystem.¹⁶

In general a habitat linkage, referred to here as a wildlife corridor, is a strip of land that connects two or more, larger land areas and is free of barriers that would seriously curtail or prevent wildlife passage. These corridors can serve as useful habitat in their own right, or can serve as travel lanes for seasonal movements of wildlife. Their value depends upon width, habitat type and structure, nature of surrounding habitat, human use patterns, and other factors. Typically, a wildlife corridor provides refuge and ease of movement, and often follows ridgelines or drainages. Wildlife movement corridors are important for the free movement of animals between population centers, for access to food and water sources during drought, as escape routes from brush fires, and, in the longer term, for dispersal of genetic traits between population centers.

Human encroachment fragments natural habitats into smaller and more isolated units. In the process, it destroys habitat of many species, modifies habitat of others, and creates new habitat for yet others. Many

¹⁶ Lieberstein, T.A. 1989. *Reserve Design in the Santa Monica Mountains*. Master's thesis, California State University, Northridge.

studies have indicated that, in general, habitat size is the most important factor in determining land vertebrate species diversity.¹⁷ The degree of habitat isolation and percentage of vegetative cover are other major factors in species variety and abundance. Wildlife corridors can prevent local extinctions by connecting relatively small open space preserves, thereby allowing gene flow and providing for a wide diversity of genetic traits throughout the interconnected populations.

Nursery sites are generally considered to include specific locations used by groups of wildlife to bear and care for young. Salmon spawning streams, maternity colonies of bats, and nesting areas for colonially nesting birds are among the types of sites typically evaluated.

No animal dens, trails, or other signs of regular use were observed at the Project site. Many species of wildlife typical of urban and suburban areas, such as coyote (*Canis latrans*), Virginia opossum (*Didelphis virginiana*), and striped skunk (*Mephitis mephitis*), are primarily nocturnal and adept at avoiding humans; they may have been present but undetected. Compared to the surrounding residential areas, the Project site may provide a refuge for common urban and suburban wildlife.

The Project site lacks the features of a wildlife movement corridor or native wildlife nursery site, as defined above. The surroundings are highly developed and urbanized, with isolated natural areas that do not form a connected corridor between large areas of habitat. The Project site has evidence of regular human presence, which would also generally deter wildlife from using it. Approximately one-half mile to the southeast of the site a major rail corridor running southwest to northeast acts as a barrier to wildlife movement. Approximately one mile from the Project site, Interstates 10 and 710 form a major barrier to southward and eastward movement. Ascot Park, located approximately 0.2 miles west of the Project site, is the nearest large area of semi-natural habitat. Although it is relatively close to the Project site, wildlife moving through would then be blocked by the rail and interstate corridors.

The Project site is located in a fairly urbanized area of the City and is surrounded by existing residential development and roadway and utility infrastructure. The Project site is an infill site that is not currently developed with any structures and contains some vegetation, but given the developed nature of the Project area, the area is not used as a significant wildlife corridor. Additionally, there are no waterways in the Project area that are used by migratory fish, and there are no wildlife nursery sites in the area. Therefore, the Project would not interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites, and no impacts related to this issue would occur.

¹⁷ Adams, L.W., and L.E. Dove 1989. *Wildlife Reserves and Corridors in the Urban Environment: A Guide to Ecological Landscape Planning and Resource Conservation*. National Institute for Urban Wildlife, Columbia, Md.

e) **Would the project conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?**

Less Than Significant With Mitigation Incorporated. As discussed previously, a Tree Preservation Report was prepared for the Project that identified 102 protected trees measuring 4-inch trunk diameter or larger and 72 non-protected measuring 8-inch trunk diameter or larger (refer to Appendix A). Of the 102 protected trees, 63 to 65 would be retained in place, and 37 to 39 would be removed. Of the 72 non-protected trees, 13 would be retained in place, and 59 would be removed. However, as required by the City and as outlined in Mitigation Measures 4-3 and 4-4, the removed non-protected trees would be replaced on the Project site at a 1:1 ratio, and the removed protected trees would be replaced on the Project site at a 4:1 ratio, respectively. Therefore, with implementation of these mitigation measures, Project impacts related to trees would be less than significant.

f) **Would the project conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?**

No Impact. The Project site is not subject to a Habitat Conservation Plan, a Natural Community Conservation Plan, or other such plan. Therefore, the Project would not conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan.

Mitigation Measures (Biological Resources)

To ensure that the Project would not result in any significant impacts related to nesting species, the following mitigation measure is required:

4-1: To avoid potential significant impacts to roosting bats, one of the following shall be implemented by the Project Applicant:

- Tree removal shall occur between March 1st and July 31st.

OR...

- If tree removal would occur between August 1st and February 28th/29th, the Project Applicant shall retain a qualified bat biologist to conduct a roosting bat survey. If evidence of bats is present, then removal of occupied roost trees shall not occur until the biologist determines that the roost is no longer in use through repeated nocturnal surveys. The results of the survey(s) shall be provided to the Department of Public Works prior to removal of any protected trees.

4-2: To avoid potential significant impacts to nesting birds, including migratory birds and raptors, one of the following shall be implemented by the Project Applicant:

- Conduct vegetation removal associated with construction from September 1st through January 31st, when birds are not nesting. Initiate grading activities prior to the breeding season (which is generally February 1st through August 31st) and keep disturbance activities constant throughout the breeding season to prevent birds from establishing nests in surrounding habitat (in order to avoid possible nest abandonment); if there is a lapse in activities of more than five days, pre-construction surveys shall be necessary as described in the bullet below.

OR...

- Conduct pre-construction surveys for nesting birds if vegetation removal or grading is initiated during the nesting season. A qualified wildlife biologist shall conduct weekly pre-construction bird surveys no more than 30 days prior to initiation of grading to provide confirmation on the presence or absence of active nests in the vicinity (at least 300 to 500 feet around the individual construction site, as access allows). The last survey should be conducted no more than three days prior to the initiation of clearance/construction work. If active nests are encountered, clearing and construction in the vicinity of the nests shall be deferred until the young birds have fledged and there is no evidence of a second attempt at nesting. A minimum buffer of 300 feet (500 feet for raptor nests) or as determined by a qualified biologist shall be maintained during construction depending on the species and location. The perimeter of the nest-setback zone shall be fenced or adequately demarcated with staked flagging at 20-foot intervals, and construction personnel and activities restricted from the area. Construction personnel should be instructed on the sensitivity of the area. A survey report by the qualified biologist documenting and verifying compliance with the mitigation and with applicable state and federal regulations protecting birds shall be submitted to the City and County, depending on within which jurisdiction the construction activity is occurring. The qualified biologist shall serve as a construction monitor during those periods when construction activities would occur near active nest areas to ensure that no inadvertent impacts on these nests would occur.

4-3: ***Non-Protected Trees***

- Prior to issuance of any permit related to development of the Project, a plot plan shall be prepared for the Project, indicating the location, size, type, and general condition of all existing trees on the Project site and within the adjacent public right(s)-of-way.

- All significant (8-inch or greater trunk diameter, or cumulative trunk diameter if multi-trunked, as measured 54 inches above the ground) non-protected trees on the Project site proposed for removal shall be replaced at a 1:1 ratio with a minimum 24-inch box tree. Net new trees located within the parkway of the adjacent public-right(s)-of-way may be counted toward replacement tree requirements.
- Removal or planting of any tree in the public right-of-way shall require approval of the Board of Public Works. All trees in the public right-of-way shall be provided in the current standards of the Urban Forestry Division of the Department of Public Works, Bureau of Street Services.

4-4 ***Protected Trees***

- All protected tree removals shall require approval from the Board of Public Works.
- A Tree Report shall be submitted to the Urban Forestry Division of the Bureau of Street Services, Department of Public Works, for review and approval prior to implementation of the Report's recommended measures.
- According to the City's Protected Tree Ordinance, a minimum of four protected trees (a minimum of 15 gallon in size) shall be planted for each protected tree that is removed.. The size of each replacement tree shall measure at least one inch or more in diameter at a point one foot above the base, and not less than seven feet in height, measured from the base.
- In consultation with the Division of Urban Forestry, twenty-five percent of the protected trees removed shall be replaced with 15-gallon *Juglans californica*.
- The location of the trees planted for the purposes of replacing a removed protected tree shall be clearly indicated on the required landscape plan, which shall also indicate the replacement tree species and further contain the phrase "Replacement Tree" in its description.

4-5 ***Previously Removed Trees***

- The 8 qualifying previously removed protected trees shall be replaced at a ratio of one *Juglans californica* and three *Quercus* for each of the 8 trees, at a minimum of 15 gallon in size. The specific size and species of the trees to be planted as replacement for the protected trees being removed shall be determined by the Urban Forestry Division.

4-6 All Trees

- **Protection Barrier:** A protection barrier shall be installed around the construction area as shown on the map included in the Tree Preservation Report (refer to Appendix A). The barrier shall be 6-foot-high chain-link fencing. Twelve-inch-high silt fence shall be attached to the base of the fence with the bottom edge buried 1-2 inches. The barrier may be placed on the line shown on the map or closer to construction, but not further. The fencing shall be maintained in good repair throughout the duration of the Project, and shall not be removed, relocated, or encroached upon without permission of the arborist involved.
- **Storage of Materials:** There shall be NO storage of materials or supplies of any kind inside the area of the protection fencing. Concrete and cement materials, block, sand and soil shall not be placed within the drip-line of any tree to remain.
- **Fuel Storage:** Fuel storage shall NOT be permitted within 150 feet of any tree to be preserved. Refueling, servicing and maintenance of equipment and machinery shall NOT be permitted within 150 feet of protected trees.
- **Debris and Waste Materials:** Debris and waste from construction or other activities shall NOT be permitted outside the construction area. Wash down of concrete or cement handling equipment, in particular, shall NOT be permitted within 150 feet of protected trees.
- **Planting near Trees Designated for Protection:** Any digging within designated protection zones shall be done using supersonic air directly as the digging medium, by means of a nozzle, whose nominal rated input pressure (available from manufacturer's literature) must not exceed 130 psig (pounds per square inch at gage) unless otherwise approved. Nozzles designed for input above 130 psig can damage fine roots. Air compressors rated between 100 to 125 psig recommended.
- **Grade Changes:** Any grade changes within the protection radius listed should be approved by a Registered Consulting Arborist before construction begins, and precautions taken to mitigate potential injuries. Grade changes can be particularly damaging to trees. Even as little as two inches of fill can cause the death of a tree. Lowering the grade can destroy major portions of a root system.
- **Damages:** Any tree damages or injuries should be reported to the project arborist as soon as possible. Severed roots shall be cut cleanly to healthy tissue, using proper pruning tools. Broken branches or limbs shall be pruned according to International Society of Arboriculture Pruning Guidelines and ANSI A-300 Pruning Standards.

- Preventive Measures: Pruning of the tree canopies and branches should be done at the direction of the project arborist to remove any dead or broken branches, and to provide any necessary clearances for the construction work or equipment.

Cumulative Impacts

With the exception of related project #3 shown on Table IV-38 included later in this document, all of the site of the related projects are either currently developed and/or have previously been developed. The site of related project #3 appears to be relatively undisturbed and could contain biological resources. Similar to the Project, the City will require the Project Applicant to conduct or have conducted an assessment of the site to determine the degree to which biological resources occur at the site and identified (if necessary) mitigation measures to reduce any potentially significant impacts to less than significant levels. As discussed previously, no sensitive natural communities or wetlands occur at the Project site, and the site is not within a migratory corridor. Similar to any site that includes trees, the Project has the potential to result in impacts related to nesting birds, roosting bats, and tree removal at the Project site. However, with implementation of Mitigation Measures 4-1 and 4-2 and Mitigation Measures 1-1 through 1-4, Project impacts related to biological resources would be less than significant. Any of the related projects that will involve the removal of trees also would be required to implement mitigation measures similar to those identified for the Project to reduce potential associated significant impacts to less than significant levels. For these reasons, cumulative impacts related to biological resources would be less than significant.

5. CULTURAL RESOURCES

- a) **Would the project cause a substantial adverse change in the significance of a historical resource as defined in §15064.5?**

No Impact. No historical resources are located at the Project site. No historical resources would be affected by the Project, and no impacts related to this issue would occur.

- b) **Would the project cause a substantial adverse change in the significance of an archaeological resource pursuant to 15064.5?**

Less Than Significant Impact. The Project site is vacant and does not contain any structures, but has been developed in the past. Based on a records search conducted by the South Central Coast Information Center (refer to Appendix D), no archaeological sites have been recorded within the Project site. However, it is possible that unknown archaeological resources could exist at the Project site, given that significant archaeological resources have been identified in the Los Angeles area. As such, prior to Project construction, the prime contractor and any subcontractor(s) shall be advised of the legal and/or regulatory implications of knowingly destroying cultural resources or removing artifacts, human remains, bottles, and other cultural materials from the Project site. In addition, in the event that buried archaeological resources are exposed during Project construction, work within 50 feet of the find shall stop until a professional archaeologist, meeting the standards of the Secretary of the Interior, can identify

and evaluate the significance of the discovery and develop recommendations for treatment, in conformance with California Public Resources Code Section 21083.2. However, construction activities could continue in other areas of the Project site. Recommendations could include preparation of a Treatment Plan, which could require recordation, collection and analysis of the discovery; preparation of a technical report; and curation of the collection and supporting documentation in an appropriate depository. Any Native American remains shall be treated in accordance with state law. Through compliance with these requirements, potential Project impacts to unknown archaeological resources would be less than significant.

c) Would the project directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?

Less Than Significant Impact. A records search was conducted with the Los Angeles County Natural History Museum to determine the likelihood for unique paleontological resources to occur at the Project site (refer to Appendix D). The records search revealed that no paleontological resources are known to exist at the Project site. However, fossils have been found in the sedimentary deposits that exist within the Project area and at the Project site.¹⁸ Thus, it is possible that unknown resources could be encountered during the Project's excavation phase. However, prior to Project construction, the prime contractor and any subcontractor(s) shall be advised of the legal and/or regulatory implications of knowingly destroying paleontological or unique geologic resources or sites from the Project site. In addition, in the event that paleontological resources or sites, or unique geologic features are exposed during Project construction, work within 50 feet of the find shall stop until a professional paleontologist, can identify and evaluate the significance of the discovery and develop recommendations for treatment. However, construction activities could continue in other areas of the Project site. Recommendations could include a preparation of a Treatment Plan, which could require recordation, collection, and analysis of the discovery; preparation of a technical report; and curation of the collection and supporting documentation in an appropriate depository. Any paleontological resources or sites, or unique geologic features shall be treated in accordance with State Law. Through compliance with these requirements, potential Project impacts to unknown paleontological resources or sites, or unique geologic features would be less than significant.

d) Would the project disturb any human remains, including those interred outside of formal cemeteries?

Less Than Significant Impact. The Project site is vacant and does not contain any structures. No human remains are known to exist at the Project site. However, in accordance with the State's Health and Safety Code Section 7050.5, in the event of discovery or recognition of any human remains at the Project

¹⁸ Los Angeles County Natural History Museum, *Paleontological Resources Search Letter*, October 27, 2014 (refer to Appendix D).

site, no further excavation or disturbance of the site or any nearby area reasonably suspected to overlie adjacent remains shall occur until the Los Angeles County Coroner has determined, in accordance with Chapter 10 (commencing with Section 27460) of Part 3 of Division 2 of Title 3 of the Government Code, that the remains are not subject to the provisions of Section 27491 of the Government Code or any other related provisions of law concerning investigation of the circumstances, manner, and cause of any death, and the recommendations concerning the treatment and disposition of the human remains have been made to the person responsible for the excavation, or to his or her authorized representative, in the manner provided in Section 5097.98 of the Public Resources Code. The coroner shall make his or her determination within two working days from the time the person responsible for the excavation, or his or her authorized representative, notifies the coroner of the discovery or recognition of the human remains. If the coroner determines that the remains are not subject to his or her authority and if the coroner recognizes the human remains to be those of a Native American, or has reason to believe that they are those of a Native American, he or she shall contact, by telephone within 24 hours, the Native American Heritage Commission. Through compliance with this regulation, potential Project impacts to human remains would be less than significant.

Cumulative Impacts

Impacts related to cultural resources are site-specific and are assessed on a site-by-site basis. All development in the City (including the proposed Project and the related projects) that involves ground-disturbing activities is required to implement the City's Standard Conditions of Approval related to archaeological and paleontological resources. Additionally, these projects are required to comply with State's Health and Safety Code Section 7050.5 in the event of discovery or recognition of any human remains. Through compliance with existing requirements, cumulative impacts related to cultural resources would be less than significant.

6. GEOLOGY AND SOILS

a) Would the project expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:

(i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42?

No Impact. The Project site is not located within an Alquist-Priolo Earthquake Fault Zone, and no known faults exist on the Project site.¹⁹ The fault closest to the Project site is the Upper Elysian Park fault, located approximately 0.8 mile from the Project site. Thus, the Project would not expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault on the Project site. Therefore, no significant impacts related to this issue would occur.

(ii) Strong seismic ground shaking?

Less Than Significant Impact. Given the Project site's location in a seismically active region, the Project site could experience seismic groundshaking in the event of an earthquake. However, the Project Applicant would be required to design and construct the Project in conformance to the most recently adopted Building Code and applicable recommendations made in a Final Geotechnical Report prepared for the Project. Conformance with the City's current Building Code requirements would minimize the potential for structural failure, injury, and loss of life during an earthquake event and thus, not cause or accelerate geologic hazards or expose people to substantial risk of injury. Therefore, Project impacts related to groundshaking would be less than significant.

(iii) Seismic-related ground failure, including liquefaction?

No Impact. A portion of the Project site is delineated by the state to be conducive to liquefaction. However, according to the Geologic & Geotechnical Engineering Review prepared for the Project, following grading of the site that occurs during the Project's construction phase, the site would be underlain by compacted fill placed on dense older alluvium and bedrock, and liquefaction would not pose a threat to the Project site. Therefore, no significant impacts related to this issue would occur.

¹⁹ *Geologic & Geotechnical Engineering Review, GeoSoils Consultants, Inc. January 2015. (Refer to Appendix E.)*

(iv) **Landslides?**

Less Than Significant Impact. The Project site contains hillsides and is located in an area with known landslides.²⁰ However, slope stability analyses conducted at the Project site indicate factors of safety above minimum Building Code values. Additionally, the Project Applicant would be required to design and construct the Project in conformance to the LAMC and applicable recommendations made in a Final Geotechnical Report prepared for the Project. Conformance with the City's current Building Code requirements would minimize the potential for structural failure, injury, and loss of life during an earthquake event and thus, not cause or accelerate geologic hazards or expose people to substantial risk of injury. Therefore, Project impacts related to landslides would be less than significant.

b) **Would the project result in substantial soil erosion or the loss of topsoil?**

Less Than Significant Impact. During the Project's construction phase, the Project developer would be required to implement SCAQMD Rule 403 – Fugitive Dust to minimize wind and water-borne erosion at the site. Also, the Project developer would be required to prepare and implement a Stormwater Pollution Prevention Plan (SWPPP), in accordance with the National Pollutant Discharge Elimination System (NPDES) General Permit for Discharges of Storm Water Associated with Construction Activity and Land Disturbance Activities. The site-specific SWPPP would be prepared prior to earthwork activities and would be implemented during Project construction. The SWPPP would include BMPs and erosion control measures to prevent pollution in storm water discharge. Typical BMPs that could be used during construction include good-housekeeping practices (e.g., street sweeping, proper waste disposal, vehicle and equipment maintenance, concrete washout area, materials storage, minimization of hazardous materials, proper handling and storage of hazardous materials, etc.) and erosion/sediment control measures (e.g., silt fences, fiber rolls, gravel bags, storm water inlet protection, and soil stabilization measures, etc.). The SWPPP would be subject to review and approval by the City for compliance with the City's Development Best Management Practices Handbook, Part A, Construction Activities. Additionally, all Project construction activities would comply with the City's grading permit regulations, which require the implementation of grading and dust control measures, including a wet weather erosion control plan if construction occurs during rainy season, as well as inspections to ensure that sedimentation and erosion is minimized. Through compliance with these existing regulations, the Project would not result in any significant impacts related to soil erosion during the construction phase. Additionally, during the Project's operational phase, most of the Project site would be developed with impervious surface, and all stormwater flows would be directed to storm drainage features and would not come into contact with bare soil surfaces. Thus, no significant impacts related to erosion would occur as a result of Project operation.

²⁰ *Ibid*

c) Would the project be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?

Less Than Significant Impact. Considering that the Project site includes hillside areas and the Project would include cut and fill slopes, unstable soils could be encountered at the Project site. However, as discussed previously, the Project Applicant would be required to prepare (or have prepared) a Final Geotechnical Report that would address the building standards and recommendations that shall be followed in order to develop the Project building in accordance with building standards that apply to building within the types of soils found at the site, including areas prone to landslide. Through compliance with the City's building code and recommendations of a Final Geotechnical Report, impacts related to soil instability would be less than significant.

d) Would the project be located on expansive soil, as identified on Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?

Less Than Significant Impact. According to Ray A. Eastman (refer to Appendix E), soils at the Project site have a high expansive potential. As stated previous, the Project Applicant would be required to prepare (or have prepared) a Final Geotechnical Report that would address the building standards and recommendations that shall be followed in order to develop the Project building in accordance with building standards that apply to building within the types of soils (including expansive soils) found at the site. Through compliance with the City's building code and recommendations of a Final Geotechnical Report, impacts related to expansive soils would be less than significant.

e) Would the project have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?

No Impact. The Project would connect to the City's existing sewer system and would not require the use of septic tanks or alternative wastewater disposal systems. Thus, the Project would not result in any impacts related to soils that are incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater. Therefore, no impacts related to this issue would occur.

Cumulative Impacts

Geotechnical impacts related to future development in the City involve hazards related to site-specific soil conditions, erosion, and ground-shaking during earthquakes. The impacts on each site are specific to that site and its users and would not be in common or contribute to (or shared with, in an additive sense) the impacts on other sites. In addition, development on each site is subject to uniform site development and construction standards that are designed to protect public safety. Therefore, cumulative geotechnical impacts related would be less than significant.

7. GREENHOUSE GAS EMISSIONS

a) Would the project generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?

Less Than Significant Impact. The analysis of Project impacts related to greenhouse gas (GHG) emissions is based on modeling results, prepared by DKA Planning, Inc. (refer to Appendix B).

Background

Various gases in the Earth's atmosphere, classified as atmospheric GHG emissions, play a critical role in determining the Earth's surface temperature. Solar radiation entering Earth's atmosphere is absorbed by the Earth's surface. When the Earth emits this radiation back toward space, the radiation changes from high-frequency solar radiation to lower-frequency infrared radiation. GHG emissions are transparent to solar radiation and absorb infrared radiation. As a result, radiation that otherwise would escape back into space is now retained, warming the atmosphere. This phenomenon is known as the greenhouse effect.

GHG emissions that contribute to the greenhouse effect include:

- Carbon Dioxide (CO₂) is released to the atmosphere when solid waste, fossil fuels (oil, natural gas, and coal), and wood and wood products are burned. CO₂ emissions from motor vehicles occur during operation of vehicles and operation of air conditioning systems. CO₂ comprises over 80 percent of GHG emissions in California.²¹
- Methane (CH₄) is emitted during the production and transport of coal, natural gas, and oil. Methane emissions also result from the decomposition of organic waste in solid waste landfills, raising livestock, natural gas and petroleum systems, stationary and mobile combustion, and wastewater treatment. Mobile sources represent 0.5 percent of overall methane emissions.²²
- Nitrous Oxide (N₂O) is emitted during agricultural and industrial activities, as well as during combustion of solid waste and fossil fuels. Mobile sources represent about 14 percent of N₂O

²¹ California Environmental Protection Agency, *Climate Action Team Report to Governor Schwarzenegger and the Legislature*, March 2006, p. 11.

²² United States Environmental Protection Agency, *Inventory of U.S. Greenhouse Gas Emissions and Sinks, 1990-2003*, April 2005 (EPA 430-R-05-003)

emissions.²³ N₂O emissions from motor vehicles generally occur directly from operation of vehicles.

- Hydrofluorocarbons (HFCs) are one of several high global warming potential (GWP) gases that are not naturally occurring and are generated from industrial processes. HFC (refrigerant) emissions from vehicle air conditioning systems occur due to leakage, losses during recharging, or release from scrapping vehicles at end of their useful life.
- Perfluorocarbons (PFCs) are another high GWP gas that are not naturally occurring and are generated in a variety of industrial processes. Emissions of PFCs are generally negligible from motor vehicles.
- Sulfur Hexafluoride (SF₆) is another high GWP gas that is not naturally occurring and are generated in a variety of industrial processes. Emissions of SF₆ are generally negligible from motor vehicles.

For most non-industrial development projects, motor vehicles make up the bulk of GHG emissions, particularly carbon dioxide, methane, nitrous oxide, and HFCs.²⁴ The other GHGs are less abundant but have higher GWP than CO₂ (refer to Table IV-7). To account for this higher potential, emissions of other GHGs are frequently expressed in the equivalent mass of CO₂, denoted as CO₂e. Expressing GHG emissions in carbon dioxide equivalents takes the contribution of all GHG emissions to the greenhouse effect and converts them to a single unit equivalent to the effect that would occur if only CO₂ were being emitted. High GWP gases such as HFCs, PFCs, and SF₆ are the most heat-absorbent.

Table IV-7
Global Warming Potential for Greenhouse Gases

Greenhouse Gas	Global Warming Potential
Carbon Dioxide (CO ₂)	1
Methane (CH ₄)	28
Nitrous Oxide (N ₂ O)	265
Hydrofluorocarbons (HFCs), Perfluorocarbons (PFCs)	7,000 – 11,000
Sulfur Hexafluoride (SF ₆)	23,500
<i>Source: California Air Resources Board, First Update to the Climate Change Scoping Plan. May 2014.</i>	

²³ United States Environmental Protection Agency, *U.S. Adipic Acid and Nitric Acid N₂O Emissions 1990-2020: Inventories, Projections and Opportunities for Reductions*, December 2001

²⁴ California Air Resources Board, *Climate Change Emission Control Regulations*, 2004

The effects of increasing global temperature are difficult to quantify. In general, increases in the ambient global temperature as a result of increased GHGs is anticipated to result in rising sea levels which could threaten coastal areas through accelerated coastal erosion, threats to levees and inland water systems and disruption to coastal wetlands and habitat.

If the temperature of the ocean warms, it is anticipated that the winter snow season would be shortened. Snowpack in the Sierra Nevada provides both water supply (runoff) and storage (within the snowpack before melting), which is a major source of supply for the state. This phenomenon could lead to significant challenges securing an adequate water supply for a growing state population. Further, the increased ocean temperature could result in increased moisture flux into the state; however, since this would likely increasingly come in the form of rain rather than snow in the high elevations, increased precipitation could lead to increased potential and severity of flood events, placing more pressure on California's levee/flood control system. If sea level rise occurs, resultant effects could include increased coastal flooding, saltwater intrusion and disruption of wetlands. As the existing climate throughout California changes over time, migration or failure of species to migrate in time to adapt to the perturbations in climate, could also result.

While efforts to reduce the rate of GHG emissions continue, the State has developed a strategy to begin the process of adapting the State's infrastructure to the impacts of climate change. The 2009 California Climate Adaptation Strategy analyzed risks and vulnerabilities and proposes strategies to reduce risks. The Strategy began an ongoing process of adaptation, as directed by Governor Schwarzenegger's Executive Order S-13-08. The Strategy analyzed two components of climate change: (1) projecting the amount of climate change that may occur using computer-based global climate models and (2) assessing the natural or human systems' abilities to cope with and adapt to change by examining past experience with climate variability and extrapolating from this to understand how the systems may respond to the additional impact of climate change. The Strategy's key preliminary adaptation recommendations included the following:

- Appointment of a Climate Adaption Advisory Panel;
- Improved water management in anticipation of reduced water supplies, including a 20 percent reduction in per capita water use by 2020 from 2011 levels;
- Consideration of project alternatives that avoid significant new development in areas that cannot be adequately protected from flooding due to climate change;
- Preparation of agency-specific adaptation plans, guidance or criteria by September 2010;
- Consideration of climate change impacts for all significant State projects;
- Assessment of climate change impacts on emergency preparedness;

- Identification of key habitats and development of plans to minimize adverse effects from climate change;
- Development of guidance by the California Department of Public Health by September 2010 for use by local health departments to assess adaptation strategies;
- Amendment of General Plans and Local Coastal Plans to address climate change impacts and to develop local risk reduction strategies; and
- Inclusion of climate change impact information into fire program planning by State fire fighting agencies.

Regulatory Setting

International

Kyoto Protocol

In 1988, the United Nations established the Intergovernmental Panel on Climate Change to evaluate the impacts of global warming and to develop strategies that nations could implement to curtail global climate change. In 1992, the United States (the “U.S.”) joined other countries around the world in signing the United Nations’ Framework Convention on Climate Change (the “UNFCCC”) agreement with the goal of controlling greenhouse gas emissions. As a result, the Climate Change Action Plan was developed to address the reduction of GHG emissions in the U.S. The plan currently consists of more than 50 voluntary programs for member nations to adopt.

The Kyoto Protocol (the “Protocol”) is a treaty made under the UNFCCC and was the first international agreement to regulate GHG emissions. Some have estimated that if the commitments outlined in the Protocol are met, global GHG emissions could be reduced an estimated five percent from 1990 levels during the first commitment period of 2008-2012. Notably, while the U.S. is a signatory to the Kyoto protocol, Congress has not ratified the Protocol and the U.S. is not bound by the Protocol’s commitments. In December 2009, international leaders from 192 nations met in Copenhagen to address the future of international climate change commitments post-Protocol.

The major feature of the Protocol is that it sets binding targets for 37 industrialized countries and the European community for reducing GHG emissions. The targets amount to an average of five percent reduction levels against 1990 levels over the five-year period 2008-2012. The major distinction between the Protocol and the UNFCCC is that while the UNFCCC encouraged industrialized countries to stabilize GHG emissions, the Protocol commits them to do so. Recognizing that developed countries are principally responsible for the current high levels of GHG emissions in the atmosphere as a result of more than 150 years of industrial activity, the Protocol places a heavier burden on developed nations under the principle of “common but differentiated responsibilities.”

On December 12, 2015, a Conference of the Parties to the UNFCCC and the 11th session of the Kyoto Protocol negotiated an agreement in Paris that would keep the rise of temperature below 2 degrees Celsius. While 186 countries published their action plans detailing how they plan to reduce their GHG emissions, these reductions would still result in up to three degrees Celsius of global warming. The Paris agreement asks all countries to review their plans every five years from 2020, acknowledges that \$100 billion is needed each year to enable countries to adapt to climate change. The agreement was signed into law on April 22, 2016.

The Western Regional Climate Action Initiative (WCI)

The Western Regional Climate Action Initiative (the “WCI”) is a partnership among seven states, including California, and four Canadian provinces to implement a regional, economy-wide cap-and-trade system to reduce global warming pollution. The WCI will cap GHG emissions from the region’s electricity, industrial, and transportation sectors with the goal to reduce the heat trapping emissions that cause global warming to 15 percent below 2005 levels by 2020. When the WCI adopted this goal in 2007, it estimated that this would require 2007 levels to be reduced worldwide between 50 percent and 85 percent by 2050. California is working closely with the other states and provinces to design a regional GHG reduction program that includes a cap-and-trade approach. The California Air Resources Board’s (CARB) planned cap and-trade program, discussed below, is also intended to link California and the other member states and provinces.

Federal

The United States Environmental Protection Agency (the “U.S. EPA”) has historically not regulated GHGs because it determined the Clean Air Act did not authorize it to regulate emissions that addressed climate change. In 2007, the U.S. Supreme Court found that GHGs could be considered within the Clean Air Act’s definition of a pollutant.²⁵ In December 2009, U.S. EPA issued an endangerment finding for GHGs under the Clean Air Act, setting the stage for future regulation. In September 2009, the National Highway Traffic Safety Administration and U.S. EPA announced a joint rule that would tie fuel economy to GHG emission reduction requirements.

In June 2013, President Obama announced a Climate Action Plan that calls for a number of initiatives, including funding \$8 billion in advanced fossil energy efficiency projects, calls for federal agencies to develop new emission standards for power plants, invests in renewable energy sources, calling for adaptation programs, and leading international efforts to address climate change. In September 2013, U.S. EPA announced its first steps to implement a portion of the Obama Climate Action Plan by proposing carbon pollution standards for new power plants.

²⁵ *Massachusetts v. Environmental Protection Agency et al* [127 S. Ct. 1438 (2007)]

Vehicle Standards

Other regulations have been adopted to address vehicle standards including the U.S. EPA and National Highway Traffic Safety Administration (the “NHTSA”) joint rulemaking for vehicle standards.

- On March 30, 2009, the NHTSA issued a final rule for model year 2011.²⁶
- On May 7, 2010, the U.S. EPA and the NHTSA issued a final rule regulating fuel efficiency and GHG emissions pollution from motor vehicles for cars and light-duty trucks for model years 2012–2016.²⁷
- On August 9, 2011, U.S. EPA and NHTSA issued a Supplemental Notice of Intent announcing plans to propose stringent, coordinated federal GHG emissions and fuel economy standards for model year 2017–2025 light-duty vehicles.²⁸
- NHSTA intends to set standards for model years 2022–2025 in a future rulemaking.²⁹
- In addition to the regulations applicable to cars and light-duty trucks, on August 9, 2011, the U.S. EPA and the NHTSA announced fuel economy and GHG emissions standards for medium- and heavy-duty trucks that applies to vehicles from model year 2014–2018.³⁰

Energy Independence and Security Act (the “EISA”)

Among other key measures, the EISA would do the following, which would aid in the reduction of national GHG emissions, both mobile and non-mobile:

²⁶ NHSTA. 2009. *Average Fuel Economy Standards Passenger Cars and Light Trucks Model Year 2011, Final Rule*. 75 Fed. Reg. 25324.

²⁷ U.S. EPA. 2010. *Light Duty Vehicle Greenhouse Gas Emission Standards and Corporate Average Fuel Economy Standards, Final Rule*. 75 Fed. Reg. 25324.

²⁸ Available: <http://www.gpo.gov/fdsys/pkg/FR-2011-08-09/pdf/2011-19905.pdf>. Accessed May 2014.

²⁹ NHSTA. 2012. *2017 and Later Model Year Light-Duty Vehicle Greenhouse Gas Emissions and Corporate Average Fuel Economy Standards*. 77 Fed. Reg. 62624.

³⁰ U.S. EPA Office of Transportation and Air Quality. 2011. *EPA and NHTSA Adopt First-Ever Program to Reduce Greenhouse Gas Emissions and Improve Fuel Efficiency of Medium- and Heavy-Duty Vehicles*. Available: <http://www.epa.gov/otaq/climate/documents/420f11031.pdf>. Accessed May 2014.

- 1) Increase the supply of alternative fuel sources by setting a mandatory Renewable Fuel Standard (RFS) requiring fuel producers to use at least 36 billion gallons of biofuel in 2022.
- 2) Prescribe or revise standards affecting regional efficiency for heating and cooling products, procedures for new or amended standards, energy conservation, energy efficiency labeling for consumer electronic products, residential boiler efficiency, electric motor efficiency, and home appliances.
- 3) While superseded by NHTSA and U.S. EPA actions described above, EISA also set miles per gallon targets for cars and light trucks and directed the NHTSA to establish a fuel economy program for medium- and heavy-duty trucks and create a separate fuel economy standard for work trucks.

Additional provisions of the EISA address energy savings in government and public institutions, promoting research for alternative energy, additional research in carbon capture, international energy programs, and the creation of “green jobs.”

State

Assembly Bill 1493

California has adopted a series of laws and programs to reduce emissions of GHGs into the atmosphere. Assembly Bill (AB) 1493 was enacted in September 2003 and requires regulations to achieve “the maximum feasible reduction of greenhouse gases” emitted by vehicles used for personal transportation. The Team reported several recommendations and strategies for reducing GHG emissions and reaching the targets established in the Executive Order.³¹ Furthermore, the report provided to Governor Schwarzenegger in 2006, referenced above, indicated that smart land use and increased transit availability should be a priority in the State of California.³² According to the California Climate Action Team, smart land use is an umbrella term for strategies that integrate transportation and land-use decisions. Such strategies generally encourage jobs/housing proximity, promote transit-oriented development (TOD), and encourage high-density residential/commercial development along transit corridors. These strategies develop more efficient land-use patterns within each jurisdiction or region to match population increases, workforce, and socioeconomic needs for the full spectrum of the population.

³¹ California Climate Action Team, *Climate Action Team Report to Governor Schwarzenegger and the Legislature*, March 2006.

³² California Climate Action Team, *Climate Action Team Report to Governor Schwarzenegger and the Legislature*, March 2006, p. 57.

Executive Order B-30-15

On April 29, 2015, Governor Brown issued an executive order setting a Statewide GHG reduction target of 40 percent below 1990 levels by 2030. This action aligns the State's GHG targets with those set in October 2014 by the European Union and is intended to help the State meet its target of reducing GHG emissions 80 percent below 1990 levels by 2050. The measure calls on State agencies to implement measures accordingly and directs CARB to update the Climate Change Scoping Plan.

A recent study shows that the State's existing and proposed regulatory framework will allow the State to reduce its GHG emissions level to 40 percent below 1990 levels by 2030 (consistent with Executive Order B-30-15), and to 60 percent below 1990 levels by 2050. Even though this study did not provide an exact regulatory and technological roadmap to achieve the 2030 and 2050 goals, it demonstrated that various combinations of policies could allow the statewide emissions level to remain very low through 2050, suggesting that the combination of new technologies and other regulations not analyzed in the study could allow the State to meet the 2030 and 2050 targets.³³

Executive Order S-3-05

On June 1, 2005, Governor Schwarzenegger issued Executive Order S-3-05, which set the following GHG emission reduction targets: by 2010, reduce GHG emissions to 2000 levels; by 2020, reduce GHG emissions to 1990 levels; and by 2050, reduce GHG emissions to 80 percent below 1990 levels. The California Environmental Protection Agency (Cal EPA) formed a Climate Action Team ("CAT") that recommended strategies that can be implemented by state agencies to meet GHG emissions targets. The Team reported several recommendations and strategies for reducing GHG emissions and reaching the targets established in the Executive Order.³⁴ Furthermore, the report provided to Governor Schwarzenegger in 2006, referenced above, indicated that smart land use and increased transit availability should be a priority in the State of California.³⁵ According to the California Climate Action Team, smart land use is an umbrella term for strategies that integrate transportation and land-use decisions. Such strategies generally encourage jobs/housing proximity, promote transit-oriented development (TOD), and encourage high-density residential/commercial development along transit corridors. These strategies

³³ Greenblatt, Jeffrey, *Energy Policy, "Modeling California Impacts on Greenhouse Gas Emissions"* (Vol. 78, pp. 158-172).

³⁴ *California Climate Action Team, Climate Action Team Report to Governor Schwarzenegger and the Legislature, March 2006.*

³⁵ *California Climate Action Team, Climate Action Team Report to Governor Schwarzenegger and the Legislature, March 2006, p. 57.*

develop more efficient land-use patterns within each jurisdiction or region to match population increases, workforce, and socioeconomic needs for the full spectrum of the population.

Assembly Bill 32

In September 2006, AB 32 was signed into law by Governor Arnold Schwarzenegger, focusing on achieving GHG emissions equivalent to statewide levels in 1990 by 2020. It mandates that CARB establish a quantified emissions cap, institute a schedule to meet the cap, implement regulations to reduce statewide GHG emissions from stationary sources, and develop tracking, reporting, and enforcement mechanisms to ensure that reductions are achieved. A companion bill, Senate Bill (SB) 1368, requires the California Public Utilities Commission and the California Energy Commission to establish GHG emission performance standards for the generation of electricity. These standards will also apply to power that is generated outside of California and imported into the state.

AB 32 charges CARB with the responsibility to monitor and regulate sources of GHG emissions. On June 1, 2007, CARB adopted three early action measures: setting a low carbon fuel standard, reducing refrigerant loss from motor vehicle air conditioning maintenance, and increasing methane capture from landfills.³⁶ On October 25, 2007, CARB approved measures improving truck efficiency (i.e., reducing aerodynamic drag), electrifying port equipment, reducing PFCs from the semiconductor industry, reducing propellants in consumer products, promoting proper tire inflation in vehicles, and reducing sulfur hexafluoride emissions from the non-electricity sector. CARB determined that the total statewide aggregated GHG 1990 emissions level and 2020 emissions limit is 427 million metric tons of CO₂e. The 2020 target reductions are currently estimated to be 174 million metric tons of CO₂e.

CARB developed an AB 32 Scoping Plan that contains strategies to achieve the 2020 emissions cap. This Scoping Plan, which was developed by CARB in coordination with the Climate Action Team, was first published in October 2008 (the “2008 Scoping Plan”). The 2008 Scoping Plan proposed a comprehensive set of actions designed to reduce overall GHG emissions in California, improve the environment, reduce the state’s dependence on oil, diversify the state’s energy sources, save energy, create new jobs, and enhance public health. An important component of the plan is a cap-and-trade program covering 85 percent of the state’s emissions. Additional key recommendations of the 2008 Scoping Plan include strategies to enhance and expand proven cost-saving energy efficiency programs; implementation of California’s clean cars standards and increasing the amount of clean and renewable energy used to power the state. Furthermore, the 2008 Scoping Plan proposes full deployment of the California Solar Initiative, high-speed rail, water-related energy efficiency measures, and a range of regulations to reduce emissions from trucks and from ships docked in California ports. As required by AB 32, CARB must update its Scoping Plan every five years to ensure that California remains on the path toward a low carbon future.

³⁶ California Air Resources Board, *Proposed Early Action Measures to Mitigate Climate Change in California*, April 20, 2007.

In order to assess the scope of reductions needed to return to 1990 emissions levels, CARB first estimated the 2020 business-as-usual (BAU) GHG emissions in the 2008 Scoping Plan. These are the GHG emissions that would be expected to result if there were no GHG emissions reduction measures, and as if the state were to proceed on its pre-AB 32 GHG emissions track. After estimating that statewide 2020 BAU GHG emissions would be 596 metric tons, the 2008 Scoping Plan then identified recommended GHG emissions reduction measures that would reduce BAU GHG emissions by approximately 174 metric tons (an approximately 28.35 percent reduction) by 2020.

On May 22, 2014, CARB approved its first update to the AB 32 Scoping Plan, recalculating 1990 GHG emissions using IPCC Fourth Assessment Report (AR4) released in 2007. It states that based on the AR4 global warming potentials, the 427 MMTCO₂e 1990 emissions level and 2020 GHG emissions limit would be slightly higher than identified in the Scoping Plan, at 431 MMTCO₂e. Based on the revised estimates of expected 2020 emissions identified in the 2011 supplement to the FED and updated 1990 emissions levels identified in the draft first update to the Scoping Plan, achieving the 1990 emission level would require a reduction of 76 MMTCO₂e (down from 507 MMTCO₂e) or a reduction by approximately 15.3 percent (down from 28.4 percent) to achieve in 2020 emissions levels in the BAU condition.

CARB's First Update "lays the foundation for establishing a broad framework for continued emission reductions beyond 2020, on the path to 80 percent below 1990 levels by 2050," and many of the emission reduction strategies recommended by CARB would serve to reduce the Project's post-2020 emissions level to the extent applicable by law by focusing on reductions from several sectors.^{37,38}

As shown on Table IV-8, these reductions are to come from a variety of sectors, including energy, transportation, high-global warming potential sources, waste, and the State's cap-and-trade emissions program.

Nearly all reductions are to come from sources that are controlled at the statewide level by State agencies, including the Air Resources Board, Public Utilities Commission, High Speed Rail Authority, and California Energy Commission. The few actions that are directly or indirectly associated with local government control are in the Transportation sector, which is charged with reducing 4.5 percent of baseline 2020 emissions. Of these actions, only one (GHG reductions through coordinated planning) specifically identifies local governments as the responsible agency.

³⁷ CARB, *First Update*, p. 4, May 2014. See also *id.* at pp. 32–33 [recent studies show that achieving the 2050 goal will require that the "electricity sector will have to be essentially zero carbon; and that electricity or hydrogen will have to power much of the transportation sector, including almost all passenger vehicles."]

³⁸ CARB, *First Update*, Table 6: *Summary of Recommended Actions by Sector*, pp. 94-99, May 2014.

Cap And Trade

CARB adopted a California Cap-and-Trade Program pursuant to its authority under AB 32. The Cap-and-Trade Program is designed to reduce GHG emissions from major sources (deemed “covered entities”) by setting a firm cap on statewide GHG emissions and employing market mechanisms to achieve AB 32's emission-reduction mandate of returning to 1990 levels of emissions by 2020. The statewide cap for GHG emissions from the capped sectors (e.g., electricity generation, petroleum refining, and cement production) commenced in 2013 and will decline over time, achieving GHG emission reductions throughout the program's duration.

**Table IV-8
Emissions Reductions Needed to Meet AB 32 Objectives in 2020**

Sector	Million Metric Tons of CO ₂ e Reduction	Percent of Statewide CO ₂ e Inventory	Summary of Recommended Actions
Energy	-25	-4.9%	Reduce State's electric and energy utility emissions, reduce emissions from large industrial facilities, control fugitive emissions from oil and gas production, reduce leaks from industrial facilities
Transportation	-23	-4.5%	Phase 2 heavy-duty truck GHG standards, ZEV action plan for trucks, construct High Speed rail system from SF to LA, coordinated land use planning, Sustainable Freight Strategy
High Global Warming Potential	-5	-1.0%	Reduce use of high-GWP compounds from refrigeration, air conditioning, aerosols
Waste	-2	-0.4%	Eliminate disposal of organic materials at landfills, in-State infrastructure development, address challenges with composting and anaerobic digestion, additional methane control and landfills
Cap and Trade Reductions	-23	-4.5%	Statewide program that reduces emissions from regulated entities through performance-based targets
Total	-78	-15.3%	

Source: California Environmental Protection Agency, “First Update to the Climate Change Scoping Plan.” May 2014.

Under the Cap-and-Trade Program, covered entities that emit more than 25,000 metric tons CO₂e per year must comply with the Cap-and-Trade Program. Triggering of the 25,000 metric tons CO₂e per year “inclusion threshold” is measured against a subset of emissions reported and verified under the California Regulation for the Mandatory Reporting of Greenhouse Gas Emissions. CARB issues allowances equal

to the total amount of allowable emissions over a given compliance period and distributes these to regulated entities. Covered entities are allocated free allowances in whole or part (if eligible), and may buy allowances at auction, purchase allowances from others, or purchase offset credits.

The Cap-and-Trade Program works with other direct regulatory measures and provides an economic incentive to reduce emissions. If California's direct regulatory measures reduce GHG emissions more than expected, then the Cap-and-Trade Program will be responsible for relatively fewer emissions reductions. If California's direct regulatory measures reduce GHG emissions less than expected, then the Cap-and-Trade Program will be responsible for relatively more emissions reductions. Thus, the Cap-and-Trade Program assures that California will meet its 2020 GHG emissions reduction mandate.

In sum, the Cap-and-Trade Program will achieve aggregate, rather than site-specific or project-level, GHG emissions reductions. Also, due to the regulatory framework adopted by CARB in AB 32, the reductions attributed to the Cap-and-Trade Program can change over time depending on the State's emissions forecasts and the effectiveness of direct regulatory measures.

As of January 1, 2015, the Cap-and-Trade Program covered approximately 85 percent of California's GHG emissions. The Cap-and-Trade Program covers the GHG emissions associated with electricity consumed in California, whether generated in-state or imported. Accordingly, GHG emissions associated with CEQA projects' electricity usage are covered by the Cap-and-Trade Program.

While the 2020 cap would remain in effect post-2020,³⁹ the Cap-and-Trade Program is not currently scheduled to extend beyond 2020 in terms of additional GHG emissions reductions.⁴⁰ However, CARB has expressed its intention to extend the Cap-and-Trade Program beyond 2020 in conjunction with setting a mid-term target. The "recommended action" in the First Update for the Cap-and-Trade Program is: "Develop a plan for a post-2020 Cap-and-Trade Program, including cost containment, to provide market certainty and address a mid-term emissions target."⁴¹ The "expected completion date" for this recommended action is 2017.⁴² It is therefore reasonable to assume that the Cap-and-Trade Program will extend beyond 2020.

³⁹ California Health & Safety Code § 38551(a) ("The statewide greenhouse gas emissions limit shall remain in effect unless otherwise amended or repealed.")

⁴⁰ See AB 1288 (Atkins, introduced 2015) that would eliminate the December 31, 2020, limit on the Cap-and-Trade Program.

⁴¹ CARB, *First Update to the Climate Change Scoping Plan: Building on the Framework*, at 98 (May 2014).

⁴² *Ibid.*

State Bill 1368

Senate Bill (SB) 1368, requires the California Public Utilities Commission and the California Energy Commission to establish GHG emissions performance standards for the generation of electricity. These standards will also apply to power that is generated outside of California and imported into the state.

State Bill 97 and CEQA Guidelines

In August 2007, the California State Legislature adopted Senate Bill 97 (SB 97), requiring the Governor's Office of Planning and Research (OPR) to prepare and transmit new CEQA guidelines for the mitigation of GHG emissions or the effects of GHG emissions to the Resources Agency by July 1, 2009. In response to SB 97, the Governor's Office of Planning and Research (OPR) adopted CEQA guidelines that became effective on March 18, 2010. The amendments provide guidance to public agencies on analysis and mitigation of the effects of GHG emissions in CEQA documents, including the following:

- Lead agencies should quantify all relevant GHG emissions and consider the full range of project features that may increase or decrease GHG emissions as compared to the existing setting;
- Consistency with the CARB Scoping Plan is not a sufficient basis to determine that a project's GHG emissions would not be cumulatively considerable;
- A lead agency may appropriately look to thresholds developed by other public agencies, including the CARB's recommended CEQA thresholds;
- To qualify as mitigation, specific measures from an existing plan must be identified and incorporated into the project. General compliance with a plan, by itself, is not mitigation;
- The effects of GHG emissions are cumulative and should be analyzed in the context of CEQA's requirements for cumulative impact analysis; and
- Given that impacts resulting from GHG emissions are cumulative, significant advantages may result from analyzing such impacts on a programmatic level. If analyzed properly, later projects may tier, incorporate by reference, or otherwise rely on the programmatic analysis.

State Bill 375

On September 30, 2008, SB 375 was instituted to help achieve AB 32 goals through regulation of cars and light trucks. SB 375 aligns three policy areas of importance to local government: (1) regional long-range transportation plans and investments; (2) regional allocation of the obligation for cities and counties to zone for housing; and (3) a process to achieve GHG emissions reductions targets for the transportation sector. It establishes a process for CARB to develop GHG emissions reductions targets for each region (as opposed to individual local governments or households). SB 375 also requires MPOs to prepare a

Sustainable Communities Strategy (SCS) within the Regional Transportation Plan (RTP) that guides growth while taking into account the transportation, housing, environmental, and economic needs of the region. SB 375 uses CEQA streamlining as an incentive to encourage residential projects, which help achieve AB 32 goals to reduce GHG emissions. While SB 375 does not prevent CARB from adopting additional regulations, such actions are not anticipated in the foreseeable future.

On October 24, 2008, CARB published draft guidance for setting interim GHG significance thresholds. This was the first step toward developing the recommended statewide interim thresholds of significance for GHG emissions that may be adopted by local agencies for their own use. The guidance does not attempt to address every type of project that may be subject to CEQA, but instead focuses on common project types that are responsible for substantial GHG emissions (i.e., industrial, residential, and commercial projects). CARB believes that thresholds in these sectors will advance climate objectives, streamline project review, and encourage in CEQA analyses of GHG emissions throughout the State.

On September 23, 2010, CARB adopted regional targets for the reduction of GHG emissions applying to the years 2020 and 2035.⁴³ For the area under the Southern California Association of Governments' (SCAG) jurisdiction—including the Project area—CARB adopted Regional Targets for reduction of GHG emissions by 8 percent for 2020 and by 13 percent for 2035. On February 15, 2011, the CARB's Executive Officer approved the final targets.⁴⁴

Title 24 Energy Efficiency Standards

California's Energy Efficiency Standards for Residential and Nonresidential Buildings, located at Title 24, Part 6 of the California Code of Regulations and commonly referred to as "Title 24," were established in 1978 in response to a legislative mandate to reduce California's energy consumption. The standards are updated periodically to allow consideration and possible incorporation of new energy efficiency technologies and methods.

California Green Building Standards

The California Green Building Standards Code, which is Part 11 of the California Code of Regulations (the "CCR"), is commonly referred to as the CALGreen Code. The 2008 edition, the first edition of the CALGreen Code, contained only voluntary standards. The 2010 CALGreen Code is a code with mandatory requirements for state-regulated buildings and structures throughout California beginning on

⁴³ California Air Resources Board. Notice of Decision: Regional Greenhouse Gas Emissions Reduction Targets for Automobiles and Light Trucks Pursuant to Senate Bill 375. <http://www.arb.ca.gov/cc/sb375/notice%20of%20decision.pdf>

⁴⁴ CARB. 2011. Executive Order No. G-11-024: Relating to Adoption of Regional Greenhouse Gas Emission Reduction Targets for Automobiles and Light Trucks Pursuant to Senate Bill 375.

January 1, 2011. The 2010 CALGreen Code contains requirements for construction site selection, storm water control during construction, construction waste reduction, indoor water use reduction, material selection, natural resource conservation, site irrigation conservation and more. The CALGreen Code provides for design options allowing the designer to determine how best to achieve compliance for a given site or building condition. The CALGreen Code also requires building commissioning which is a process for the verification that all building systems, like heating and cooling equipment and lighting systems are functioning at their maximum efficiency. The current 2013 CALGreen Code became effective January 1, 2014 and includes new requirements for additions to existing residential and non-residential development. The upcoming 2016 CALGreen Code standard will become effective January 1, 2017.

Regional

SCAQMD Recommendations for Significance Thresholds

The SCAQMD convened a GHG CEQA Significance Threshold Working Group to provide guidance to local lead agencies on determining significance for GHG emissions in their CEQA documents. Members of the working group include government agencies implementing CEQA and representatives from stakeholder groups that will provide input to the SCAQMD staff on developing GHG CEQA significance thresholds. On December 5, 2008, the SCAQMD Governing Board adopted interim GHG significance threshold for projects where the SCAQMD is lead agency. This threshold uses a tiered approach to determine a project's significance, with 10,000 metric tons of CO₂ equivalent (MTCO₂e) as a screening numerical threshold for stationary sources.

The SCAQMD has not adopted guidance for CEQA projects under other lead agencies. In September 2010, the Working Group released additional revisions which recommended a screening threshold of 3,500 MTCO₂e for residential projects, 1,400 MTCO₂e for commercial projects, and 3,000 MTCO₂e for mixed use projects, additionally the Working Group identified project-level efficiency target of 4.8 MTCO₂e per service population as a 2020 target and 3.0 MTCO₂e per service population as a 2035 target. The recommended area wide or plan-level target for 2020 was 6.6 MTCO₂e and the plan-level target for 2035 was 4.1 MTCO₂e. The SCAQMD has not established a timeline for formal consideration of these thresholds.⁴⁵ In the meantime, the project level thresholds are used as a non-binding guide.

The SCAQMD has also adopted Rules 2700, 2701, and 2702 that address GHG emissions reductions. However, these rules address boilers and process heaters, forestry, and manure management projects, none of which are proposed or required of the Project.

⁴⁵ SCAG, *Final PEIR for the 2012-2035 RTP/SCS, Appendix G*. Accessible at http://rtpscs.scag.ca.gov/Documents/peir/2012fPEIR_AppendixG_ExampleMeasures.pdf

SCAG 2016-2040 RTP/SCS

On April 6, 2016, SCAG adopted its 2016-2040 RTP/SCS update, calling for a continuation of integrated planning for land use and transportation that will help achieve the State's goal of reducing per capita GHG emissions by eight percent by 2020 compared to 2005 levels, by 18 percent by 2035, and 21 percent by 2040. The Plan calls for public transportation improvements that will reduce GHG emissions per household by up to 30 percent, one percent reduction in GHG emissions from having zero emission vehicles, neighborhood vehicles, and car-sharing/ride-sourcing make up two percent of the vehicle fleet by 2040. However, until the 2016-2040 RTP/SCS is incorporated into the region's federally-approved AQMP, the 2012-2035 RTP/SCS is the relevant transportation plan for air quality regulatory purposes.

Local

The City has adopted its LA Green Plan that outlines goals and actions to reduce the generation of GHG emissions to 35 percent below 1990 levels. Key strategies include increasing the generation of renewable energy, improving energy conservation and efficiency, and changing land use patterns to reduce dependence on autos.

The City adopted a Green Building Ordinance in April 2008 that calls for reduction of the use of natural resources for new development. Larger projects must be certified by the Leadership in Energy and Environmental Design (LEED), including the following:

- New non-residential building or structure of 50,000 gross square feet or more of floor area;
- New mixed-use or residential building of 50,000 gross square feet or more in excess of six stories;
- New mixed-use or residential building of six or fewer stories consisting of at least 50 dwelling units in a building, which has at least 50,000 gross square feet of floor area, and in which at least 80 percent of the building's floor area is dedicated to residential units;
- The alternation or rehabilitation of 50,000 gross square feet or more of floor area in an existing non-residential building for which construction costs exceed a valuation of 50 percent of the replacement cost of the existing building;
- The alteration of at least 50 dwelling units in an existing mixed-use or residential building, which has at least 50,000 gross square feet of floor area, for which construction costs exceed a valuation of 50 percent of the replacement cost of the existing building.

The City's Green Building Ordinance has several requirements that call for reductions in GHG emissions from reducing in energy use, water use, and solid waste generation, including the following:

Section 99.04.204. Energy Reduction. Equipment and fixtures shall comply with the following where applicable:

1. Installed gas-fired space heating equipment shall have an Annual Fuel Utilization Ratio (AFUE) of .90 or higher.
2. Installed electric heat pumps shall have a Heating Seasonal Performance Factor (HSPF) of 8.0 or higher.
3. Installed cooling equipment shall have a Seasonal Energy Efficiency Ratio (SEER) higher than 13.0 and an Energy Efficiency Ratio (EER) of at least 11.5.
4. Installed tank type water heaters shall have an Energy Factor (EF) higher than .6.
5. Installed tankless water heaters shall have an Energy Factor (EF) higher than .80.
6. Perform duct leakage testing to verify a total leakage rate of less than 6 percent of the total fan flow.
7. Building lighting in the kitchen and bathrooms within the dwelling units shall consist of at least 90 percent ENERGY STAR qualified hard-wired fixtures (luminaires).
8. Installed swimming pool circulating pump motors shall be multi-speed or variable-speed. The pump motor controls shall have the capability of operating the pump at a minimum of three speeds; low speed, medium speed, and high speed. The daily low speed shall not exceed 300 watts. The daily medium speed shall be adjustable.

Section 99.04.210. Appliances. Appliance Rating. Each appliance provided and installed shall meet ENERGY STAR if an ENERGY STAR designation is applicable for that appliance.

Section 99.04.211. Renewable Energy. Future Access for Electrical Solar System. An electrical conduit shall be provided from the electrical service equipment to an accessible location in the attic or other location suitable for future connection to a solar system. The conduit shall be adequately sized by the designer but shall not be less than one inch. The conduit shall be labeled as per the Los Angeles Fire Department requirements. The electrical panel shall be sized to accommodate the installation of a future electrical solar system. Exception: Buildings designed and constructed with a solar photovoltaic system or an alternate system with means of generating electricity at time of final inspection.

Section 99.04.211.4.1. Space for Future Electrical Solar System Installation. A minimum of 250 square feet of contiguous unobstructed roof area shall be provided for the installation of future photovoltaic or other electrical solar panels. The location shall be suitable for installing future solar panels as determined by the designer.

Section 99.04.303.1. Twenty Percent Savings. A schedule of plumbing fixtures and fixture fittings that will reduce the overall use of potable water within the building by at least 20 percent shall be provided. The reduction shall be based on the maximum allowable water use per plumbing fixture and fitting as required by the California Building Standards Code. The 20 percent reduction in potable water use shall be demonstrated by one of the following methods:

1. Each plumbing fixture and fitting shall meet reduced flow rates specified on Table 4.303.2; or
2. A calculation demonstrating a 20 percent reduction in the building “water use” baseline as established on Table 4.303.1 shall be provided. For low-rise residential occupancies, the calculation shall be limited to the following plumbing fixture and fitting types: water closets, urinals, lavatory faucets, kitchen faucets and showerheads.

Section 99.04.303.2. Multiple Showerheads Serving One Shower. When single shower fixtures are served by more than one showerhead, the combined flow rate of all the showerheads shall not exceed the maximum flow rates specified in the 20 percent reduction column contained on Table 4.303.2 or the shower shall be designed to only allow one showerhead to be in operation at a time. Exception: The maximum flow rate for showerheads when using the calculation method specified in Section 99.04.303.1, Item 2, is 2.5 gpm @ 80 psi.

Section 99.04.304.1. Irrigation Controllers. When automatic irrigation system controllers for landscaping are provided and installed at the time of final inspection, the controllers shall comply with the following:

1. Controllers shall be weather- or soil moisture-based controllers that automatically adjust irrigation in response to changes in plants' needs as weather conditions change;
2. Weather-based controllers without integral rain sensors or communication systems that account for local rainfall shall have a separate wired or wireless rain sensor that connects or communicates with the controller(s). Soil moisture-based controllers are not required to have rain sensor input. Buildings on sites with over 2,500 square feet of cumulative irrigated landscaped areas shall have irrigation controllers that meet the criteria in Section 99.04.304.1.

Section 99.04.406. Enhanced Durability and Reduced Maintenance. Joints and Openings. Openings in the building envelope separating conditioned space from unconditioned space needed to accommodate gas, plumbing, electrical lines and other necessary penetrations must be sealed in compliance with the California Energy Code.

Section 99.05.407.3. Water Resistance and Moisture Management. Flashing Details. Provide flashing details on the building plans which comply with accepted industry standards or manufacturer's instructions around windows and doors, roof valley, and chimneys to roof intersections.

Section 99.04.407.4. Material Protection. Protect building materials delivered to the construction site from rain and other sources of moisture.

Section 99.04.408. Construction Waste Reduction, Disposal And Recycling. Construction Waste Reduction of at Least 50 Percent. Comply with Section 66.32 et seq. of the LAMC.

Mobility Plan 2035

On January 20, 2016, the City of Los Angeles adopted its Mobility Plan 2035, a transportation element of its General Plan. The Plan calls for strategies that advance five goals: 1) Safety First, 2) World Class Infrastructure, 3) Access for All Angelenos, 4) Collaboration, Communication, and Informed Choices, and 5) Clean Environments and Healthy Communities.

While the Plan focuses on developing a multi-modal transportation system, its key policy initiatives include considering the strong link between land use and transportation and targeting GHG emissions through a more sustainable transportation system. As such, the Plan's call for integrated land use planning, clean fuel vehicles are consistent with State and regional plans calling for more compact growth in areas with transportation infrastructure.

Methodology

The methodology utilized for this analysis is based on a Technical Advisory released by the Governor's Office of Planning and Research (OPR) on June 19, 2008 titled *CEQA and Climate Change: Addressing Climate Change Through California Environmental Quality Act (CEQA) Review*. Both one-time emissions and indirect emissions are expected to occur each year after build-out of the Project. One-time emissions from construction and vegetation removal were amortized over a 30-year period because no significance threshold has been adopted for such emissions. The Project emission reductions are results of Project's commitments and regulatory changes, which include the implementation of the Renewables Portfolio Standard (RPS) of 33 percent, the Pavley regulation and Advanced Clean Cars program mandating higher fuel efficiency standards for light-duty vehicles, and the Low Carbon Fuel Standard (LCFS).

The California Climate Action Registry (Climate Registry) General Reporting Protocol provides basic procedures and guidelines for calculating and reporting GHG emissions from a number of general and industry-specific activities.⁴⁶ The General Reporting Protocol is based on the "Greenhouse Gas Protocol: A Corporate Accounting and Reporting Standard" developed by the World Business Council for

⁴⁶ California Climate Action Registry, *General Reporting Protocol Version 3.1*, January 2009, www.sfenvironment.org/sites/default/files/fliers/files/ccar_grp_3-1_january2009_sfe-web.pdf, accessed August 1, 2016.

Sustainable Development and the World Resources Institute through “a multi-stakeholder effort to develop a standardized approach to the voluntary reporting of GHG emissions.”⁴⁷ The General Reporting Protocol provides a basic framework for calculating and reporting GHG emissions from the project. The information provided in this analysis is consistent with the General Reporting Protocol’s reporting requirements.

The General Reporting Protocol recommends the separation of GHG emissions into three categories that reflect different aspects of ownership or control over emissions. They include the following:

- Scope 1: Direct, on-site combustion of fossil fuels (e.g., natural gas, propane, gasoline, and diesel).
- Scope 2: Indirect, off-site emissions associated with purchased electricity or purchased steam.
- Scope 3: Indirect emissions associated with other emissions sources, such as third-party vehicles and embodied energy (e.g., energy used to convey, treat, and distribute water and wastewater).⁴⁸

The General Reporting Protocol provides a range of basic calculations methods. However, the General Reporting Protocol calculations are typically designed for existing buildings or facilities. These retrospective calculation methods are not directly applicable to planning and development situations where buildings do not yet exist.

CARB recommends consideration of indirect emissions to provide a more complete picture of the GHG footprint of a facility. Annually reported indirect energy usage aids the conservation awareness of a facility and provides information to CARB to be considered for future strategies.⁴⁹ For example, CARB has proposed requiring the calculation of direct and indirect GHG emissions as part of the AB 32 reporting requirements. Additionally, the Office of Planning and Research has noted that lead agencies “should make a good-faith effort, based on available information, to calculate, model, or estimate... GHG emissions from a project, including the emissions associated with vehicular traffic, energy consumption,

⁴⁷ *Ibid.*

⁴⁸ Embodied energy is a scientific term that refers to the quantity of energy required to manufacture and supply to the point of use a product, material, or service.

⁴⁹ California Air Resources Board, *Initial Statement of Reasons for Rulemaking, Proposed Regulation for Mandatory Reporting of Greenhouse Gas Emissions Pursuant to the California Global Warming Solutions Act of 2006 (AB 32)*, Planning and Technical Support Division Emission Inventory Branch, October 19, 2007, www.arb.ca.gov/regact/2007/ghg2007/isor.pdf, accessed August 1, 2016.

water usage and construction activities.⁵⁰ Therefore, direct and indirect emissions have been calculated for the Project.

GHG emissions were quantified from construction and operation of the Project using SCAQMD's California Emissions Estimator Model (CalEEMod). Operational emissions include both direct and indirect sources including mobile sources, water use, solid waste, area sources, natural gas, and electricity use emissions. CalEEMod is a statewide land use emissions computer model designed to provide a uniform platform for government agencies, land use planners, and environmental professionals to quantify potential criteria pollutant and GHG emissions associated with both construction and operations from a variety of land use projects. The model is considered by the SCAQMD to be an accurate and comprehensive tool for quantifying air quality and GHG impacts from land use projects throughout California.⁵¹

Thresholds of Significance

CARB, SCAQMD and the City of Los Angeles have yet to adopt project-level significance thresholds for GHG emissions that would be applicable to the Project.⁵² As a result, this analysis relies on primary direction from the CEQA Guidelines. OPR's amendments to the CEQA Guidelines for GHGs were adopted by the Resources Agency on December 30, 2009, indicating that a project could have a significant impact if it would:

1. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment; or
2. Conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of greenhouse gases.⁵³

⁵⁰ OPR Technical Advisory, p. 5.

⁵¹ See www.caleemod.com.

⁵² The South Coast Air Quality Management District formed a GHG Significance Threshold Working Group. Information on this Working Group is available at www.aqmd.gov/home/regulations/ceqa/air-quality-analysis-handbook/ghg-significance-thresholds/page/2.

⁵³ A recent opinion by the California Supreme Court on November 30, 2015 (*Center for Biological Diversity v. California Department of Fish and Wildlife*) has suggested that environmental analyses need to support its assumptions and provide evidentiary support to find consistency with a "Business as Usual" approach with the AB 32 Scoping Plan.

Section 15064.4 of the CEQA Guidelines was adopted to assist lead agencies in determining the significance of the impacts of GHGs. It urges the quantification of GHG emissions where possible and includes language necessary to avoid an implication that a “life-cycle” analysis is required. It also recommends considering other qualitative factors that may be used in the determination of significance (i.e., extent to which the project may increase or reduce GHG emissions; whether the project exceeds an applicable significance threshold; and extent to which the project complies with regulations or requirements adopted to implement a reduction or mitigation of GHGs). Further, it states that:

1. A lead agency should consider the following factors, among others, when assessing the significance of greenhouse gas emissions on the environment:
 - a. The extent to which the project may increase or reduce greenhouse gas emissions as compared to the existing environmental setting;
 - b. Whether the project emissions exceed a threshold of significance that the lead agency determines applies to the project; and
 - c. The extent to which the project complies with regulations or requirements adopted to implement a Statewide, regional, or local plan for the reduction or mitigation of greenhouse gas emissions. Such requirements must be adopted by the relevant public agency through a public review process and must reduce or mitigate the project’s incremental contribution of greenhouse gas emissions. If there is substantial evidence that the possible effects of a particular project are still cumulatively considerable notwithstanding compliance with the adopted regulations or requirements, an EIR must be prepared for the project.

Lead agencies are to establish thresholds in which a lead agency may appropriately look to thresholds developed by other public agencies, or suggested by other experts, such as CAPCOA, so long as any threshold chosen is supported by substantial evidence (see CEQA Guidelines Section 15064.7(c)). The CEQA Guidelines amendments also clarify that the effects of GHG emissions are cumulative. The CEQA Guidelines were amended in response to Senate Bill 97 to specify that compliance with a GHG emissions reduction plan renders a cumulative impact insignificant.

To qualify, such a plan or program must be specified in law or adopted by the public agency with jurisdiction over the affected resources through a public review process to implement, interpret, or make specific the law enforced or administered by the public agency.⁵⁴ Examples of such programs include a “water quality control plan, air quality attainment or maintenance plan, integrated waste management plan, habitat conservation plan, natural community conservation plans [and] plans or regulations for the

⁵⁴ *Id.*

reduction of greenhouse gas emissions.”⁵⁵ Put another way, CEQA Guidelines Section 15064(h)(3) allows a lead agency to make a finding of non-significance for GHG emissions if a project complies with the California Cap-and-Trade Program and/or other regulatory schemes to reduce GHG emissions.⁵⁶

Per CEQA Guidelines Section 15064(h)(3), a project’s incremental contribution to a cumulative impact can be found not cumulatively considerable if the project will comply with an approved plan or mitigation program that provides specific requirements that will avoid or substantially lessen the cumulative problem within the geographic area of the project.⁵⁷

To evaluate a project’s potential greenhouse gas emissions under CEQA, a lead agency may adopt a significance criterion of whether the project will be consistent with statewide greenhouse gas emission reduction goals, as set forth in the California Global Warming Solutions Act of 2006 (or “AB 32”) and the California Air Resources Board 2008 Climate Change Scoping Plan (“Scoping Plan”) that implements A.B. 32. (*Center for Biological Diversity v. Cal. Dept. of Fish and Game* (2015) 62 Cal.4th 204, 220; see also CEQA Guidelines § 15064.4.)

The statewide greenhouse gas reduction goals include cutting greenhouse gas emissions by approximately 30 percent from the BAU emission levels projected for 2020. The Scoping Plan sets forth the BAU projection, which assumes no conservation or regulatory efforts beyond what was in place when the

⁵⁵ *Id.* (emphasis added).

⁵⁶ See San Joaquin Valley Air Pollution Control District, *CEQA Determinations of Significance for Projects Subject to ARB’s GHG Cap-and-Trade Regulation, APR—2030* (June 25, 2014), where the SJVAPCD “determined that GHG emissions increases that are covered under ARB’s Cap-and-Trade regulation cannot constitute significant increases under CEQA...” Further, SCAQMD has taken this position as a lead agency, preparing three Negative Declarations and one Draft EIR that applied its 10,000 MTCO₂e/yr. significance threshold in such a way that GHG emissions covered by the Cap-and-Trade Program do not constitute emissions that must be measured against the threshold. See SCAQMD, *Final Negative Declaration for Ultramar Inc. Wilmington Refinery Cogeneration Project*, SCH #2012041014 (www.aqmd.gov/docs/default-source/ceqa/documents/permit-projects/2014/ultramar_neg_dec.pdf?sfvrsn=2) (October 2014); SCAQMD, *Final Negative Declaration for Phillips 66 Los Angeles Refinery Carson Plant—Crude Oil Storage Capacity Project*, SCH No. 2013091029 (December 2014) (www.aqmd.gov/docs/default-source/ceqa/documents/permit-projects/2014/phillips-66-fnd.pdf?sfvrsn=2); *Final Mitigated Negative Declaration for Toxic Air Contaminant Reduction for Compliance with SCAQMD Rules 1420.1 and 1402 at the Exide Technologies Facility in Vernon, CA*, SCH No. 2014101040 (www.aqmd.gov/docs/default-source/ceqa/documents/permit-projects/2014/exide-mnd_final.pdf?sfvrsn=2) (December 2014); and *Draft Environmental Impact Report for the Breitburn Santa Fe Springs Blocks 400/700 Upgrade Project*, SCH No. 2014121014 (www.aqmd.gov/docs/default-source/ceqa/documents/permit-projects/2015/deir-breitburn-chapters-1-3.pdf?sfvrsn=2) (April 2014).

⁵⁷ 14 CCR § 15064(h)(3).

forecast was made. A lead agency may use the BAU projection as the baseline to compare a project's expected greenhouse gas emissions rather than using a baseline of emissions in the existing physical environment. However, the lead agency must provide substantial evidence to show that a project's specific *project-level* reduction in greenhouse gas emissions as compared to the BAU projection will actually meet the *statewide* goals of greenhouse gas reductions.

There are three ways a lead agency could make that showing. First, a lead agency may evaluate the data behind the Scoping Plan's BAU model to determine how a specific project in a proposed location would contribute to the statewide greenhouse gas reduction goals. Second, a lead agency may assess a project's consistency with AB 32's goals in whole or in part by considering a project's compliance with regulatory programs designed to reduce greenhouse gas emissions from particular activities, such as building efficiency and conservation standards. Third, a lead agency may rely on existing numerical thresholds of significance for greenhouse gas emissions reductions.

Thus, in the absence of any adopted, quantitative threshold, the Project would not have a significant effect on the environment if it is found to be consistent with the applicable regulatory plans and policies to reduce GHG emissions:

- Executive Orders S-3-05 and B-30-15;
- AB 32 Scoping Plan;
- SCAG's 2012-2035 Regional Transportation Plan/Sustainable Communities Strategy;
- City of Los Angeles Mobility Plan 2035;
- City of Los Angeles ClimateLA implementation plan; and
- City of Los Angeles Green Building Ordinance

The following information provides an extensive analysis of the Project's consistency with these State, regional, and local climate action-related policies. This section focuses on disclosing potential GHG emissions.

Project Impacts

Construction of the Project would emit GHG emissions through the combustion of fossil fuels by heavy-duty construction equipment and through vehicle trips generated by construction workers traveling to and from the Project site. These emissions would vary day to day over the 26-month duration of construction activities. As illustrated on Table IV-9, construction emissions of CO₂ would peak in 2018, when up to 28,686 pounds of CO₂e per day are anticipated (with implementation of Mitigation Measures 3-1 through 3-5).

Table IV-9
Estimated Construction Emissions (Pounds Per Day)

Construction Year	CO ₂	CH ₄	N ₂ O	CO ₂ e
2017	24,419	5	0	24,520
2018	28,563	6	0	28,686
2019	3,272	<1	0	3,281
2020	3,240	<1	0	3,249
<i>Source: DKA Planning 2015, based on CalEEMod 2013.2.2</i>				

Greenhouse gas emissions were calculated for long-term operations. Both one-time emissions and indirect emissions are expected to occur each year after build-out of the Project. One-time emissions from construction and vegetation removal were amortized over a 30-year period because no significance threshold has been adopted for such emissions. The Project emission reductions are results of Project's commitments and regulatory changes, which include the implementation of the Renewables Portfolio Standard (RPS) of 33 percent, the Pavley regulation and Advanced Clean Cars program mandating higher fuel efficiency standards for light-duty vehicles, and the Low Carbon Fuel Standard (LCFS).

This analysis compares the Project's GHG emissions to the emissions that would be generated by the Project in the absence of any GHG reduction measures (i.e., the No Action Taken ("NAT") Scenario. This approach is consistent with the concepts used in the CARB's *Climate Change Scoping Plan* for the implementation of AB 32. This methodology is used to analyze consistency with applicable GHG reduction plans and policies and demonstrate the efficacy of the measures contained therein, but it is not a threshold of significance.

The analysis in this section includes potential emissions under NAT scenarios and from the Project at build-out based on actions and mandates expected to be in force in 2020. Early-action measures identified in the *Climate Change Scoping Plan* that have not been approved were not credited in this analysis. By not speculating on potential regulatory conditions, the analysis takes a conservative approach that likely overestimates the Project's GHG emissions at build-out.

The NAT scenario is used to establish a comparison with project-generated GHG emissions. The NAT scenario does not consider site-specific conditions, project design features, or prescribed mitigation measures. As an example, a NAT scenario would apply a base ITE trip-generation rate for a project and would not consider site-specific benefits resulting from the proposed residential uses or close proximity to public transportation, such as the Metro 256 and DASH El Sereno/City of Torrance. The analysis below establishes NAT as complying with the minimum performance level required under Title 24. The NAT scenario also considers State mandates that were already in place when CARB prepared the *Supplemental FED* (e.g., Pavley I Standards, full implementation of California's Statewide Renewables Portfolio Standard beyond current levels of renewable energy, and the California Low Carbon Fuel Standard).

Emissions calculations for the Project include credits or reductions for the regulatory compliance measures and project design features set forth throughout this analysis, such as reductions in energy or water demand. In addition, as mobile source GHG emissions are directly dependent on the number of vehicle trips, a decrease in the number of Project generated trips as a result of project features will provide a proportional reduction in mobile source GHG emissions. This scenario conservatively did not include actions and mandates that are not already in place but are expected to be in force in 2020 (e.g., Pavley II), which could further reduce GHG emissions from use of light-duty vehicles by 2.5 percent.

As shown on Table IV-10, the emissions for the Project and its associated CARB 2020 NAT scenario are estimated to be 937 and 1,357 MTCO₂e per year, respectively, which shows the Project would reduce emissions by 31 percent from the CARB 2020 NAT scenario. Based on these results, the Project exceeds the reduction target as a numeric threshold of 15.3 percent set forth in the 2014 Revised AB 32 Scoping Plan.

Table IV-10
Estimated Project Annual CO₂e GHG Emissions (Metric Tons per Year)

Scenario and Source	Business As Usual Scenario*	As Proposed Scenario	Reduction from Business As Usual Scenario	Change from Business as Usual Scenario
Area Sources	11	11	-	0%
Energy Sources	408	237	-171	-42%
Mobile Sources	834	585	-248	-30%
Waste Sources	22	22	-	0%
Water Sources	34	34	-	0%
Construction	30	30	-	0%
Total Emissions	1,338	919	-420	-31%
<p><i>Daily construction emissions amortized over 30-year period pursuant to SCAQMD guidance. Annual construction emissions derived by taking total emissions over duration of activities and dividing by construction period. To ensure a conservative estimate, emissions from existing development were not included in the calculation of net emissions increases.</i></p> <p><i>* BAU scenario does not assume 30% reduction in in mobile source emissions from Pavley emission standards (19.8%), low carbon fuel standards (7.2%), vehicle efficiency measures 2.8%); does not assume 42% reduction in energy production emissions from the State's renewables portfolio standard (33%), natural gas extraction efficiency measures (1.6%), and natural gas transmission and distribution efficiency measures (7.4%).</i></p> <p><i>Source: DKA Planning, 2016.</i></p>				

The analysis in this document uses the 2014 Revised AB 32 Scoping Plan's statewide goals as one approach to assess the efficacy of the Project's GHG reduction features and characteristics, as well as the

Project's consistency with statewide and regional GHG reduction plans. . The report's methodology is to compare the Project's emissions as proposed to the Project's emissions if the Project were built using a NAT approach in terms of design, methodology, and technology. This means the Project's emissions were calculated as if it was constructed with project design features to reduce GHG and with several regulatory measures adopted in furtherance of AB 32.

While the AB 32 Scoping Plan's cumulative statewide objectives were not intended to serve as the basis for project-level assessments, this analysis finds that its NAT comparison based on the Scoping Plan is appropriate because the Proposed Project would contribute to statewide GHG reduction goals. Specifically, the Proposed Project's location in an existing urban setting provide opportunities to reduce transportation-related emissions.

It should be noted that each source category of GHG emissions from the Proposed Project is subject to a number of regulations that directly or indirectly reduce climate change-related emissions:

Stationary and area sources. Emissions from small on-site sources are subject to specific emission reduction mandates and/or are included in the State's Cap and Trade program.

Transportation. Both construction and operational activities from the Project Site would generate transportation-related emissions from combustion of fossil fuels that are covered in the State's Cap and Trade program.

Energy Use. Both construction and operational activities from the Project Site would generate energy-related emissions that are covered by the State's renewable portfolio mandates, including SB 350, which requires that at least 50 percent of electricity generated and sold to retail customers from renewable energy sources by December 31, 2030.

Building structures. Operational efficiencies will be built into the project that reduce energy use and waste, as mandated by CALGreen building codes.

Water and wastewater use. The Project would be subject to drought-related water conservation emergency orders and related State Water Quality Control Board restrictions.

Major appliances. The Project would include major appliances that are regulated by California Energy Commission requirements for energy efficiency.

Solid waste management. The Project would be subject to solid waste diversion policies administered by CalRecycle that reduce GHG emissions.

In addition to the GHG emission reductions described above, it is important to note that the CO₂ estimates from mobile sources (particularly CO₂, CH₄, and N₂O emissions) are likely much greater than the emissions that would actually occur. The methodology used assumes that all emissions sources are new sources and that emissions from these sources are 100 percent additive to existing conditions. This is a

standard approach taken for air quality analyses. In many cases, such an assumption is appropriate because it is impossible to determine whether emissions sources associated with a project move from outside the air basin and are in effect new emissions sources, or whether they are sources that were already in the air basin and just shifted to a new location. Because the effects of GHGs are global, a project that shifts the location of a GHG-emitting activity (e.g., where people live, where vehicles drive, or where companies conduct business) would result in no net change in global GHG emissions levels.

For example, if a substantial portion of California's population migrated from the South Coast Air Basin to the San Joaquin Valley Air Basin, this would likely decrease GHG emissions in the South Coast Air Basin and increase emissions in the San Joaquin Valley Air Basin, but little change in overall global GHG emissions. However, if a person moves from one location where the land use pattern requires auto use (e.g., commuting, shopping) to a new development that promotes shorter and fewer vehicle trips, more walking, and overall less energy usage, then it could be argued that the new development would result in a potential net reduction in global GHG emissions.

As described throughout this analysis, the Project contains numerous regulatory compliance measures and project design features that would reduce the Project's GHG emissions profile and would represent improvements vis-à-vis the NAT scenario. Also, the Project would not exceed the SCAQMD's proposed (but not adopted) threshold of significance of 3,000 metric tons per year of GHG emissions for residential projects. As a result of this and the analysis of net emissions, the Project's contribution to global climate change is not "cumulatively considerable" and is considered less than significant.

b) Would the project conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

Less Than Significant Impact. As noted earlier, the Project would be consistent with a number of relevant plans and policies that govern climate change.

- Executive Orders S-3-05 and B-30-15;
- AB 32 Scoping Plan;
- SCAG's 2016-2040 Regional Transportation Plan/Sustainable Communities Strategy;
- City of Los Angeles Mobility 2035 Plan;
- City of Los Angeles ClimateLA implementation plan; and
- City of Los Angeles Green Building Ordinance

Consistency with Executive Orders S-03-05 and B-30-15.

The Project is consistent with the State's Executive Orders S-3-05 and B-30-15, which are orders from the State's Executive Branch for the purpose of reducing GHG emissions. These strategies call for developing more efficient land-use patterns to match population increases, workforce, and socioeconomic needs for the full spectrum of the population. The Project includes elements of smart land use as it is located in an urban infill area well-served by transportation infrastructure that includes public transit provided by Metro and LADOT.

Although the Project's emissions level in 2050 cannot be reliably quantified, statewide efforts are underway to facilitate the State's achievement of that goal and it is reasonable to expect the Project's emissions profile to decline as the regulatory initiatives identified by CARB in the First Update are implemented, and other technological innovations occur. Stated differently, the Project's emissions total at build-out presented in this analysis represents the maximum emissions inventory for the Project as California's emissions sources are being regulated (and foreseeably expected to continue to be regulated in the future) in furtherance of the State's environmental policy objectives. As such, given the reasonably anticipated decline in Project emissions once fully constructed and operational, the Project is consistent with the Executive Order's horizon-year goal.

Many of the emission reduction strategies recommended by CARB would serve to reduce the Project's post-2020 emissions level to the extent applicable by law and help lay the foundation "...for establishing a broad framework for continued emission reductions beyond 2020, on the path to 80 percent below 1990 levels by 2050," as called for in CARB's First Update to the AB 32 Scoping Plan.^{58,59}

As such, the Project's post-2020 emissions trajectory is expected to follow a declining trend, consistent with the 2030 and 2050 targets and Executive Order S-3-05 and B-30-15.

Consistency with the AB 32 Scoping Plan

The AB 32 Scoping Plan provides the basis for policies that will reduce cumulative GHG emissions within California to 1990 levels by 2020. As a result, the Proposed Project is judged against its consistency with the AB 32 Scoping Plan to determine whether it will result in adverse cumulative impacts to global climate change. The Proposed Project is consistent with the AB 32 Scoping Plan's focus on emission reductions from several key sectors:

⁵⁸ CARB, *First Update*, p. 4, May 2014. See also *id.* at pp. 32–33 [recent studies show that achieving the 2050 goal will require that the "electricity sector will have to be essentially zero carbon; and that electricity or hydrogen will have to power much of the transportation sector, including almost all passenger vehicles."]

⁵⁹ CARB, *First Update*, Table 6: *Summary of Recommended Actions by Sector*, pp. 94–99, May 2014.

- **Energy Sector:** Continued improvements in California's appliance and building energy efficiency programs and initiatives, such as the State's zero net energy building goals, would serve to reduce the Project's emissions level.⁶⁰ Additionally, further additions to California's renewable resource portfolio would favorably influence the Project's emissions level.⁶¹
- **Transportation Sector:** Anticipated deployment of improved vehicle efficiency, zero emission technologies, lower carbon fuels, and improvement of existing transportation systems all will serve to reduce the Project's emissions level.⁶²
- **Water Sector:** The Project's emissions level will be reduced as a result of further desired enhancements to water conservation technologies.⁶³
- **Waste Management Sector:** Plans to further improve recycling, reuse and reduction of solid waste will beneficially reduce the Project's emissions level.⁶⁴

As shown on Table IV-11, the Project would be consistent with all feasible and applicable strategies recommended in the Scoping Plan.

Table IV-11
Project Consistency With AB 32 Scoping Plan GHG Emissions Reduction Strategies

Strategy	Project Consistency
<i>California Cap-and-Trade Program.</i> Implement a broad-based California cap-and-trade program to provide a firm limit on emissions.	N/A. The statewide program is not relevant to the Project.
<i>California Light-Duty Vehicle Greenhouse Gas Standards.</i> Implement adopted Pavley standards and planned second phase of the system. Align zero-emission vehicle, alternative and renewable fuel and vehicle technology programs with long-term climate change goals.	N/A. The development of standards is not relevant to the Project.
<i>Energy Efficiency.</i> Maximize energy efficiency building and appliance standards and pursue additional efficiency efforts including new technologies, and new policy and mechanisms. Pursue comparable investment in energy efficiency from all retail providers of electricity in California.	Consistent. The Project would be designed and constructed to meet Cal Green building standards by including several measures designed to reduce energy consumption.
<i>Renewables Portfolio Standard.</i> Achieve 33 percent	Consistent. The Project would use energy

⁶⁰ CARB, *First Update*, pp. 37-39, 85, May 2014.

⁶¹ CARB, *First Update*, pp. 40-41, May 2014.

⁶² CARB, *First Update*, pp. 55-56, May 2014.

⁶³ CARB, *First Update*, p. 65, May 2014.

⁶⁴ CARB, *First Update*, p. 69, May 2014.

Table IV-11
Project Consistency With AB 32 Scoping Plan GHG Emissions Reduction Strategies

Strategy	Project Consistency
renewable energy mix statewide.	from the Los Angeles Department of Water and Power, which has goals to diversify its portfolio of energy sources to increase the use of renewable energy.
<i>Low-Carbon Fuel Standard.</i> Develop and adopt the Low Carbon Fuel Standard.	N/A. The statewide program is not relevant to the Project.
<i>Regional Transportation-Related Greenhouse Gases.</i> Develop regional greenhouse gas emissions reduction targets for passenger vehicles.	N/A. The development of regional planning goals is not relevant to the Proposed Project. The project's infill location near Los Angeles County Metropolitan Transportation Authority (i.e., Routes 256, 79, 78, 378) and the City of Los Angeles DASH El Sereno/City Terrace route transit services make it consistent with the smart growth objectives of the region's Sustainable Communities Strategy (SCS).
<i>Vehicle Efficiency Measures.</i> Implement light-duty vehicle efficiency measures.	N/A. State agencies are responsible for implementing efficiency measures.
<i>Goods Movement.</i> Implement adopted regulations for the use of shore power for ships at berth. Improve efficiency in goods movement activities.	N/A. State agencies are responsible for implementing regulations and promoting efficiency in goods movement.
<i>Million Solar Roofs Program.</i> Install 3,000 MW of solar-electric capacity under California's existing solar programs.	Neutral. The Project does not include solar roofs and is not part of the proposed Statewide initiative.
<i>Medium/Heavy-Duty Vehicles.</i> Adopt medium and heavy-duty vehicle efficiency measures.	N/A. State agencies are responsible for implementing efficiency measures.
<i>Industrial Emissions.</i> Require assessment of large industrial sources to determine whether individual sources within a facility can cost-effectively reduce greenhouse gas emissions. Reduce greenhouse gas emissions from fugitive emissions from oil and gas extraction and gas transmission.	N/A. This measure addresses industrial facilities.
<i>High Speed Rail.</i> Support implementation of a high speed rail system.	N/A. This calls for the California High Speed Rail Authority and stakeholders to develop a statewide rail transportation system.
<i>Green Building Strategy.</i> Expand the use of green building practices to reduce the carbon footprint of California's new and existing inventory of buildings.	Consistent. The Project would be designed and constructed to meet Cal Green building standards and would include several measures designed to reduce energy consumption.
<i>High Global Warming Potential Gases.</i> Adopt measures to reduce high global warming potential gases.	N/A. State agencies are responsible for implementing these measures.
<i>Recycling and Waste.</i> Reduce methane emissions at landfills. Increase waste diversion, composting and other	Consistent. The Project would have minimal impact on solid waste facilities.

**Table IV-11
Project Consistency With AB 32 Scoping Plan GHG Emissions Reduction Strategies**

Strategy	Project Consistency
beneficial uses of organic materials and mandate commercial recycling. Move toward zero waste.	
<i>Sustainable Forests.</i> Preserve forest sequestration and encourage the use of forest biomass for sustainable energy generation.	N/A. Resource Agency departments are responsible for implementing this measure.
<i>Water.</i> Continue efficiency programs and use cleaner energy sources to move and treat water.	Consistent. The project would use water-efficient landscaping including point-to-point irrigation and a smart controller drip system to reduce water use.
<i>Agriculture.</i> In the near-term, encourage investment in manure digester and at the five-year Scoping Plan update determine if the program should be made mandatory by 2020.	N/A. The Project does not include agricultural facilities.
<i>Source: DKA Planning, 2016.</i>	

Consistency with SCAG's 2016-2040 RTP/SCS

At the regional level, the 2016-2040 RTP and Sustainable Communities Strategy represent the region's Climate Action Plan that defines strategies for reducing GHGs. In order to assess the Project's potential to conflict with the RTP/SCS, this section analyzes the Project's land use profile for consistency with those in the Sustainable Communities Strategy. Generally, projects are considered consistent with the provisions and general policies of applicable City and regional land use plans and regulations, such as SCAG's Sustainable Communities Strategy, if they are compatible with the general intent of the plans and would not preclude the attainment of their primary goals.

The Project is an infill development that is also consistent with the 2016 RTP/SCS and its focus on integrated land use planning. Specifically, the Project site is served by both Metro Route 256 and the LADOT's El Sereno/City Terrace community shuttle. Table IV-12 illustrates the Project's consistency with the Actions and Strategies set forth in the 2016-2040 RTP/SCS. The Project would also be consistent with the applicable goals and principles set forth in the 2016-2040 RTP/SCS and the Compass Growth Vision Report. Therefore, the Project would be consistent with the GHG reduction related actions and strategies contained in the 2016-2040 RTP/SCS.

Table IV-12
Project Consistency with SCAG 2016-2040 RTP/SCS

Actions and Strategies	Responsible Party(ies)	Consistency Analysis
Land Use Strategies		
Reflect the changing population and demands, including combating gentrification and displacement, by increasing housing supply at a variety of affordability levels.	Local jurisdictions	Consistent. The Project includes residences that would add to the supply of housing in metropolitan Los Angeles County.
Focus new growth around transit.	Local Jurisdictions	Consistent. The Project is an infill development that would be consistent with the 2016 RTP/SCS focus on growing near transit facilities.
Plan for growth around livable corridors, including growth on the Livable Corridors network.	SCAG, Local Jurisdictions	Consistent. The Project is an infill development that would be consistent with the 2016 RTP/SCS focus on growing along the 2,980 miles of Livable Corridors in the region.
Provide more options for short trips through Neighborhood Mobility Areas and Complete Communities.	SCAG, Local Jurisdictions	Consistent. The Project would help further jobs/housing balance objectives that can improve the use of Neighborhood Electric Vehicles for short trips, by pre-wiring for electric vehicles. The Project is also generally consistent with the Complete Communities initiative that focuses on creation of mixed-use districts in growth areas.
Support local sustainability planning, including developing sustainable planning and design policies, sustainable zoning codes, and Climate Action Plans.	Local Jurisdictions	Not Applicable. While this strategy calls on local governments to adopt General Plan updates, zoning codes, and Climate Action Plans to further sustainable communities, the Project would not interfere with such policymaking and would be consistent with those policy objectives.
Protect natural and farm lands, including developing conservation strategies.	SCAG Local Jurisdictions	Consistent. The Project is an infill development that would help reduce demand for growth in urbanizing areas.
Transportation Strategies		
Preserve our existing transportation system.	SCAG County Transportation Commissions Local Jurisdictions	Not Applicable. While this strategy calls on investing in the maintenance of our existing transportation system, the Project would not interfere with such policymaking.
Manage congestion through programs like the Congestion Management	County Transportation Commissions	Consistent. The Project is an infill development that will minimize congestion impacts on the region because of its proximity to public transit, Complete

Table IV-12
Project Consistency with SCAG 2016-2040 RTP/SCS

Actions and Strategies	Responsible Party(ies)	Consistency Analysis
Program, Transportation Demand Management, and Transportation Systems Management strategies.	Local Jurisdictions	Communities, and general density of population and jobs.
Promote safety and security in the transportation system.	SCAG County Transportation Commissions Local Jurisdictions	Not Applicable. While this strategy aims to improve the safety of the transportation system and protect users from security threats, the Project would not interfere with such policymaking.
Complete our transit, passenger rail, active transportation, highways and arterials, regional express lanes, goods movement, and airport ground transportation systems.	SCAG County Transportation Commissions Local Jurisdictions	Not Applicable. This strategy calls for transportation planning partners to implement major capital and operational projects that are designed to address regional growth. The Project would not interfere with this larger goal of investing in the transportation system.
<i>Technological Innovation and 21st Century Transportation</i>		
Promote zero-emissions vehicles.	SCAG Local Jurisdictions	Consistent. While this action/strategy is not necessarily applicable on a project-specific basis, the Project would include pre-wiring for electric vehicle charging infrastructure.
Promote neighborhood electric vehicles.	SCAG Local Jurisdictions	Consistent. While this action/strategy is not necessarily applicable on a project-specific basis, the Project would include pre-wiring for electric vehicle charging infrastructure.
Implement shared mobility programs.	SCAG Local Jurisdictions	Not Applicable. While this strategy is designed to integrate new technologies for last-mile and alternative transportation programs, the Proposed Project would not interfere with these emerging programs.
Source: Southern California Association of Governments; 2016-2040 RTP/SCS, Chapter 5: The Road to Greater Mobility and Sustainable Growth; April 2016.		

Consistency with the City of Los Angeles Mobility Plan 2035

While the Mobility Plan 2035 focuses on developing a multi-modal transportation system, its key policy initiatives include considering the strong link between land use and transportation and targeting GHG through a more sustainable transportation system. The Proposed Project is fully consistent with these general objectives, including the most relevant strategy, Program No. D7, which calls for the development

of GHG tracking program that would quantify reductions in GHG from reductions in vehicle miles traveled.

Consistency with the City of Los Angeles ClimateLA Plan

Construction of the Project would be consistent with the “ClimateLA” plan’s goal of reducing or recycling 70 percent of trash (including construction waste) by 2015. The Project would promote this goal by complying with waste reduction measures mandated by CALGreen and City’s Green Building Code, as well as solid waste diversion policies administered by CalRecycle that in turn reduce GHG emissions.

Long-term operations of the Project also would be consistent with the “ClimateLA” focus on transportation, energy, water use, land use, waste, open space and greening, and economic factors to achieve emissions reductions.

With regard to transportation, the Project is consistent with the Plan’s focus on reducing emissions from private vehicle use. Specifically, the site’s infill location with immediate access to significant public transit, pedestrian, and bicycle facilities results in a transit-oriented development that will reduce auto dependence.

To reduce emissions from energy usage, the Project would be consistent with “ClimateLA” and its focus on increasing the amount of renewable energy provided by the Los Angeles Department of Water and Power; presenting a comprehensive set of green building policies to guide and support private sector development; and helping citizens to use less energy. Both construction and operational activities from the Project site would generate energy-related emissions that are reduced by the State’s renewable portfolio mandates, including SB 350, which requires that at least 50 percent of electricity generated and sold to retail customers come from renewable energy sources by December 31, 2030.

With regard to water, the Project would be consistent with reducing water from growth through water conservation and recycling; reducing per capita water consumption by 20 percent; and implementing the City’s water and wastewater integrated resources plan that will increase conservation, and maximize the capture and reuse of storm water. Specifically, the Project would be subject to drought-related water conservation emergency orders and related State Water Quality Control Board restrictions, as well as CALGreen and City Green Building Code that call for water-conserving fixtures and processes. These elements of the Project would be consistent with goals set forth in the “ClimateLA” plan.

With regard to waste, the Project would be consistent with the “ClimateLA” goal of reducing or recycling 70 percent of trash by 2015. Operational efficiencies will be built into the Project that reduce energy use and waste, as mandated by the City’s Green Building Code and CALGreen building code. With regard to ongoing operations, the Project would be subject to solid waste diversion policies administered by CalRecycle that reduce GHG emissions.

With regard to open space and greening, the Project would not interfere with “ClimateLA” and its focus on creating 35 new parks; revitalizing the Los Angeles River to create open space opportunities; planting one million trees throughout the City; identifying opportunities to “daylight” streams; identifying promising locations for stormwater infiltration to recharge groundwater aquifers; and collaborating with schools to create more parks in neighborhoods.

Consistency with the City of Los Angeles Green Building Ordinance

The Los Angeles Green Building Ordinance requires that Projects filed on or after January 1, 2014 comply with the Los Angeles Green Building Code as amended to comply with the 2013 CALGreen Code. Because the Project includes fewer than 50 homes, the Project would not be subject to the mandatory measures under the Green Building Ordinance that would help reduce GHG emissions. The Project would comply with these mandatory measures, as the Project would provide on-site bicycle parking spaces. Furthermore, the Green Building Ordinance includes measures that would increase energy efficiency on the Project Site, including installing Energy Star rated appliances and installation of water-conserving fixtures. Therefore, the Project is consistent with the Los Angeles Green Building Ordinance.

The Project’s design features as required by CALGreen would nevertheless be compatible with the City of Los Angeles’ Green Building Ordinance standards that reduce emissions beyond a NAT scenario and are consistent.

The Project would be consistent with applicable State, regional and local GHG reduction strategies. Given that the Project would generate GHG emissions that are less than significant, and given that GHG emission impacts are cumulative in nature, the Project’s incremental contribution to cumulatively significant GHG emissions would be less than cumulatively considerable, and impacts would be less than significant.

Cumulative Impacts

The emission of GHGs by a single project into the atmosphere is not itself necessarily an adverse environmental effect. Rather, it is the increased accumulation of GHG from more than one project and many sources in the atmosphere that may result in global climate change. The consequences of that climate change can cause adverse environmental effects. A project’s GHG emissions typically would be very small in comparison to state or global GHG emissions and, consequently, they would, in isolation, have no significant direct impact on climate change. The State has mandated a goal of reducing statewide emissions to 1990 levels by 2020, even though statewide population and commerce is predicted to continue to expand. In order to achieve this goal, CARB is in the process of establishing and implementing regulations to reduce statewide GHG emissions. At a minimum, most project-related emissions, such as energy, mobile, and construction, would be covered by the Cap-and-Trade Program.

Currently, there are no applicable CARB, SCAQMD, or City of Los Angeles significance thresholds or specific reduction targets, and no approved policy or guidance to assist in determining significance at the project or cumulative levels. Additionally, there is currently no generally accepted methodology to determine whether GHG emissions associated with a specific project represent new emissions or existing, displaced emissions. Therefore, consistent with CEQA Guideline Section 15064h(3), the City as Lead Agency has determined that the Project's contribution to cumulative GHG emissions and global climate change would be less than significant if the Project is consistent with the applicable regulatory plans and policies to reduce Greenhouse Gas Emissions: Executive Orders S-3-05 and B-30-15; AB 32, the 2012-2035 RTP/SCS and the City of Los Angeles Green Building Ordinance and Mobility 2035 Plan.

Implementation of the Project's regulatory compliance measures and project design features, including State mandates, would contribute to GHG reductions. These reductions represent a reduction from NAT and support State goals for GHG emissions reduction. The methods used to establish this relative reduction are consistent with the approach used in the CARB's *Climate Change Scoping Plan* for the implementation of AB 32.

The Project is consistent with the approach outlined in CARB's *Climate Change Scoping Plan*, particularly its emphasis on the identification of emission reduction opportunities that promote economic growth while achieving greater energy efficiency and accelerating the transition to a low-carbon economy. In addition, as recommended by CARB's *Climate Change Scoping Plan*, the Project would use "green building" features as a framework for achieving cross-cutting emissions reductions as new buildings and infrastructure would be designed to achieve the standards of CALGreen.

As part of SCAG's 2016-2040 SCS/RTP, a reduction in VMT within the region is a key component to achieve the 2020 and 2035 GHG emission reduction targets established by CARB. The Project results in significant VMT reduction in comparison to NAT and would be consistent with the SCS/RTP.

The Project also would indirectly comply with the City of Los Angeles Green Building Code, which emphasizes improving energy conservation and energy efficiency, increasing renewable energy generation, and changing transportation and land use patterns to reduce auto dependence. The Project's regulatory compliance measures and project design features provided above and throughout this analysis would advance these objectives. Further, the related projects would also be anticipated to comply with many of these same emissions reduction goals and objectives.

Additionally, the Project has incorporated sustainability design features in accordance with regulatory requirements as provided in the regulatory compliance measures throughout this analysis and project design features to reduce VMT (i.e., the Project site's location adjacent to transit service) and to reduce the Project's potential impact with respect to GHG emissions. With implementation of these features, the Project results in a 31 percent reduction in GHG emissions from NAT. The Project's GHG reduction measures make the Project consistent with AB 32.

As discussed above, the Project is consistent with the applicable GHG reduction plans and policies. The NAT comparison demonstrates the efficacy of the measures contained in these policies. Moreover, while the Project is not directly subject to the Cap and Program, that Program will indirectly reduce the Project's GHG emissions by regulating "covered entities" that affect the Project's GHG emissions, including energy, mobile, and construction emissions. More importantly, the Cap-and-Trade Program will backstop the GHG reduction plans and policies applicable to the Project in that the Cap-and-Trade Program will be responsible for relatively more emissions reductions should California's direct regulatory measures reduce GHG emissions less than expected. This will ensure that the GHG reduction targets of AB 32 are met.

Thus, given the Project's consistency with State, regional, and City of Los Angeles GHG emission reduction goals and objectives, the Project would not conflict with any applicable plan, policy, or regulation of an agency adopted for the purpose of reducing the emissions of GHGs. In the absence of adopted standards and established significance thresholds, and given this consistency, it is concluded that the Project's impacts are cumulatively less than significant.

8. HAZARDS AND HAZARDOUS MATERIALS

a) Would the project create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?

No Impact. The Project includes development of 42 single-family residential homes and would not require routine transport, use, or disposal of hazardous materials. Thus, the Project would not create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials. Therefore, no impacts related to this issue would occur.

b) Would the project create significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?

No Impact. A Phase I Environmental Site Assessment (ESA) was prepared for the Project by AEI Consultants (refer to Appendix F). The following are the findings of the assessment:

Recognized Environmental Condition (REC) is defined by the ASTM Standard Practice E1527-13 as the presence or likely presence of any hazardous substances or petroleum products in, on, or at a property: (1) due to release to the environment; (2) under conditions indicative of a release to the environment; or (3) under conditions that pose a material threat of a future release to the environment. AEI's assessment has revealed the following RECs associated with the subject property or nearby properties:

- Based on a review of aerial photographs, there is a potential that the subject property was historically used for agricultural purposes. There is a potential that agricultural chemicals, such as pesticides, herbicides and fertilizers, were used on site, and that the subject property has been

impacted by the use of such agricultural chemicals. In general, historical agricultural use is not the subject of environmental enforcement actions by regulatory agencies, and therefore, could be considered a de minimis condition. Additionally, potential agricultural uses were only noted in 1923; by 1928 potential agricultural uses had ceased. As such, any potential agricultural chemicals would have likely naturally attenuated since this time. However, AEI understands that the subject property is slated for redevelopment. Consequently, it is considered prudent to determine whether sampling relating to the former agricultural use of the subject property is required by the local planning department or other applicable oversight agency prior to the commencement of redevelopment activities. As such, AEI contacted the local planning department to determine whether sampling relating to the former agricultural use of the subject property is required in preparation for development, and the agency stated that no such requirements exist at this time.

Controlled Recognized Environmental Condition (CREC) is defined by the ASTM Standard Practice E1527-13 as a past release of hazardous substances or petroleum products that has been addressed to the satisfaction of the applicable regulatory authority, with hazardous substances or petroleum products allowed to remain in place subject to the implementation of required controls. AEI's assessment has revealed the following CRECs associated with the subject property or nearby properties:

- No on-site CRECs were identified during the course of this assessment.

Historical Recognized Environmental Condition (HREC) is defined by the ASTM Standard Practice E1527-13 as a past release of any hazardous substances or petroleum products that has occurred in connection with the property and has been addressed to the satisfaction of the applicable regulatory authority or meeting unrestricted use criteria established by a regulatory authority, without subjecting the property to any required controls. AEI's assessment has revealed the following HRECs associated with the subject property or nearby properties:

- No on-site HRECs were identified during the course of this assessment.

Other Environmental Considerations warrant discussion, but do not qualify as RECs as defined by the ASTM Standard Practice E1527-13. These include, but are not limited to, de minimis conditions and/or environmental considerations such as the presence of ACMs, LBP, radon, mold, and lead in drinking water, which can affect the liabilities and financial obligations of the client, the health and safety of site occupants, and the value and marketability of the subject property. AEI's assessment has revealed the following environmental considerations associated with the subject property or nearby properties:

- Although access to the subject property is currently gated, the area was formerly accessible by the general public. The subject property appears to have been utilized for unauthorized dumping of waste building materials, tires, and other equipment. Mr. Al Benegas, key site manager, indicated these materials were planned to be removed during the demolition of the current structures at the property. No dumping of hazardous materials was observed. No evidence of impact to the subject

property, such as surface staining, odors, stressed vegetation, or spillage of contents, was observed. Based on this information, the materials are not considered to represent evidence of a recognized environmental condition. However, the materials represent a housekeeping concern, and should be removed from the property. Additionally, it should be noted that if any of the building materials are found to be asbestos containing, additional costs may be incurred in removing these materials.

- Based on the date of development, it is possible that the subject property was historically equipped with at least one septic system. Based on the residential nature of occupancy, any on-site septic systems are not expected to represent a significant environmental concern. However, if any septic systems are encountered upon future redevelopment, they should be addressed under local regulatory guidelines.
- The northern adjacent property (AR Morse, Steve's Auto Repair; 2700 & 2706 North Eastern Avenue) was listed in the database as a LUST site. The case was opened during tank removal of two USTs. The contaminants of concern were listed as hydrocarbons. Soil was listed as the only media impacted. The case was open in 1989 and closed in 1996. However, no other significant information about the release was available from the regulatory database report or online. This property is located across Lombardy Boulevard (approximately 50-80 feet from the property boundary) in a hydrologically downgradient position relative to the subject property. Based on the information available to date, the site may be a potential source of vapor-phase contaminant migration. A review of the LUST case file with the RWQCB may provide additional information in determining if a potential source of vapor-phase contaminant migration is present. Based on the case closure, relative distance, media impacted, time elapsed allowing for natural attenuation, identification of a responsible party, and the hydrological gradient, groundwater impacts from this adjacent site are not expected to represent a significant environmental concern at this time.

Conclusions: The Phase I ESA was conducted in conformance with the scope and limitations of ASTM Standard Practice E1527-13 of the subject property and revealed no evidence of RECs in connection with the subject property other than those stated above. AEI recommends no further investigations for the subject property at this time. Therefore, no impacts related to this issue would occur.

c) Would the project emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?

No Impact. The Project includes development of 42 single-family residential homes and would not require routine transport, use, or disposal of hazardous materials. Also, the Project site is not located within one-quarter mile of an existing or proposed school. Thus, the Project would not emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school. Therefore, no impacts related to this issue would occur.

d) Would the project be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?

No Impact. The Project is not included on any list compiled pursuant to Government Code Section 6892.5. Thus, the Project would not create a significant hazard to the public or the environment as a result of being listed on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5. Therefore, no impacts related to this issue would occur.

e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?

No Impact. The Project site is not located within two miles of a public airport. The closest airport is the El Monte Airport located approximately 11 miles northeast of the site. Thus, the Project would not result in a safety hazard associated with an airport for people residing or working in the Project area. Therefore, no impacts related to this issue would occur.

f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?

No Impact. The Project site is not located within the vicinity of a private airstrip. The closest airport is the El Monte Airport located approximately 11 miles northeast of the site. Thus, the Project would not result in a safety hazard associated with an airport for people residing or working in the Project area. Therefore, no impacts related to this issue would occur.

g) Would the project impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?

No Impact. No aspects of the Project would inhibit access to hospitals, emergency response centers, school locations, communication facilities, highways and bridges, or airports. Further, the Project would comply with all applicable City policies related to disaster preparedness and emergency response. Thus, no impacts related to this issue would occur.

h) Would the project expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?

Less Than Significant Impact. The Project is located within a Very High Fire Hazard Severity Zone.⁶⁵ The Project would be required to be designed and constructed in accordance with the Los Angeles Fire Code and would be required to incorporate measures, including but not limited the following:

- Ignition-resistant roofing and other building materials
- Fire-Retardant-Treated Wood or noncombustible materials
- Roof coverings, valleys, and gutters
- Attic ventilation
- Eave or cornice vents
- Sprinkler systems
- Landscaping with fire-retardant plants
- Vegetation clearance

Additionally, prior to issuance of an Occupancy Permit, the Project Applicant would be required to coordinate with the Los Angeles Fire Department (LAFD) to ensure that the Project incorporates all appropriate fire-prevention measures. Through compliance with the LAFD's requirements, no significant impacts related to wildland fires would occur as a result of the Project.

Cumulative Impacts

The geographic extent of the Project's environmental impacts is limited to the Project site and would not contribute to any other potential environmental impact that may occur beyond the Project site boundaries. All related projects would be subject to discretionary or ministerial review by their respective jurisdictions, which would be responsible for assessing potential hazards risks associated with those related projects, and if necessary, the applicants of those projects would be required to implement measures appropriate for the type and extent of hazardous materials present and the land use proposed to reduce the risk associated with the hazardous materials to an acceptable level. As stated previously, with mitigation, the Project would not result in any significant impacts related to hazards and hazardous materials. Therefore, no significant cumulative impacts related to hazards and hazardous materials would occur.

⁶⁵ ZIMAS Report for the Project site, February 19, 2015.

9. HYDROLOGY AND WATER QUALITY

a) Would the project violate any water quality standards or waste discharge requirements?

No Impact. The Project includes development of 42 single-family residential homes and would not have any point-source discharges. Therefore, the Project would have no impact on water quality standards or waste discharge and would not violate any water quality standards or waste discharge requirements.

b) Would the project substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?

No Impact. The Project site contains hillside areas. Subsurface materials at the Project site primarily include artificial fill, colluvium, older alluvium, and bedrock of Monterey Formation. During storm events, most of the stormwater flows from the Project site to the local streets where the runoff enters the City's storm drain system. Ten borings were conducted at the Project site to a maximum depth of 45 feet. According to the Geologic & Geotechnical Engineering Review prepared for the Project, although seepage was encountered in three of the borings, no groundwater was encountered (refer to Appendix E). It is unlikely that any stormwater that contacts the Project site reaches groundwater level. For these reasons, the Project site is not an area of groundwater recharge. All water consumption associated with the Project would be supplied by the Metropolitan Water District (MWD) and not from groundwater beneath the Project site. Thus, the Project would have no affect on groundwater supplies or recharge, and no impacts related to this issue would occur.

c) Would the project substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner, which would result in substantial erosion or siltation on- or off-site?

Less Than Significant Impact. During the Project's construction phase, the Project developer would be required to implement SCAQMD Rule 403 – Fugitive Dust to minimize wind and water-borne erosion at the site. Also, the Project developer would be required to prepare and implement a SWPPP, in accordance with the NPDES General Permit for Discharges of Storm Water Associated with Construction Activity and Land Disturbance Activities. The site-specific SWPPP would be prepared prior to earthwork activities and would be implemented during Project construction. The SWPPP would include BMPs and erosion control measures to prevent pollution in storm water discharge. Typical BMPs that could be used during construction include good-housekeeping practices (e.g., street sweeping, proper waste disposal, vehicle and equipment maintenance, concrete washout area, materials storage, minimization of hazardous materials, proper handling and storage of hazardous materials, etc.) and erosion/sediment control measures (e.g., silt fences, fiber rolls, gravel bags, storm water inlet protection, and soil stabilization

measures, etc.). The SWPPP would be subject to review and approval by the City for compliance with the City's Development Best Management Practices Handbook, Part A, Construction Activities. Additionally, all Project construction activities would comply with the City's grading permit regulations, which require the implementation of grading and dust control measures, including a wet weather erosion control plan if construction occurs during rainy season, as well as inspections to ensure that sedimentation and erosion is minimized. Through compliance with these existing regulations, the Project would not result in any significant impacts related to soil erosion and siltation during the construction phase. Additionally, during the Project's operational phase, most of the Project site would be developed with impervious surface, and all stormwater flows would be directed to storm drainage features and would not come into contact with bare soil surfaces. Thus, no significant impacts related to erosion and siltation would occur as a result of Project operation.

d) Would the project substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?

Less Than Significant Impact. As stated previously, during storm events, most of the stormwater flows from the Project site to the local streets where the runoff enters the City's storm drain system. Although implementation of the Project would result in the creation of other impervious surfaces at the Project site, such as the proposed residential homes and driveways, the Project developer would be required to implement BMPs and to develop appropriate drainage infrastructure on the site to meet regulatory water quality requirements and to control drainage from the site to not exceed existing rates. Thus, the Project would not increase the runoff from the site entering the City's existing storm drain facilities. As such, the Project would not cause flooding on or off site. Therefore, Project impacts related to flooding would be less than significant.

e) Would the project create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?

Less Than Significant Impact. As stated previously, during storm events, most of the stormwater flows from the Project site to the local streets where the runoff enters the City's storm drain system. Although implementation of the Project would result in the creation of other impervious surfaces at the Project site, such as the proposed residential building, driveways, and pedestrian walkways, the Project developer would be required to implement BMPs and to develop appropriate drainage infrastructure on the site to meet regulatory water quality requirements and to control drainage from the site to not exceed existing rates. Thus, the Project would not increase the runoff from the site entering the City's existing storm drain facilities. As such, the Project would not exceed the capacity of the existing or planning drainage system. Therefore, Project impacts related to storm drain capacity would be less than significant.

f) Would the project otherwise substantially degrade water quality?

Less Than Significant Impact. To address water quality during the Project's construction phase, the Project Applicant would be required to prepare and implement a SWPPP, in accordance with the NPDES General Permit for Discharges of Storm Water Associated with Construction Activity and Land Disturbance Activities. The site-specific SWPPP would be prepared prior to earthwork activities and would be implemented during Project construction. The SWPPP would include BMPs and erosion control measures to prevent pollution in storm water discharge. Typical BMPs that could be used during construction include good-housekeeping practices (e.g., street sweeping, proper waste disposal, vehicle and equipment maintenance, concrete washout area, materials storage, minimization of hazardous materials, proper handling and storage of hazardous materials, etc.) and erosion/sediment control measures (e.g., silt fences, fiber rolls, gravel bags, storm water inlet protection, and soil stabilization measures, etc.). The SWPPP would be subject to review and approval by the City for compliance with the City's Development Best Management Practices Handbook, Part A, Construction Activities. Additionally, all Project construction activities would comply with the City's grading permit regulations, which require the implementation of grading and dust control measures, including a wet weather erosion control plan if construction occurs during rainy season, as well as inspections to ensure that sedimentation and erosion is minimized. Therefore, through compliance with NPDES requirements and City grading regulations, Project construction impacts related to water quality would be less than significant.

During the Project's construction phase, in accordance with the City's Low Impact Development (LID) Ordinance, the Project Applicant would be required to incorporate appropriate stormwater pollution control measures into the design plans and submit these plans to the City's Department of Public Works, Bureau of Sanitation, Watershed Protection Division (WPD) for review and approval. Upon satisfaction that all stormwater requirements have been met, WPD staff would stamp the plan approved. Through compliance with the City's LID Ordinance, the Project would meet the City's water quality standards. Therefore, Project impacts related to operational water quality would be less than significant.

g) Would the project place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?

No Impact. The Project site is not located within a 100-year flood hazard area. Thus, the Project would not place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map. Therefore, no impacts related to this issue would occur.

h) Would the project place within a 100-year flood hazard area structures which would impede or redirect flood flows?

No Impact. The Project site is not located within a 100-year flood hazard area. Thus, the Project would not place within a 100-year flood hazard area structures that would impede or redirect flood flows. Therefore, no impacts related to this issue would occur.

i) Would the project expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?

No Impact. The Project site is not located in any area susceptible to floods associated with a levee or dam. Thus, the Project would not expose people or structures to a significant risk of loss, injury, or death involving flooding, including flooding as a result of the failure of a levee or dam. Therefore, no impacts related to this issue would occur.

j) Would the project expose people or structures to a significant risk of loss, injury or death involving inundation by seiche, tsunami, or mudflow?

No Impact. The Project site is not in an area susceptible to seiches, tsunamis, or mudflows. Thus, the Project would not expose people or structures to a significant risk of loss, injury, or death involving inundation by seiche, tsunami, or mudflow. Therefore, no impacts related to this issue would occur.

Cumulative Impacts

The sites of the proposed Project and the related projects are located in an urbanized area where most of the surrounding properties are already developed. The existing storm drainage system serving this area has been designed to accommodate runoff from an urban built-out environment. When new construction occurs it generally does not lead to substantial additional runoff, since new developments is required to control the amount and quality of stormwater runoff coming from their respective sites. Additionally, all new development in the City is required to comply with the City's LID Ordinance and incorporate appropriate stormwater pollution control measures into the design plans to ensure that water quality impacts are minimized. Therefore, cumulative impacts related to hydrology and water quality would be less than significant.

10. LAND USE AND PLANNING

a) Would the project physically divide an established community?

No Impact. The Project site is located in an urbanized area of the City. The site is surrounded by existing open space, school, commercial, and residential land uses, roadways, and other infrastructure. The site is zoned and designated for residential land uses. Thus, the Project would not physically divide an established community. Therefore, no impacts related to this issue would occur.

b) **Would the project conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?**

Less Than Significant Impact. As discussed below, the Project would be substantially consistent with all of the applicable plans, policies, and regulations associated with development of the Project site. Therefore, Project impacts related to land use and planning would be less than significant.

Regulatory Framework

Regional Plans

Southern California Association of Governments

The Southern California Association of Governments (SCAG) functions as the Metropolitan Planning Organization for six counties: Los Angeles, Orange, San Bernardino, Riverside, Ventura, and Imperial. The SCAG region encompasses a population exceeding 18 million persons in an area of more than 38,000 square miles. As the federally-designated Metropolitan Planning Organization, SCAG is mandated to research and create plans for transportation, growth management, hazardous waste management, and air quality. Applicable SCAG publications are discussed below.

Compass Blueprint Growth Vision Report/Compass Blueprint 2% Strategy Areas

The Compass Blueprint Growth Vision Report/Compass Blueprint 2% Strategy (the “Compass Blueprint Report”), adopted by SCAG as part of its June 2004 Southern California Compass Growth Vision Report, is an implementing mechanism for the regional growth strategies outlined in the SCAG’s 1996 Regional Comprehensive Plan and Guide (the “RCPG”). The Compass Blueprint Report is intended to provide a strategy to accommodate the projected 24 million residents expected to live in the region by 2035, while balancing valuable quality of life goals. The Compass Blueprint Report emphasizes focusing growth in existing and emerging centers and along major transportation corridors, creating significant areas of mixed-use development and walkable communities, targeting growth around existing and planned transit stations, and preserving existing open space and stable residential areas.

Four principles were established for the Compass Blueprint Report that are intended to promote and maximize regional mobility, livability, prosperity, and sustainability. It is SCAG’s intention that decisions regarding growth, transportation, land use, and economic development should support and be guided by these principles. Specific policy and planning strategies are also provided as a way to achieve each of the principles, as summarized below.

- *Principle 1. Improve mobility for all residents.* Strategies to support Principle 1 include: (1) encourage transportation investments and land use decisions that are mutually supportive; (2)

locate new housing near existing jobs and new jobs near existing housing; (3) encourage transit-oriented development; and (4) promote a variety of travel choices.

- *Principle 2. Foster livability in all communities.* Strategies to support Principle 2 include: (a) promote infill development and redevelopment to revitalize existing communities; (b) promote developments that provide a mix of uses; (c) promote “people scaled,” pedestrian friendly communities; and (d) support the preservation of stable, single-family neighborhoods.
- *Principle 3. Enable prosperity for all people.* Strategies to support Principle 3 include: (a) provide a variety of housing types in each community to meet the housing needs of all income levels; (b) support educational opportunities that promote balanced growth; (c) ensure environmental justice regardless of race, ethnicity, or income class; (d) encourage civic engagement; and (e) support local and state fiscal policies that encourage balanced growth.
- *Principle 4. Promote sustainability for future generations.* Strategies to support Principle 4 include: (a) preserve rural, agricultural, recreational, and environmentally sensitive areas; (b) focus development in urban centers and existing cities; (c) develop strategies to accommodate growth that use resources efficiently, eliminate pollution, and significantly reduce waste; and (d) utilize “green” development techniques.

The Compass Blueprint Report is a guideline for how and where the Growth Vision can be implemented. It calls for moderate changes to current land use and transportation trends in two percent of the land area of the region, known as the 2% Strategy Opportunity Areas. These areas are defined as having a high potential to implement projects, plans, and/or policies consistent with the Compass Blueprint Report principles that would result in the greatest progress towards economic, mobility, livability and sustainability benefits to local neighborhoods.

Regional Comprehensive Plan

SCAG has also prepared the 2008 Regional Comprehensive Plan (the “2008 RCP”) in response to SCAG’s Regional Council directive in the 2002 Strategic Plan to define solutions to interrelated housing, traffic, water, air quality, and other regional challenges. The 2008 RCP is an advisory document that describes future conditions if current trends continue, defines a vision for a healthier region, and recommends an Action Plan with a target year of 2035. The 2008 RCP may be voluntarily used by local jurisdictions in developing local plans and addressing local issues of regional significance. The plan incorporates principles and goals of the Compass Growth Vision Report and includes nine chapters addressing land use and housing, transportation, air quality, energy, open space, water, solid waste, economy, and security and emergency preparedness. The action plans contained therein provide a series of recommended near-term policies that developers and key stakeholders should consider for implementation, as well as potential policies for consideration by local jurisdictions and agencies when conducting project review.

The 2008 RCP replaced the RCPG for use in SCAG's Intergovernmental Review (IGR) process. SCAG's Community, Economic and Human Development Committee and the Regional Council took action to accept the 2008 RCP, which now serves as an advisory document for local governments in the SCAG region for their information and voluntary use in developing local plans and addressing local issues of regional significance. However, as indicated by SCAG, because of its advisory nature, the 2008 RCP is not used in SCAG's IGR process. Rather, SCAG reviews new projects based on consistency with the Regional Transportation Plan (the "RTP") (discussed below) and the Compass Blueprint Report.

2016-2040 Regional Transportation Plan/Sustainable Communities Strategy

On September 30, 2008, SB 375 was instituted to help achieve AB 32 goals through regulation of cars and light trucks. SB 375 aligns three policy areas of importance to local government: (1) regional long-range transportation plans and investments; (2) regional allocation of the obligation for cities and counties to zone for housing; and (3) a process to achieve GHG emissions reductions targets for the transportation sector. It establishes a process for the CARB to develop GHG emissions reductions targets for each region (as opposed to individual local governments or households). SB 375 also requires Metropolitan Planning Organizations to prepare a Sustainable Communities Strategy (SCS) within the RTP that guides growth while taking into account the transportation, housing, environmental, and economic needs of the region. SB 375 uses CEQA streamlining as an incentive to encourage residential projects, which help achieve AB 32 goals to reduce GHG emissions.

On September 23, 2010, CARB adopted regional targets for the reduction of GHG emissions applying to the years 2020 and 2035. For the area under the SCAG jurisdiction, including the Project area, CARB adopted Regional Targets for reduction of GHG emissions by eight percent for 2020 and by 13 percent for 2035. On February 15, 2011, CARB's Executive Officer approved the final targets.

On April 4, 2012, the Regional Council of SCAG adopted the 2012-2035 Regional Transportation Plan/Sustainable Communities Strategy (the "2012-2035 RTP/SCS"). SCAG updates the RTP/SCS every four years. Through the conduct of a continuing, comprehensive, and coordinated transportation planning process in conformance with all applicable federal and state requirement, SCAG developed and prepared its latest RTP/SCS, the Final 2016-2040 RTP/SCS (the "2016-2040 RTP/SCS"), which sets forth the long-range regional plan, policies and strategies for transportation improvements and regional growth throughout the SCAG region through the horizon year of 2040, includes a regional growth forecast that was developed by working with local jurisdictions using the most recent land use plans and policies and planning assumptions, and a financially constrained plan and a strategic plan. The constrained plan includes transportation projects that have committed, available or reasonably available revenue sources, and thus, are probable for implementation. The strategic plan is an illustrative list of additional transportation investments that the region would pursue if additional funding and regional commitment were secured. Such investments are potential candidates for inclusion in the constrained RTP/SCS through future amendments or updates. The strategic plan is provided for information purposes only and is not part of the financially constrained and conforming 2016-2040 RTP/SCS.

The 2016-2040 RTP/SCS includes a financial plan identifying the revenues committed, available or reasonably available to support the SCAG region's surface transportation investments. The financial plan was developed following basic principles including incorporation of county and local financial planning documents in the region where available, and utilization of published data sources to evaluate historical trends and augment local forecasts as needed.

The 2016-2040 RTP/SCS includes a sustainable communities strategy which sets forth a forecasted development pattern for the region, which, when integrated with the transportation network, and other transportations measures and policies, if implemented, will reduce the GHG emissions from automobiles and light trucks to achieve the regional GHG targets set by CARB for the SCAG region.

South Coast Air Quality Management District

Air Quality Management Plan

The Project is also located within the South Coast Air Basin (the "Basin") and is, therefore, within the jurisdiction of the SCAQMD. In conjunction with SCAG, the SCAQMD is responsible for formulating and implementing air pollution control strategies, including periodic updates to the AQMP, and guidance to local government about how to incorporate these strategies into their land use plans and decisions about development.

SCAG is responsible for generating the socio-economic profiles and growth forecasts on which land use, transportation, and air quality management and implementation plans are based. The growth forecasts provide the socioeconomic data used to estimate vehicle trips and VMT. Emission estimates then can be forecast by SCAQMD based on these projected estimates. Reductions in emissions due to changes in the socio-economic profile of the region are an important way of taking account of changes in land use patterns. For example, changes in jobs/housing balance induced by changes in urban form and transit-oriented development induce changes in VMT by more closely linking housing to jobs. Thus, socio-economic growth forecasts are a key component to guide the Basin toward attainment of the NAAQS.

The current AQMP establishes a comprehensive regional air pollution control program leading to the attainment of State and federal air quality standards in the Basin. In addition to setting minimum acceptable exposure standards for specified pollutants, the AQMP incorporates SCAG's growth management strategies that can be used to reduce vehicle trips and VMT, and hence air pollution. These include, for example, co-location of employment and housing, and mixed-use land patterns that allow the integration of residential and non-residential uses.

Air quality impacts of the Project and consistency of the Project with the AQMP are discussed in response to Checklist Question 3a of this IS/MND.

*Los Angeles County Metropolitan Transportation Authority*Congestion Management Plan

The Congestion Management Plan (CMP) for Los Angeles County is intended to address vehicular congestion relief by linking land use, transportation, and air quality decisions. The CMP also seeks to develop a partnership among transportation decision-makers to devise appropriate transportation solutions that include all modes of travel, and to propose transportation projects, which are eligible to compete for state gas tax funds. Within Los Angeles, the Los Angeles County Metropolitan Transportation Authority (Metro) is the designated congestion management agency responsible for coordinating the CMP.

The Project's potential impacts with respect to the CMP are discussed in response to Checklist Question 16b of this IS/MND.

Local Plans*City of Los Angeles*City of Los Angeles General Plan

The City of Los Angeles General Plan (the "General Plan"), adopted December 1996 and re-adopted August 2001, provides general guidance on land use issues for the entire City. The General Plan consists of a Framework Element, a Land Use Element, and 10 citywide elements. The Framework Element of the General Plan serves as guide for the City's overall long-range growth and development policies and serves as a guide to update the community plans and the citywide elements. The citywide elements address functional topics that cross community boundaries, such as transportation, and address these topics in more detail than is appropriate in the Framework Element, which is the "umbrella document" that provides the direction and vision necessary to bring cohesion to the City's overall general plan. The Framework Element provides a conceptual relationship between land use and transportation, and provides guidance for future updates to the various elements of the General Plan, but does not supersede the more detailed community and specific plans. The Land Use chapter of the Framework Element contains Long Range Land Use Diagrams that depict the generalized distribution of centers, districts, and mixed-use boulevards throughout the City, but the community plans determine the specific land use designations. The Land Use Element of the General Plan is contained within 35 community plans.

Northeast Los Angeles Community Plan

The Northeast Los Angeles Community Plan area was established more than 30 years ago to encompass the hills and valleys lying east of the Los Angeles River and north of the Boyle Heights Community Plan area within the City. The area serves as a transition between the downtown center of Los Angeles and the neighboring cities of Glendale, Pasadena, South Pasadena, and Alhambra to the north and east, as well as the City of Monterey Park and the unincorporated community of City of Terrace on the south.

The Community Plan area comprises some 15,000 acres and is occupied by roughly 250,000 inhabitants living in a diverse collection of communities and neighborhoods. Their histories can be traced back to the mid-nineteenth century when the first farms and orchards, subdivisions, railroad and streetcar lines, and irrigation canals were established.

By the beginning of the twentieth century, Northeast Los Angeles was a major gateway to traffic moving between Central Los Angeles and distant regions to the east and north. It was also recognized throughout the emerging metropolis as the location of major recreational resources (Eastlake Park and the Los Angeles Zoo), the largest medical facility (General Hospital), one of the area's most important centers of higher learning (Occidental College), and the City's first museum, the Southwest Museum.

By the end of the Twentieth Century, these institutions largely remain and have been augmented by the Southwest Indian Museum, the University of Southern California Health Sciences Schools, and California State University at Los Angeles, as well as a major shopping center, The Eagle Rock Plaza. However, the area's prominence in the region has been diminished since World War II because of the tremendous exodus to growing suburbs fostered by freeway development and commercial and industrial decentralization that characterized development in Southern California.

The impact of freeway development on the Plan Area cannot be overemphasized. It has provided an efficient means for developing outlying areas and allowing the resulting traffic to bypass the older industrial and commercial corridors of Northeast Los Angeles. It also had the effect of dividing former neighborhoods and communities; altering established commercial activity almost exclusively serving the immediate neighborhoods scattered along Cypress Avenue, Figueroa Street, and San Fernando Road.

These major developments in Northeast Los Angeles have changed the arrangement of land uses and the relationship of the plan area with the rest of the expanding metropolis. However, within the plan area, the distinctiveness of neighborhoods and communities persists because they are separated by hills and watercourses, and man-made features such as railroad tracks and freeways. Localized demographic, social, and economic factors have also varied over time.

The Project site falls within the El Sereno community, which is located in the southeast part of the Plan area adjacent to the cities of South Pasadena, Alhambra, and Monterey Park and City Terrace, an unincorporated community in Los Angeles County. The San Bernardino Freeway (I-10) generally corresponds to the southern boundary, and the Long Beach Freeway (I-710) and its proposed northerly extension to Pasadena parallels the eastern boundary. Huntington Drive, which formerly carried a major streetcar line is the principal commercial east-west corridor, and Eastern Avenue is the most prominent north-south commercial street.

Land uses have evolved into a complex and troublesome mixture in some areas. Residential uses are often not buffered adequately from neighboring industrial and commercial uses. Some extremely large apartment complexes intrude into older, lower density residential areas. There are also inadequate

neighborhood retail services to support the areas where several of the large residential complexes, mostly built in the 1980's, are concentrated.

Entertainment uses are almost non-existent. In recent years, Glassell Park has increased its visibility and identity in Northeast Los Angeles. The Mount Washington/Glassell Park Specific Plan is widely known as the primary mechanism regulating development east of Verdugo Road and south of El Paso Drive. Moreover, the community has erected attractive monument signs in the median of Eagle Rock Boulevard to announce itself to passing motorists.

The land use designation for the Project site in the Northeast Los Angeles Community Plan is Low Residential.

City of Los Angeles Planning and Zoning Code

All development activity in the City, including the Project site, is subject to the LAMC, particularly Chapter 1, General Provisions and Zoning, also known as the City of Los Angeles Planning and Zoning Code (the "Zoning Code"). The Zoning Code includes development standards for the various districts in the City. As shown on Figure II-4 (refer to Section II, Project Description), the Project site is currently zoned [Q]R1-1D (Qualified Condition, One-Family Zone, Height District 1) and [Q]RD6-1D (Qualified Condition, Restricted Density Multiple Dwelling Zone, Height District 1).

Project Impacts

Compass Blueprint Report

The Project's consistency with the Compass Blueprint Report is discussed on Table IV-13. As discussed, the Project would be consistent with applicable land use policies of the Compass Blueprint Report, and Project impacts related to inconsistency with this report would be less than significant.

Table IV-13
Project Consistency with Applicable Policies of the Compass Blueprint Report

Policy	Project Consistency
Encourage transportation investments and land use decisions that are mutually supportive.	Consistent. The Project would take advantage of existing and proposed transportation investments by redeveloping the Project site with land uses that are consistent with the existing Low Residential land use designation for the Project site near existing transit lines.
Locate new housing near existing jobs and new jobs near existing housing.	Consistent. The Project is infill development of housing within the Northeast Los Angeles Community Plan area of the City and within proximity to transit.

Table IV-13
Project Consistency with Applicable Policies of the Compass Blueprint Report

Policy	Project Consistency
Encourage transportation investments and land use decisions that are mutually supportive.	Consistent. The Project is infill development of housing within the Northeast Los Angeles Community Plan area of the City and within proximity to transit.
Locate new housing near existing jobs and new jobs near existing housing.	Consistent. The Project is infill development of housing within the Northeast Los Angeles Community Plan area of the City and within proximity to transit.
Encourage transit-oriented development.	Consistent. The Project site is in close proximity to existing transit lines, including Metro lines 76, 78, 79, 378, and 256 and LADOT's DASH line.
Promote a variety of travel choices.	Consistent. The Project site is in close proximity to existing transit lines, including Metro lines 76, 78, 79, 378, and 256 and LADOT's DASH line .
Promote infill development and redevelopment to revitalize existing communities.	Consistent. The Project is infill development of housing within the Northeast Los Angeles Community Plan area of the City and within proximity to transit.
Support the preservation of stable single-family neighborhoods.	Consistent. The Project site is zoned for single-family residential land uses, and the Project includes development of single-family homes, consistent with the existing Low Residential land use designation for the Project site and the City's Small Lot Subdivision Ordinance. The Project would be an extension of the existing single-family homes located to the northeast/east of the Project site, and would not impinge on any existing single-family neighborhoods.
Provide a variety of housing types in each community to meet the housing needs of all income levels.	Consistent. The Project includes development of 42 homes, each with 3 bedrooms.
Focus development in urban centers and existing cities.	Consistent. The Project is infill development of housing within the Northeast Los Angeles Community Plan area of the City and within proximity to transit.
Utilize "green" development techniques.	Consistent. The Project would comply with CalGreen requirements of the California Building Code and incorporates green and conservation features. The Project would also be consistent with the City of Los Angeles Building Code, which includes measures to reduce the Project's energy and water use, reduce

Table IV-13
Project Consistency with Applicable Policies of the Compass Blueprint Report

Policy	Project Consistency
	waste, and reduce the carbon footprint.
Develop strategies to accommodate growth that use resources efficiently, and minimize pollution and greenhouse gas emissions.	Consistent. The Project includes development of single-family residential land uses, land uses that are allowed under the existing land use designation.. The Project is infill development of housing within the Northeast Los Angeles Community Plan area of the City and within proximity to transit.
<i>Source: Southern California Association of Governments, Southern California Compass Blueprint 2% Strategy, Southern California Compass Blueprint Growth Vision Report, June 2004.</i>	

2008 RCP

A discussion of the Project's consistency with the relevant policies of the 2008 RCP is presented on Table IV-14. As discussed, the Project would be consistent with all of the applicable 2008 RCP policies, and no significant impacts related to inconsistency with the 2008 RCP would occur.

Table IV-14
Project Consistency with the 2008 RCP

Policies	Consistency Discussion
Land Use and Housing	
LU-4 Local governments should provide for new housing, consistent with State Housing Element law, to accommodate their share of forecast regional growth.	Consistent. The Project would provide 42 net dwelling units, which would accommodate a share of the forecasted regional growth.
LU-4.1 Local governments should adopt and implement General Plan Housing Elements that accommodate housing needs identified through the Regional Housing Needs Assessment (RHNA) process. Affordable housing should be provided consistent with RHNA income category distributions adopted for each jurisdiction. To provide housing, especially affordable housing, jurisdictions should leverage existing State programs such as HCD's Workforce Incentive Program and density bonus law and create local incentives (e.g., housing trust funds, inclusionary zoning, tax-increment-financing districts in redevelopment areas and transit villages) and partnerships with non-governmental stakeholders.	Consistent. As discussed in response to Checklist Question 13a, the Project would provide housing that is consistent with housing needs called out in the RHNA.

**Table IV-14
Project Consistency with the 2008 RCP**

Policies	Consistency Discussion
LU-6.2 Developers and local governments should integrate green building measures into project design and zoning such as those identified in the U.S. Green Building Council's Leadership in Energy and Environmental Design, Energy Star Homes, Green Point Rated Homes, and the California Green Builder Program.	Consistent. The Project would comply with CalGreen requirements of the California Building Code and incorporates green and conservation features. The Project would also be consistent with the City of Los Angeles Building Code, which includes measures to reduce the Project's energy and water use, reduce waste, and reduce the carbon footprint.
Open Space and Habitat	
PleOSC-10 Developers and local governments should promote infill development and redevelopment to revitalize existing communities.	Consistent. The Project is an infill development in an existing community.
OSC-11 Developers should incorporate and local governments should include land use principles, such as green building, that use resources efficiently, eliminate pollution and significantly reduce waste into their projects, zoning codes and other implementation mechanisms.	Consistent. The Project would incorporate sustainable building practices to eliminate pollution and reduce waste. As described above, the Project would comply with the CalGreen requirements of the California Building Code.
OSC-12 Developers and local governments should promote water-efficient land use and development.	Consistent. The Project would comply with CalGreen requirements of the California Building Code, which includes measures to reduce the Project's energy and water use. This would include the use of drought tolerant landscaping and water efficient fixtures and plumbing.
OSC-14 Developers and local governments should implement mitigation for open space impacts through the following activities: <ul style="list-style-type: none"> • Individual projects should either avoid significant impacts to regionally significant open space resources or mitigate the significant impacts through measures consistent with regional open space policies for conserving natural lands, community open space, and farmlands. All projects should demonstrate consideration of alternatives that would avoid or reduce impacts to open space. • Project sponsors should ensure that transportation systems proposed in the RTP avoid or mitigate significant impacts to natural lands, community open space and important farmland, including cumulative impacts and open space impacts from the growth associated with transportation projects 	Consistent. The Project would be an urban infill development that avoids significant impacts to regionally significant open space resources. The Project is located on a developed site surrounded by a dense urban environment in the City. There are no rural, agricultural, recreational, or environmentally sensitive areas on the Project site.

Table IV-14
Project Consistency with the 2008 RCP

Policies	Consistency Discussion
<p>and improvements.</p> <ul style="list-style-type: none"> • Project sponsors should fully mitigate direct and indirect impacts to open space resulting from implementation of regionally significant impacts. 	
Water	
<p>WA-9 Developers and local governments should consider potential climate change hydrology and resultant impacts on available water supplies and reliability in the process of creating or modifying systems to manage water resources for both year-round use and ecosystem health.</p>	<p>Consistent. The Project would comply with CalGreen requirements of the California Building Code and incorporates green and conservation features. The Project would also be consistent with the City of Los Angeles Building Code, which includes measures to reduce the Project's energy and water use, reduce waste, and reduce the carbon footprint.</p>
<p>WA-11 Developers and local governments should encourage urban development and land uses to make greater use of existing and upgraded facilities prior to incurring new infrastructure impacts.</p>	<p>Consistent. The Project would be required to confirm with LADWP that the capacity of the existing water infrastructure could supply the domestic needs of the Project during the construction and operation phases. The Project Applicant would be required to construct any upgrade to the water infrastructure serving the Project site that is needed to accommodate the Project's water consumption needs.</p>
<p>WA-12 Developers and local governments should reduce exterior uses of water in public areas, and should promote reduced use in private homes and businesses, by shifting to drought-tolerant native landscape plants (xeriscaping), using weather-based irrigation systems, educating other public agencies about water use, and installing water related pricing incentives.</p>	<p>Consistent. The Project would comply with CalGreen requirements of the California Building Code and incorporates green and conservation features. The Project would also be consistent with the City of Los Angeles Building Code, which includes measures to reduce the Project's energy and water use, reduce waste, and reduce the carbon footprint.</p>
<p>WA-32 Developers and local governments should pursue water management practices that avoid energy waste and create energy savings/supplies.</p>	<p>Consistent. The Project would comply with CalGreen requirements of the California Building Code for water and energy conservation.</p>
Energy	
<p>EN-8 Developers should incorporate and local governments should include the following land use principles that use resources efficiently, eliminate pollution and significantly reduce waste into their projects, zoning codes and other</p>	<p>Consistent. The Project includes development of single-family residential land uses, land uses that are allowed under the existing land use designation.. The Project is infill development of housing within the</p>

**Table IV-14
Project Consistency with the 2008 RCP**

Policies	Consistency Discussion
<p>implementation mechanisms:</p> <ul style="list-style-type: none"> Mixed-use residential and commercial development that is connected with public transportation and utilizes existing infrastructure. Land use and planning strategies to increase biking and walking trips. 	<p>Northeast Los Angeles Community Plan area of the City and within proximity to transit.</p>
<p>EN-10 Developers and local governments should integrate green building measures into project design and zoning such as those identified in the U.S. Green Building Council's Leadership in Energy and Environmental Design, Energy Star Homes, Green Point Rated Homes, and the California Green Builder Program. Energy saving measures that should be explored for new and remodeled buildings include:</p> <ul style="list-style-type: none"> Using energy efficient materials in building design, construction, rehabilitation, and retrofit. Encouraging new development to exceed Title 24 energy efficiency requirements. Developing Cool Communities measures including tree planting and light-colored roofs. These measures focus on reducing ambient heat, which reduces energy consumption related to air conditioning and other cooling equipment. Utilizing efficient commercial/residential space and water heaters: This could include the advertisement of existing and/or development of additional incentives for energy efficient appliance purchases to reduce excess energy use and save money. Federal tax incentives are provided online at http://www.energystar.gov/index.cfm?c=Projects.pr_tax_credits. Encouraging landscaping that requires no additional irrigation: utilizing native, drought tolerant plants can reduce water usage up to 60 percent compared to traditional lawns. Encouraging combined heating and cooling (CHP), also known as cogeneration, in all buildings. Encouraging neighborhood energy systems, which allow communities to generate their own electricity. Orienting streets and buildings for best solar access. Encouraging buildings to obtain at least 20% of their electric load from renewable energy. 	<p>Consistent. The Project would meet/exceed Title 24 standards through compliance with the CalGreen standards.</p>
<p>EN-12 Developers and local governments should encourage that new buildings are able to incorporate solar panels in roofing and tap other renewable energy sources to offset</p>	<p>Partially Consistent. Although the Project is not required to include solar panels, the Project would receive electricity supply from LADWP, which</p>

Table IV-14
Project Consistency with the 2008 RCP

Policies	Consistency Discussion
new demand on conventional power sources.	obtains a portion of its electricity supplies from renewable sources.
Solid Waste	
<p>SW-14 Developers and local governments should integrate green building measures into project design and zoning including, but not limited to, those identified in the U.S. Green Building Council's Leadership in Energy and Environmental Design, Energy Star Homes, Green Point Rated Homes, and the California Green Builder Program. Construction reduction measures to be explored for new and remodeled buildings include:</p> <ul style="list-style-type: none"> • Reuse and minimization of construction and demolition (C&D) debris and diversion of C&D waste from landfills to recycling facilities. • An ordinance that requires the inclusion of a waste management plan that promotes maximum C&D diversion. • Source reduction through (1) use of building materials that are more durable and easier to repair and maintain, (2) design to generate less scrap materials through dimensional planning, (3) increased recycled content, (4) use of reclaimed building materials, and (5) use of structural materials in a dual role as finish material (e.g., stained concrete flooring, unfinished ceilings, etc.). • Reuse of existing building structure and shell in renovation projects. <p>Building lifetime waste reduction measures that should be explored for new and remodeled buildings include:</p> <ul style="list-style-type: none"> • Development of indoor recycling program and space. • Design for deconstruction. • Design for flexibility through use of moveable walls, raised floors, modular furniture, moveable task lighting, and other reusable components. 	<p>Consistent. The Project would participate in a demolition and construction waste recycling program as well as an operational recycling program.</p>
<p><i>Source: Southern California Association of Governments, Regional Comprehensive Plan, October 2008.</i></p>	

2016-2040 RTP/SCS

The Project's consistency with the applicable goals of the 2016-2040 RTP/SCS is discussed on Table IV-15. As discussed, the Project would be consistent with the 2016-2040 RTP/SCS. Therefore, impacts related to inconsistency with the 2016-2040 RTP/SCS would be less than significant.

**Table IV-15
Project Consistency with the 2016-2040 RTP/SCS**

Goal	Consistency Discussion
Protect the environment and health of our residents by improving air quality and encouraging active transportation (non-motorized transportation, such as bicycling and walking).	Consistent. The Project would reduce VMT by providing a residential infill development in close proximity to existing transit lines, including Metro lines 76, 78, 79, 378, and 256 and LADOT's DASH line.
Actively encourage and create incentives for energy efficiency, where possible.	Consistent. The Project would comply with CalGreen requirements of the California Building Code, for water and energy conservation. The Project would exceed Title 24 standards with compliance with the City's Green Building Ordinance and the Project would also be consistent with the City of Los Angeles Building Code, which includes measures to reduce the Project's energy and water use, reduce waste, and reduce the carbon footprint.
Encourage land use and growth patterns that facilitate transit and non-motorized transportation.	Consistent. The Project would reduce VMT by providing a residential infill development in close proximity to existing transit lines.
<i>Source: Southern California Association of Governments, Regional Transportation Plan/Sustainable Communities Strategy, April 2012.</i>	

General Plan (Framework Element)

The Project's consistency with the General Plan Framework Element land use policies is discussed on Table IV-16. As shown, the Project would be consistent with many of the applicable policies, and Project impacts related to inconsistency of the Project with the General Plan Framework Element would be less than significant.

Table IV-16
Project Consistency with Applicable Policies of the Framework Element

Objective	Project Consistency
Framework Element: Land Use Chapter	
3.2.1 Provide a pattern of development consisting of distinct districts, centers, boulevards, and neighborhoods that are differentiated by their functional role, scale, and character. This shall be accomplished by considering factors such as the existing concentrations of use, community-oriented activity centers that currently or potentially service adjacent neighborhoods, and existing or potential public transit corridors and stations.	Consistent. The Project includes infill development of single-family residential land uses that are allowed under the existing land use designation..
3.2.2 Establish, through the Framework Long-Range Land Use Diagram, community plans, and other implementing tools, patterns and types of development that improve the integration of housing with commercial uses and the integration of public services and various densities of residential development within neighborhoods at appropriate locations.	Consistent. The Project includes infill development of single-family residential land uses that are allowed under the existing land use designation..
3.2.4 Provide for the siting and design of the City's stable residential neighborhoods and enhance the character of commercial and industrial districts.	Consistent. The Project site is zoned and designated for single-family residential land uses. The Project includes development of the Project site with single-family residential uses that are allowed under the existing land use designation..
3.7.1 Accommodate the development of multi-family residential units in areas designated in the community plans...with the density permitted for each parcel to be identified in the community plans.	Consistent. The Project site is zoned and designated for single-family residential land uses. The Project includes development of the Project site with single-family residential uses that are allowed under the existing land use designation for the site.
3.7.4 Improve the quality of new multi-family dwelling units based on the standards in <u>Chapter 5 Urban Form and Neighborhood Design</u> Chapter of this Element.	Consistent. The Project would be required to comply with all of the City's applicable design standards.
<i>Source: City of Los Angeles General Plan.</i>	

Northeast Community Plan

As discussed on Table IV-17, the Project would be consistent with all applicable policies of the Northeast Los Angeles Community Plan. As such, the Project would not result in any inconsistencies with the Plan.

Therefore, Project impacts related to inconsistency with the Northeast Los Angeles Community Plan would be less than significant.

**Table IV-17
Project Consistency with Applicable Policies of the
Northeast Los Angeles Community Plan**

Policy	Project Consistency
<i>Residential</i>	
1-1.1 Protect existing stable single-family and other lower density residential neighborhoods from encroachment by higher density residential and other uses that are incompatible as to scale and character or would otherwise diminish the quality of life.	Consistent. The Project includes development of single-family homes and is an extension of the existing single-family residential neighborhood in the existing El Sereno neighborhood within the Northeast Los Angeles Community Plan area.
1-1.2 Promote neighborhood preservation, particularly in existing single-family neighborhoods, as well as in areas with existing multiple-family residences.	Consistent. The Project includes development of single-family homes and is an extension of the existing single-family residential neighborhood in the existing El Sereno neighborhood within the Northeast Los Angeles Community Plan area.
1-2.1 Designate specific areas to provide for adequate residential development to accommodate anticipated increases in population while maintaining a balance between single-family and multiple-family uses.	Consistent. The Project includes development of single-family homes and is an extension of the existing single-family residential neighborhood in the existing El Sereno neighborhood within the Northeast Los Angeles Community Plan area. As discussed in response to Checklist Question 13a, the Project would provide housing that is consistent with housing needs called out in the RHNA.
1-3.1 Protect the quality and scale of the residential environment through attention to the appearance of communities, including attention to building and site design.	Consistent. The Project would comply with all of the City's applicable Design Guidelines and Standards for residential development.
1-5.1 Limit development according to the adequacy of the existing and assured street circulation system within the Plan Area and surrounding areas.	Consistent. As discussed in response to Checklist Question 16a, the roadway infrastructure serving the Project site would be adequate to accommodate the Project, and the Project would not result in any significant traffic impacts.
1-5.2 Ensure the availability of paved streets, adequate sewers, drainage facilities, fire protection services and facilities, and other emergency services and public utilities to support development in hillside areas.	Consistent. As discussed in response to Checklist Question 16a, the roadway infrastructure serving the Project site would be adequate to accommodate the Project, and the Project would not result in any significant traffic impacts. As discussed in response to Checklist Issue 14. Public Services, existing fire and police protection services

Table IV-17
Project Consistency with Applicable Policies of the
Northeast Los Angeles Community Plan

Policy	Project Consistency
	would be adequate to serve the Project, and no significant impacts related to fire and police protection would occur.
1-5.3 Consider the steepness of the topography and the geologic stability in any proposal for development within the Plan area.	Consistent. The Project site is located in a hillside area. The Project would be designed and constructed in accordance with the recommendations of a Final Geotechnical Report and the City's Building Code, as required by the City.
1-5.4 Require that any proposed development be designed to enhance and be compatible with adjacent development.	Consistent. The Project includes development of single-family homes and is an extension of the existing single-family residential neighborhood in the existing El Sereno neighborhood within the Northeast Los Angeles Community Plan area. The Project would be required to be designed and constructed in accordance with all of the City's applicable design standards, and the Citywide Hillside Ordinance.
Open Space	
4-1.1 Encourage the retention of passive and visual open space which provides a balance to the urban development of the Plan Area.	Consistent. The Project includes development of single-family homes and is an extension of the existing single-family residential neighborhood in the existing El Sereno neighborhood within the Northeast Los Angeles Community Plan area. As discussed in response to Checklist Question 1a, the Project would not affect any scenic views.
Park and Recreational Facilities	
5-1.1 Preserve the existing recreational facilities and park space.	Consistent. The Project would not affect any designated recreational facilities or park space.
Police Protection	
8-1.1 Coordinate with the Police Department as part of the review of significant development projects and General Plan Amendments affecting land use to determine the impact on service demands.	Consistent. As part of preparation of this MND, the Los Angeles Police Department (LAPD) was consulted to help determine what demand the Project could have on LAPD services and any mitigation measures that could be implemented to reduce Project demand. (Refer to response to Checklist Question 14ii.)

Table IV-17
Project Consistency with Applicable Policies of the
Northeast Los Angeles Community Plan

Policy	Project Consistency
8-1.3 Encourage design of building and facilities in accordance with principles that minimize opportunities for crime and enhance personal safety.	Consistent. The Project developer would be required to design and construct the Project in accordance with "Design Out Crime Guidelines: Crime Prevention Through Environmental Design," published by the LAPD.
Transportation	
10-1.1 Maintain Levels of Service for streets and highways not to exceed LOS "D" for secondary arterials, collector streets, and local streets; not to exceed LOS "E" on Major Highways or in the community's major business districts.	Consistent. A traffic impact analysis was prepared for the Project (refer to response to Checklist Question 16a.) The analysis concluded that the existing transportation facilities are adequate to accommodate the Project's traffic, and no significant impacts related to traffic would occur.
13-1.4 New development projects should be designed to minimize disturbance to existing flow with proper ingress and egress to parking.	Consistent. The Project would include adequate driveway access to prevent auto queuing.
<i>Source: Northeast Los Angeles Community Plan.</i>	

Zoning Code

The Project site is currently zoned [Q]R1-1D (Qualified Condition, One-Family Zone, Height District 1) and [Q]RD6-1D (Qualified Condition, Restricted Density Multiple Dwelling Zone, Height District 1). To allow for development of the Project, the Project Applicant is requesting the following zoning-related discretionary approvals:

- **Vesting Tract Map (VTT) for Small Lot Purposes per LAMC Section 17.03** – Request is for the Advisory Agency to approve a Vesting Tentative Tract Map to create 42 single-family lots in accordance with the Small Lot Subdivision Ordinance No. 176,354 in the Northeast Los Angeles Community Plan.
- **Tree Removal Permit** – Request is authorization from the Board of Public Works or the Advisory Agency for the removal of a maximum of 39 protected trees, pursuant to LAMC Section 17.05.
- **Vesting Zone Change (ZC) per LAMC Section 12.32** – Request to permit a change of zone from [Q]R1-1D and [Q] RD6-1D to (T)(Q)RD5-1D.

- The proposed Q Conditions for the Project are as follows:
 - 1) **Infrastructure.** Construction materials and equipment shall not be permitted to be stored in the public right-of-way in any manner that reduces roadway clearance to less than 20-feet in width. Storage of construction materials and equipment on public property requires a street use permit from the Bureau of Street Services.
 - 2) **Building Design.** The design of the project shall conform to the approved Project Plans. Specifically, they shall provide for and adhere to
 - a. Building materials match architectural style of new development.
 - b. Architectural design elements of the front and rear building elevations including articulation of facades, modulations of walls, shape, type details and the location of windows, doors, columns, balconies and garage doors vary from the adjacent/abutting buildings.
 - c. No building or structure shall exceed 30 feet in height from adjacent finished grade, measured as the vertical distance from the adjacent finished grade of the site to an imaginary plane located above and parallel to the finished grade; except that when the roof of the uppermost story of a building or structure or portion of the building or structure has a slope of less than 25 percent, the maximum height shall be 26 feet above adjacent finished grade, with the exception of the stair enclosures to the roof decks that may exceed 26 feet in height.
 - d. The finished floor elevation directly above an exposed underfloor area shall be limited to 6 feet above finished grade. (This does not apply to cantilevers above the garages.)
 - e. Attached decks shall be limited such that no portion of the walking surface of a deck with visible underpinnings shall exceed a height of 6' above grade and decks shall be integrated into the architecture of the house, and not appear as an add-on to the primary building mass.
 - f. Lots 39 thru 42 shall be oriented so that their front entry is facing Lombardy Boulevard.
 - g. External security grilles or permanently affixed security bars attached to windows or doors shall be prohibited
 - 3) **Floor Area.** Floor Area shall be limited to a maximum of 0.75 FAR. (The first 400sf of covered parking area shall not be counted towards the maximum Floor Area.)

- 4) **Parking.** Each lot shall be provided two (2) covered parking stalls. The overall development shall provide guest parking at a ratio of 0.25 stalls per unit.
- 5) **Landscape.** The landscape design of the project shall conform to the approved Project Plans. Specifically, they shall provide for and adhere to:

- a. Landscaping palette for required landscape plans shall be comprised of drought tolerant and/or native plant material that is fire retardant and controls erosion.
- b. Retaining walls and building understory areas shall be fully screened with plantings in a reasonable amount of time, as shown on approved landscape plan

Landscape plans must be submitted to Bureau of Street Services Urban Forestry Division prior to DCP clearance. Upon satisfaction of the requirements set forth under LAMC Ordinance No. 177,404 (Protected Trees) deemed necessary by the Urban Forestry Division, an approval letter will be issued by the Urban Forestry Division and submitted with new development filings as part of submission packages.

- c. A signed "Certified Arborist's or Licensed Landscape Architect's Certificate of Compliance" must be filed with the Department of Building & Safety prior to issuance of a "Certificate of Compliance" to ensure that landscaping plans are fully implemented
 - d. A public "entryway improvement" of signage, landscaping, and other distinctive elements shall be installed at the intersection of Eastern Avenue and Lombardy Boulevard as identified on the approved Landscape Plan. The area shall be maintained by the project's Homeowners Association.
- 6) **Retaining Walls.** The engineering and design of the project shall conform to the grading and wall plans shown on the approved Project Plans. Specifically, they shall provide for and adhere to:
- a. The overall development site shall be limited to a total of fifty-four (54) retaining walls as depicted on the approved Project Plans. (Walls less than 42" in height shall not be counted toward this overall limitation.)
 - b. Retaining walls shall not extend beyond 6.0 feet in height.
 - c. Individual walls are not limited to length and may extend beyond the proposed lot lines shown on VTT -73531-SLO so long as each future lot has no more than two (2) retaining walls/wall segments located within its bounds.

- d. Retaining walls located within the public right-of-way and required by the Bureau of Engineering – Public Works shall not count against the retaining wall limitation on this site.
 - e. All retaining walls shall provide a standard surface backdrain system and all drainage shall be conducted to the street in a non-erosive device, as required by approved Soils Report.
 - f. All retaining and garden walls shall be landscaped per the approved Landscape plan.
- 7) **Environmental.** The engineering and design of the project shall conform to the grading and wall plans that are part of the approved Project Plans. Specifically, they shall provide for and adhere to:
- a. A Geotechnical Investigation Report that evaluates the proposed project's soil and grading shall be submitted to the LADBS Grading Division for review. An approved Soils & Grading report letter from LADBS - Grading Division shall be required prior to approval of a grading, foundation or building permit.
 - b. All new graded slopes shall be no steeper than 2:1 (rise:run), except when the Grading Division has determined that slopes may exceed 2: 1 as part of an approved Soils Report.
 - c. Grading shall be limited to a maximum of 82,000 cubic yards cut and 5,000 fill. Export of soils from the project site shall be limited to 78,000 cubic yards.
 - d. To meet LID requirements, cisterns shall be installed to collect all run-off required to meet water quality standards. Rainwater collected in the cisterns shall be used for on-site landscape irrigation.
- 8) **Truck Traffic Restricted Hours.** Truck traffic directed to the project site for the purpose of delivering construction materials or construction-machinery shall be limited to the hours beginning at 8:00 AM and ending at 4:00 PM, Monday through Saturday. No truck deliveries shall occur outside of that time period. No truck queuing related to such deliveries to the project site shall occur on any local or collector street within the project vicinity outside of that time period.
- 9) **Construction Workers.** Construction workers shall be encouraged to carpool or vanpool to the Project Site during construction of the Proposed Project to reduce vehicle trips. All construction vehicles shall park on-site during construction.

- 10) **Security.** Construction fencing/canopies shall be built on Eastern Avenue and Lombardy Boulevard around the site to minimize trespassing, vandalism, short-cut attractions and attractive nuisances. Construction fencing with wind-screen shall be constructed along the eastern and southern property lines adjacent to R1-1D zoned properties.
 - 11) **24-Hour Contact.** A 24-hour “hotline” shall be required to receive and forward information relayed by adjacent homeowners and stakeholders to site representatives for immediate dissemination to the project team. The applicant shall be required to respond within 24 hours of any complaint received on this hotline.
- **Zoning Administrator’s Determination (ZAD) per LAMC Section 12.24 X.26** – Request is to allow 54 walls varying in height from 3.5 feet to 6.0 feet in lieu of the maximum of 2 10-foot retaining walls otherwise required in LAMC Section 12.21 C.8(a).
 - **Haul Route** approval by the Board of Building & Safety Commissioners or Advisory Agency.

The existing zoning for the Project site allows for development of single-family residential homes, similar to what is proposed. The Project would meet all zoning requirements related to building height, setbacks, and parking. As such, the Project would not conflict with the zoning code. Therefore, Project impacts related to zoning inconsistency would be less than significant.

c) Would the project conflict with any applicable habitat conservation plan or natural community conservation plan?

No Impact. The Project site is not subject to any applicable habitat conservation plan or natural community conservation plan. Therefore, the Project would not conflict with any applicable habitat conservation plan or natural community conservation plan.

Cumulative Impacts

As discussed previously, the Project would not result in any inconsistencies with any of the applicable plans, policies, or regulations associated with development of the Project site. The City would assess the consistency of the related projects with all applicable plans, policies, and regulations associated with those sites, individually. Regardless of any potentially inconsistencies the related projects may result in, because the Project would not result in any inconsistencies, the Project would not have the potential to contribute to any cumulative inconsistency impacts.

11. MINERAL RESOURCES

- a) **Would the project result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?**

No Impact. The Project site is located in a fairly urbanized part of the City. There are no known mineral resources on the Project site or in the vicinity. Thus, the Project would not result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state. Therefore, no impacts related to issue would occur.

- b) **Would the project result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?**

No Impact. The Project site is located in a fairly urbanized part of the City. The Project site is not identified as a mineral resource recovery site. Thus, the Project would not result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan. Therefore, no impacts related to issue would occur.

Cumulative Impacts

As discussed previously, the Project would not result in any impacts related to mineral resources. Regardless to what degree the related projects could result in impacts related to mineral resources, because the Project would not result in any impacts related to mineral resources, the Project would not have the potential to contribute to any cumulative impacts.

12. NOISE

- a) **Would the project result in exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?**

Less Than Significant With Mitigation Incorporated. The information below is based on a noise modeling results prepared for the Project by DKA Planning (refer to Appendix G).

Characteristics of Sound

Sound is technically described in terms of the loudness (amplitude) and frequency (pitch) of the sound. The standard unit of measurement for sound is the decibel (dB). The human ear is not equally sensitive to sound at all frequencies. The "A-weighted scale," abbreviated dBA, reflects the normal hearing sensitivity range of the human ear. On this scale, the range of human hearing extends from approximately 3 to 140 dBA. Table IV-18 provides examples of A-weighted noise levels from common sources.

Table IV-18
A-Weighted Decibel Scale

Typical A-Weighted Sound Levels	Sound Level (dBA, L_{eq})
Threshold of Pain	140
Jet Takeoff at 100 Meters	125
Jackhammer at 15 Meters	95
Heavy Diesel Truck at 15 Meters	85
Conversation at 1 Meter	60
Soft Whisper at 2 Meters	35
<i>Source: United States Occupational Safety & Health Administration, Noise and Hearing Conservation Technical Manual, 1999.</i>	

Noise Definitions

Community Noise Equivalent Level (CNEL): CNEL is an average sound level during a 24-hour period. CNEL is a noise measurement scale, which accounts for noise source, distance, single event duration, single event occurrence, frequency, and time of day. Human reaction to sound between 7:00 p.m. and 10:00 p.m. is as if the sound were actually 5 dBA higher than if it occurred from 7:00 a.m. to 7:00 p.m. From 10:00 p.m. to 7:00 a.m., humans perceive sound as if it were 10 dBA higher due to the lower background level. Hence, the CNEL is obtained by adding an additional 5 dBA to sound levels in the evening from 7:00 p.m. to 10:00 p.m. and 10 dBA to sound levels in the night from 10:00 p.m. to 7:00 a.m. Because CNEL accounts for human sensitivity to sound, the CNEL 24-hour figure is always a higher number than the actual 24-hour average.

Equivalent Noise Level (L_{eq}): L_{eq} is the average noise level on an energy basis for any specific time period. The L_{eq} for one hour is the energy average noise level during the hour. The average noise level is based on the energy content (acoustic energy) of the sound. L_{eq} can be thought of as the level of a continuous noise that has the same energy content as the fluctuating noise level. The equivalent noise level is expressed in units of dBA.

Effects of Noise

The degree to which noise can impact the environment ranges from levels that interfere with speech and sleep to levels that cause adverse health effects. Human response to noise is subjective and can vary from person to person. Factors that influence individual response include the intensity, frequency, and pattern of noise, the amount of background noise present before the intruding noise, and the nature of work or human activity that is exposed to the noise source.

Audible Noise Changes

Small perceptible changes in sound level for a person with normal hearing sensitivity is approximately 3 dBA. A change of at least 5 dBA would be noticeable and would likely cause some community reaction. A 10-dBA increase is heard as a doubling in loudness and would cause a community response.

Noise levels decrease as the distance from the noise source to the receiver increases. Noise generated by a stationary noise source, or “point source,” will decrease by approximately 6 dBA over hard surfaces (e.g., reflective surfaces such as parking lots or smooth bodies of water) and 7.5 dBA over soft surfaces (e.g., absorptive surfaces such as soft dirt, grass, or scattered bushes and trees) for each doubling of the distance. For example, if a noise source produces a noise level of 89 dBA at a reference distance of 50 feet, then the noise level would be 83 dBA at a distance of 100 feet from the noise source, 77 dBA at a distance of 200 feet, and so on. Noise generated by a mobile source will decrease by approximately 3 dBA over hard surfaces and 4.5 dBA over soft surfaces for each doubling of the distance.

Noise is most audible when traveling by direct line-of-sight.⁶⁶ Barriers, such as walls or buildings that break the line-of-sight between the source and the receiver can greatly reduce noise levels from the source since sound can only reach the receiver by diffraction. Sound barriers can reduce sound levels by up to 20 dBA. However, if a barrier is not high or long enough to break the line-of-sight from the source to the receiver, its effectiveness is greatly reduced.

REGULATORY SETTING

Federal

Noise Standards

There are no federal noise standards that directly regulate environmental noise related to the construction or operation of the Project, which is a private development in the City. With regard to noise exposure and workers, the Office of Safety and Health Administration (OSHA) regulations safeguard the hearing of workers exposed to occupational noise.

State

Noise Standards

The California Department of Health Services (the “DHS”) has established guidelines for evaluating the compatibility of various land uses as a function of community noise exposure. These guidelines for land use and noise exposure compatibility are shown on Table IV-19. In addition, Section 65302(f) of the

⁶⁶ *Line-of-sight is a visual path between the noise source and the noise receptor.*

California Government Code requires each county and city in the state to prepare and adopt a comprehensive long-range general plan for its physical development, with Section 65302(g) requiring a noise element to be included in the general plan. The noise element must: (1) identify and appraise noise problems in the community; (2) recognize Office of Noise Control guidelines; and (3) analyze and quantify current and projected noise levels.

Table IV-19
Community Noise Exposure (CNEL)

Land Use	Normally Acceptable ^a	Conditionally Acceptable ^b	Normally Unacceptable ^c	Clearly Unacceptable ^d
Single-family, Duplex, Mobile Homes	50 - 60	55 - 70	70 - 75	above 75
Multi-Family Homes	50 - 65	60 - 70	70 - 75	above 75
Schools, Libraries, Churches, Hospitals, Nursing Homes	50 - 70	60 - 70	70 - 80	above 80
Transient Lodging – Motels, Hotels	50 - 65	60 - 70	70 - 80	above 75
Auditoriums, Concert Halls, Amphitheaters	---	50 - 70	---	above 70
Sports Arena, Outdoor Spectator Sports	---	50 - 75	---	above 75
Playgrounds, Neighborhood Parks	50 - 70	---	67 - 75	above 75
Golf Courses, Riding Stables, Water Recreation, Cemeteries	50 - 75	---	70 - 80	above 80
Office Buildings, Business and Professional Commercial	50 - 70	67 - 77	above 75	---
Industrial, Manufacturing, Utilities, Agriculture	50 - 75	70 - 80	above 75	---

^a *Normally Acceptable:* Specified land use is satisfactory, based upon the assumption that any buildings involved are of normal conventional construction without any special noise insulation requirements.

^b *Conditionally Acceptable:* New construction or development should be undertaken only after a detailed analysis of the noise reduction requirements is made and needed noise insulation features included in the design. Conventional construction, but with closed windows and fresh air supply systems or air conditioning will normally suffice.

^c *Normally Unacceptable:* New construction or development should generally be discouraged. If new construction or development does proceed, a detailed analysis of the noise reduction requirements must be made and needed noise insulation features included in the design.

^d *Clearly Unacceptable:* New construction or development should generally not be undertaken.

Source: Office of Planning and Research, State of California General Plan Guidelines, October 2003 (in coordination with the California Department of Health Services); City of Los Angeles, General Plan Noise Element, adopted February 1999.

City

The LAMC provides two types of noise standards that are relevant to this analysis: 1) construction noise standards, and 2) general noise ordinance standards. The construction noise standards apply only to construction activities, while the general noise ordinance standards apply to noise generated by land use activities.

Construction Noise Standards

LAMC Section 41.40 regulates noise due to construction work. LAMC Section 41.40 prohibits the use of any “power driven drill, riveting machine, excavator or any other machine, tool, device or equipment which makes loud noises to the disturbance of persons occupying sleeping quarters in any dwelling hotel or apartment or other place of residence” between the hours of 9:00 PM and 7:00 AM. Section 41.40 further states that “the operation, repair or servicing of construction equipment and the job-site delivering of construction materials in such areas shall be prohibited” during the hours of 9:00 PM and 7:00 AM. LAMC Section 41.40 also prohibits any construction work, including the operation, repair, or servicing of construction equipment and the job-site delivering of construction materials, within 500 feet of residential buildings before 8:00 AM or after 6:00 PM on Saturday or national holidays or at any time on Sunday. Within the permitted construction times and distances, there are no noise limits. Construction noise intruding onto property zoned for manufacturing or industrial uses is exempted from the LAMC Section 41.40 standards.

LAMC Section 112.05 states that between the hours of 7:00 AM and 10:00 PM, in any residential zone of the City or within 500 feet thereof, no person shall operate or cause to be operated any powered equipment or powered hand tool that produces a maximum noise level exceeding 75 dB(A) at a distance of 50 feet. This limit applies to construction equipment, including crawler-tractors, dozers, rotary drills and augers, loaders, power shovels, cranes, derricks, motor graders, paving machines, off-highway trucks, ditchers, trenchers, compactors, scrapers, wagons, pavement breakers, compressors, and pneumatic or other powered equipment. This limit shall not apply where compliance is technically infeasible. The burden of proving that compliance is technically infeasible shall be on the person or persons charged with any violation of this section. Technical infeasibility shall mean that the noise limit cannot be complied with despite the use of mufflers, shields, sound barriers and/or other noise reduction devices or techniques during the operation of the equipment.

General Noise Ordinance Standards

LAMC Chapter XI, “Noise Regulation,” regulates noise from non-transportation noise sources such as commercial or industrial operations, mechanical equipment or residential activities. Although these regulations do not apply to vehicles operating on public rights-of-way, the regulations do apply to noise generated by vehicles on private property, such as truck operations at commercial or industrial facilities. The exact noise standards vary depending on the type of noise source, but the allowable noise levels are generally determined relative to the existing ambient noise levels at the affected location. LAMC Section

111.01 (a) defines the ambient noise as “the composite of noise from all sources near and far in a given environment, exclusive of occasional and transient intrusive noise sources and of the particular noise source or sources to be measured. Ambient noise shall be averaged over a period of at least 15 minutes...” LAMC Section 111.03 provides minimum ambient noise levels for various land uses, as described on Table IV-20. In the event that the actual measured ambient level at a subject location is lower than that provided in the table, the level in the table shall be assumed.

Table IV-20
City of Los Angeles Minimum Ambient Noise Levels

Zone	Allowable Average Noise Level (L_{eq})	
	Daytime (7 am – 10 pm)	Nighttime (10 pm – 7 am)
A1, A2, RA, RE, RS, RD, RW1, RW2, R1, R2, R3, R4, and R5	50 dB(A)	40 dB(A)
P, PB, CR, C1, C1.5, C2, C4, C5, and CM	60 dB(A)	55 dB(A)
M1, MR1, and MR2	60 dB(A)	55 dB(A)
M2 and M3	65 dB(A)	65 dB(A)
Source: LAMC		

At the boundary line between two zones, the allowable noise level of the quieter zone shall be used. The allowable noise levels are then adjusted if certain conditions apply to the alleged offensive noise, as follows:

- For steady tone noise with an audible fundamental frequency or overtones (except for noise emanating from any electrical transformer or gas metering and pressure control equipment existing and installed prior to September 8, 1986) – reduce allowable noise level by 5 dB(A).
- For repeated impulsive noise – reduce allowable noise level by 5 dB(A).
- For noise occurring less than 15 minutes in any period of 60 consecutive minutes between the hours of 7:00 AM and 10:00 PM – increase allowable noise level by 5 dB(A).

The City’s noise ordinance is not explicit in defining the length of time over which an average noise level should be assessed. However, based on the noted reference to “60 consecutive minutes,” above, it is concluded that the one-hour L_{eq} metric should be used.

Regarding the location at which the noise measurements should be taken, the LAMC states that “except when impractical, the microphone shall be located four to five feet above the ground and ten feet or more from the nearest reflective surface. However, in those cases where another elevation is deemed appropriated, the latter shall be utilized.”

LAMC Section 112.02 addresses noise from air conditioning, refrigeration, heating, pumping, and filtering equipment. The section states that such equipment may not generate noise that would exceed the ambient noise level at any adjacent property by more than 5 dB(A).

LAMC Section 114.02 addresses noise from motor driven vehicles (the LAMC only addresses vehicles on private property and does not address vehicles on public highways). The section states that such vehicles may not generate noise that would exceed the ambient noise level at any occupied residential property by more than 5 dB(A).

LAMC Section 114.03 states that “It shall be unlawful for any person, between the hours of 10:00 PM and 7:00 AM of the following day, to load or unload any vehicle, or operate any dollies, carts, forklifts, or other wheeled equipment, which causes any impulsive sound, raucous or unnecessary noise within 200 feet of any residential building.”

Project Impacts

Construction Noise

During demolition, construction, ground clearing, grading, structural, and other noise-generating activities would occur at the Project site between the hours of 7:00 a.m. and 9:00 p.m. in accordance with the LAMC. Table IV-21 summarizes projected noise levels at nearby sensitive receptors during construction. Land uses on the properties surrounding the Project site include an elementary school, and single- and multi-family residential buildings. There are a number of nearby sensitive receptors to the Project site, including the following:

- Farmdale Elementary School, 90 feet northwest of the Project site
- 2635 Lombardy Avenue, single family residence about 15 feet east of Project site
- 2543 Mallory Street, single family residence about 15 feet east of Project site
- 2518 Eastern Avenue, multi-family residences 15 feet south of the Project site
- Klamath Street residences, directly south of the Project site

To ascertain current ambient noise levels at nearby receptors, DKA Planning took short-term, 15-minute noise readings on April 10, 2015 using a Quest Technologies SoundPro DL Sound Level Meter.⁶⁷ Noise

⁶⁷ *The SoundPro meter complies with the American National Standards Institute (ANSI) and International Electrothnical Commission (IEC) for general environmental noise measurement instrumentation. The meter was equipped with an omni-directional microphone, calibrated before the day's measurements, and set at approximately five feet above the ground. Weather conditions were clear with negligible wind.*

measurements were taken at the first four locations near the Project site. Predominant noise was caused by motor vehicles traveling on adjacent roadways, including Eastern and Lombardy Avenues, including bus stops at the intersection of these two arterials. As shown on Table IV-21, ambient noise levels ranged from 61.0 dBA L_{eq} at the residence at 2543 Mallory Street to 71.6 dBA L_{eq} at Farmdale Elementary School.

Table IV-21
Construction Noise Levels - Unmitigated

Sensitive Receptor	Distance from Site (feet)	Maximum Construction Noise Level (dBA)	Existing Ambient (dBA, L_{eq})	New Ambient (dBA, L_{eq})	Increase
Residence, 2543 Mallory Street	15	78.5	61.0	78.6	17.6
Residence, 2635 Lombardy Avenue	15	81.5	62.8	81.6	18.8
Residences, 2518 Eastern Avenue	15	81.5	69.7	81.8	12.1
Farmdale Elementary School	90	76.4	71.6	77.6	6.0

Source: DKA Planning, 2015.

Construction activities would generate noise from construction activities that would vary over the 24 months of activity on- and off-site, and would include on-site equipment such as scrapers, tractors, loaders and smaller equipment such as saws, hammers, and pneumatic tools associated with the Project's construction. There would be secondary noise from construction worker vehicles and vendor deliveries. Given the ambient conditions in the neighborhood and the proximity of the nearby receptors, significant noise impacts could occur at all five monitoring locations during construction of the Project.

- Noise levels of up to 81.8 dBA are projected at the residences at 2518 Eastern Avenue, an increase of 12.1 dBA. This elevated noise level would exceed the 75 dBA limit established in the LAMC for construction machinery at 50 feet. These would also exceed the 5 dB noise increase considered to be a noise violation by the LAMC.
- Noise levels of up to 81.6 dBA are projected at the residence at 2635 Lombardy Avenue, an increase of 18.8 dBA. This elevated noise level would exceed the 75 dBA limit established in the LAMC for construction machinery at 50 feet. These would also exceed the 5 dB noise increase considered to be a noise violation by the LAMC.
- Noise levels of up to 78.6 dBA are projected at the residence at 2543 Mallory Street, an increase of 17.6 dBA. This elevated noise level would exceed the 75 dBA limit established in the LAMC for construction machinery at 50 feet. This would also exceed the 5 dB noise increase considered to be a noise violation by the LAMC.
- Noise levels of up to 77.6 dBA are projected at Farmdale Elementary School, an increase of 6.0 dBA. This elevated noise level would exceed the 75 dBA limit established in the LAMC for

construction machinery at 50 feet. This would also exceed the 5 dB noise increase considered to be a noise violation by the LAMC.

These on-site construction-related noise impacts would be significant. However, implementation of Mitigation Measures 12-1 through 12-6 would reduce the Project's construction noise impact to less than significant (refer to Table IV-24 shown after the list of Noise Mitigation Measures).

With regard to off-site construction-related noise impacts, up to 7,800 haul truck trips are expected to remove up to 78,000 cubic yards of cut materials from the Project site, conservatively assuming 10 cubic yards of soil capacity per haul truck. This cut material could be transported 20 miles to nearby landfills by ten-wheeled heavy-duty trucks. This would equate to an average of approximately 87 haul trips per day over a three-month grading period. While such vehicle activity would marginally increase ambient noise levels along local roadways, this is not expected to significantly increase ambient noise levels by 5 dBA at sensitive receptors for two reasons. First, this level of haul activity would average four haul trips per hour onto local streets, which would not produce sustained increases in noise levels over an hour or any other monitoring period. As noted in the City's "L.A. CEQA Thresholds Guide," a 3 dBA increase in roadway noise levels requires an approximate doubling of roadway traffic volume, assuming travel speed and fleet mix remain constant. Second, the Project site is immediately adjacent to two freeways (i.e., San Bernardino and Long Beach freeways) and allows immediate access for haul trucks that would avoid travel on local roads with sensitive receptors. This noise impact would be significant. However, implementation of Mitigation Measure 12-7 would reduce the Project's construction noise impact to less than significant by ensuring the haul truck's immediate ingress onto either of the two adjacent freeways.

Operational Noise

During Project operations, the development would produce both direct noise impacts on the site from residential-related activities, as well as indirect noise impacts from vehicles traveling on local roads to access the site. The direct impacts would include stationary noises from sources associated with building operations, such as heating, ventilation, and air conditioning (HVAC) systems.

Section 41.40 and Chapter XI, Articles 1 through 6, of the LAMC requires that noise generated by mechanical equipment not exceed 5 dBA above ambient noise levels at adjacent property lines. Large ground level heating, ventilation, and HVAC systems typically generate noise levels between 50 and 65 dBA at 50 feet.⁶⁸ Roof-top mounted equipment typically produces noise levels of up to approximately 56 dBA at 50 feet. Based on the distance from the Project site to nearby receptors, the ambient noise levels, and the relatively quiet operation of HVAC systems, there would not increase in ambient noise levels from these on-site noise sources. Therefore, noise impacts associated with stationary noise would be less than significant.

⁶⁸ Los Angeles Department of City Planning, *San Pedro Community Plan Draft EIR*, August 2012.

The majority of operational noise impacts would be from indirect noise impacts associated with the 400 net new vehicle trips each weekday.⁶⁹ During the peak morning hour, the Project would add 32 new vehicle trips to local roadways and 42 trips in the peak evening hour, an average of up to 0.75 vehicle trip per minute. This increased vehicle traffic would result in inaudible increases in roadway noise. As noted in the City's "L.A. CEQA Thresholds Guide," a 3 dBA increase in roadway noise levels requires an approximate doubling of roadway traffic volume, assuming travel speed and fleet mix remain constant.

As a result, mobile noise generated by the Project would not cause the ambient noise level measured at the property lines of adjacent uses along affected roadways to rise to the "normally unacceptable" or "clearly unacceptable" category as defined by the 2003 California General Plan Guidelines or result in any 5 dBA or more increase in noise level. As a result, noise impacts associated with these inaudible, off-site vehicular noise would be less than significant.

b) Would the project result in exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?

Less Than Significant Impact. The information below is based on a noise modeling results prepared for the Project by DKA Planning (refer to Appendix G).

Characteristics of Vibration

Vibration is an oscillatory motion through a solid medium in which the motion's amplitude can be described in terms of displacement, velocity, or acceleration. Unlike noise, vibration is not a common environmental problem. It is unusual for vibration from sources such as buses and trucks to be perceptible. Common sources of vibration include trains, buses, and some construction activities.

Vibration Definitions

There are several different methods that are used to quantify vibration. The peak particle velocity (PPV) is defined as the maximum instantaneous peak of the vibration signal. The PPV is most frequently used to describe vibration impacts to buildings and is usually measured in inches per second. The root mean square (RMS) amplitude is most frequently used to describe the effect of vibration on the human body. The RMS amplitude is defined as the average of the squared amplitude of the signal. Decibel notation (VdB) is commonly used to measure RMS. The decibel notation acts to compress the range of numbers required to describe vibration.⁷⁰

⁶⁹ KOA Corporation, *Trip Generation Review – Proposed Project at 2520-2608 North Eastern Avenue, El Sereno*; March 19, 2015.

⁷⁰ Federal Transit Administration, *Transit Noise and Vibration Impact Assessment*, May 2006.

Effects of Vibration

High levels of vibration may cause physical personal injury or damage to buildings. However, ground-borne vibration levels rarely affect human health. Instead, most people consider ground-borne vibration to be an annoyance that may affect concentration or disturb sleep. In addition, high levels of ground-borne vibration may damage fragile buildings or interfere with equipment that is highly sensitive to ground-borne vibration.

Perceptible Vibration Changes

Unlike noise, ground-borne vibration is not an environmental issue that most people experience every day. The background vibration velocity level in residential areas is usually 50 RMS or lower, well below the threshold of perception for humans, which is around 65 RMS.⁷¹ Most perceptible indoor vibration is caused by sources within buildings, such as movement of people or slamming of doors. Typical outdoor sources of ground-borne vibration are construction equipment, trains, and traffic on rough roads. If the roadway is smooth, the vibration from traffic is typically not perceptible.

Applicable Regulations

To counter the effects of ground-borne vibration, the Federal Transit Administration (FTA) has published guidance relative to vibration impacts. According to the FTA, non-engineered timber and mason buildings can be exposed to ground-borne vibration levels of 0.2 inches per second without experiencing structural damage, while reinforced-concrete, steel, or timber buildings can be exposed to ground-borne vibration levels of 0.5 inches per second.⁷²

In terms of construction-related impacts on buildings, the City has not adopted policies or guidelines relative to groundborne vibration. While the Los Angeles County Code (LACC Section 12.08.350) states a presumed perception threshold of 0.01 inch per second RMS, this threshold applies to groundborne vibrations from long-term operational activities, not construction. Consequently, as both the City and the County of Los Angeles do not have a significance threshold to assess vibration impacts during construction, the FTA and California Department of Transportation's (Caltrans) adopted vibration standards for buildings are used to evaluate potential impacts related to Project construction. Based on these standards, impacts relative to groundborne vibration would be considered significant if the following were to occur:

⁷¹ *Ibid.*

⁷² *Ibid.*

- Project construction activities would cause a PPV groundborne vibration level to exceed 0.5 inches per second at any off-site reinforced-concrete, steel, or timber structure;
- Project construction activities would cause a PPV groundborne vibration level to exceed 0.2 inches per second at any non-engineered timber and masonry buildings (i.e., “fragile” buildings);⁷³ and
- Project construction activities would cause a PPV ground-borne vibration level to exceed 0.12 inches per second at any building that is extremely susceptible to vibration damage (i.e., “extremely fragile” buildings).⁷⁴

Table IV-22 identifies PPV and RMS velocity (in VdB) levels for the types of off-road and on-road equipment that could operate at the Project site during construction.

Table IV-22
Vibration Source Levels for Construction Equipment

Equipment	Approximate PPV (in/sec)					Approximate RMS (VdB)				
	25 Feet	50 Feet	60 Feet	75 Feet	100 Feet	25 Feet	50 Feet	60 Feet	75 Feet	100 Feet
Large Bulldozer	0.089	0.031	0.024	0.017	0.011	87	78	76	73	69
Caisson Drilling	0.089	0.031	0.024	0.017	0.011	87	78	76	73	69
Loaded Trucks	0.076	0.027	0.020	0.015	0.010	86	77	75	72	68
Jackhammer	0.035	0.012	0.009	0.007	0.004	79	70	68	65	61
Small Bulldozer	0.003	0.001	0.0008	0.0006	0.0004	58	49	47	44	40
<i>Source: Federal Transit Administration 2006</i>										

Project Vibration Impacts

As shown on Table IV-22, vibration velocities could range from 0.003 to 0.089 inch/sec PPV at 25 feet from the source activity, with corresponding vibration levels ranging from 58 VdB to 87 VdB at 25 feet from the source activity, depending on the type of construction equipment in use.

Groundborne vibration would be generated by a number of construction activities. Vibration velocities projected to occur at the nearest off-site sensitive receptor would produce up to a 0.191 inches/second PPV at the three residential sites adjacent to the Project site that were analyzed. This PPV is below the 0.2 inches/second that are considered potentially harmful levels of vibration for a non-engineered timber

⁷³ *Ibid.*

⁷⁴ *Ibid.*

and masonry building. Other potential types of construction equipment would produce less vibration and have lesser potential impacts on neighboring sensitive receptors. As shown on Table IV-23, the peak particle velocity and vibration levels that would occur at these on- and off-site sensitive uses during construction would be less than the thresholds associated with building damage. Therefore, construction-related vibration impacts would be less than significant.

Table IV-23
Vibration Levels at Off-Site Sensitive Uses from Project Construction

Sensitive Uses Offsite	Distance to Project Site (ft.)	Estimated PPV (in/sec) ^a	Estimated Vibration Levels (VdB) ^b
Residence, 2543 Mallory Street	15	0.191	0.2
Residence, 2635 Lombardy Avenue	15	0.191	0.2
Residences, 2518 Eastern Avenue	15	0.191	0.2
Farmdale Elementary School	90	0.013	0.2
^a The vibration velocities at the off-site sensitive uses are determined with the following equation from the Federal Transit Administration's Transit Noise and Vibration Impact Assessment, Final Report: $PPV_{equip} = PPV_{ref} \times (25/D)^{1.5}$, where PPV_{equip} = peak particle velocity in in/sec of equipment, PPV_{ref} = reference vibration level in in/sec at 25 feet, D = distance from the equipment to the receiver.			
^b The vibration levels at the off-site sensitive uses are determined with the following equation from the Federal Transit Administration's Transit Noise and Vibration Impact Assessment, Final Report: $L_v(D) = L_v(25 \text{ ft}) - 30 \log(D/25)$, where L_v = vibration level of equipment, D = distance from the equipment to the receiver, $L_v(25 \text{ ft})$ = vibration level of equipment at 25 feet.			
Source: Source: DKA Planning 2015.			

c) Would the project result in a substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?

Less Than Significant With Mitigation Incorporated. As discussed in response to Checklist Question 12a, with mitigation, the Project would not generate a substantial permanent increase in noise in excess of City noise standards. Therefore, Project impacts related to permanent noise increase would be less than significant.

d) Would the project result in a substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?

Less Than Significant With Mitigation Incorporated. As discussed in response to Checklist Question 12a, with mitigation, the Project would not result in a substantial temporary or periodic increase in ambient noise levels in excess of City noise standards. Therefore, Project impacts related to temporary or periodic noise increase would be less than significant.

e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?

No Impact. The Project site is not located within an airport land use plan or within two miles of a public airport or public use airport. Therefore, the Project would not expose people residing or working in the Project area to excessive noise levels and no impact would occur.

f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?

No Impact. The Project site is not located in the vicinity of a private airstrip. Therefore, the Project would not expose people residing or working in the Project area to excessive noise levels and no impact would occur.

Mitigation Measures (Noise)

To ensure that the Project would not result in significant noise impacts during construction, the following mitigation measures are required (refer to Table IV-24):

- 12-1: The Project shall comply with the City of Los Angeles Building Regulations Ordinance No. 178048, which requires a construction site notice to be provided that includes the following information: job site address, permit number, name and phone number of the contractor and owner or owner's agent, hours of construction allowed by code or any discretionary approval for the site, and City telephone numbers where violations can be reported. The notice shall be posted and maintained at the construction site prior to the start of construction and displayed in a location that is readily visible to the public.
- 12-2: Two weeks prior to commencement of construction, notification shall be provided to the off-site residential and school uses within 500 feet of the Project site that discloses the construction schedule, including the types of activities and equipment that would be used throughout the duration of the construction period.
- 12-3: Temporary sound barriers, capable of achieving a sound attenuation of at least 10 dBA (e.g., construction sound wall with sound blankets), and capable of blocking the line-of-sight to the adjacent residences shall be installed as feasible.
- 12-4: Noise-generating construction equipment operated at the Project Site shall be equipped with effective state-of-the-art noise control devices, i.e., mufflers, lagging, solar power or electric plug-in on-site power generators and/or motor enclosures or other shielding equipment. All equipment shall be properly maintained to assure that no additional noise, due to worn or improperly maintained parts, would be generated.

- 12-5: All construction areas for staging and warming-up equipment shall be located as far as possible from adjacent residences.
- 12-6: Portable noise sheds for smaller, noisy equipment, such as air compressors, dewatering pumps, and generators shall be provided where feasible.
- 12-7: A haul route for exporting cut materials from the site to a nearby landfill that access the San Bernardino and/or Long Beach Freeways should minimize travel on residential streets with sensitive receptors.

Table IV-24
Construction Noise Levels - Mitigated

Sensitive Receptor	Distance from Site (feet)	Maximum Construction Noise Level (dBA)	Existing Ambient (dBA, L_{eq})	New Ambient (dBA, L_{eq})	Increase
Residence, 2543 Mallory Street	15	62.5	61.0	64.8	3.8
Residence, 2635 Lombardy Avenue	15	65.5	62.8	67.4	4.6
Residences, 2518 Eastern Avenue	15	65.5	69.7	71.1	1.4
Farmdale Elementary School	90	60.4	71.6	71.8	0.3

Source: DKA Planning, 2015.

Cumulative Impacts

None of the related projects are in close proximity to the Project site. As such, distance and intervening buildings would attenuate noise generated by construction and operational activities associated with the related projects at the Project site (and vice versa) and would not result in any significant cumulative noise impacts.

13. POPULATION AND HOUSING

a) **Would the project induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?**

Less Than Significant Impact. Due to the scale at which housing markets operate, the analysis of potential Project impacts is presented in terms of the following two principal geographic scales/zones around the Project site:

- **2010 Census Data.** The local Census Tracts in and around the Project site provide the smallest geographic measurable unit for existing population and housing.

- **City of Los Angeles.** SCAG's 2016-2040 RTP/SCS and the California Department of Finance consider the City as a separate unit from other cities and any unincorporated areas.

Regulatory Framework

Regional

Southern California Association of Governments

SCAG is the federally designated metropolitan planning organization for six Southern California counties including the County of Los Angeles. SCAG prepared, and adopted, the 1996 Regional Comprehensive Plan and Guide (RCPG), the 5th Cycle for 2014-2021 Regional Housing Needs Assessment (2014-2021 RHNA) (approved November 26, 2012), the 2008 Regional Transportation Plan (RTP), and the Regional Transportation Improvement Program (RTIP) to address regional growth and measure progress toward achieving regional planning goals and objectives. SCAG has released its 2008 Regional Comprehensive Plan (RCP), as an update to the adopted 1996 RCPG. In April 2016, SCAG adopted the 2016-2040 RTP/SCS based, in part, on data from the 2010 U.S. Census.

2008 Regional Comprehensive Plan

SCAG prepared and issued the 2008 RCP in response to the SCAG's Regional Council directive in the 2002 Strategic Plan to define solutions to interrelated housing, traffic, water, air quality, and other regional challenges.

The 2008 RCP serves as a policy framework for implementation of short-term strategies and long-term initiatives to improve regional mobility and sustainability, while also directly addressing the interrelationships between natural resource sustainability, economic prosperity, and quality of life. The 2008 RCP incorporates principles and goals of the 2004 Compass Blueprint Growth Vision, as discussed below. The 2008 RCP includes nine chapter areas: Land Use and Housing, Transportation, Air Quality, Energy, Open Space and Habitat, Water, Solid Waste, Economy, and Security and Emergency Preparedness. Each chapter is organized into three sections: goals, outcomes, and action plans.

The RCP chapters that are relevant to population and housing are the Growth Management and Housing Chapters. The purpose of the Growth Management Chapter is to present forecasts which establish the socioeconomic context for the RCPG, particularly the Regional Mobility and Air Quality Chapters. It also addresses issues related to growth and land consumption by encouraging local land use actions that could ultimately lead to the development of an urban form that will help minimize development costs, save natural resources, and enhance the quality of life in the region.

The Housing Chapter includes advisory strategies for bringing housing costs and decent shelter within reach of more households in order to support the economic health and social vitality of the region. Its goals include providing for decent and affordable housing for all people; an adequate supply and

availability of housing; housing stock maintenance and preservation; and promoting a mix of housing opportunities region wide.

Regional Housing Needs Assessment

The RHNA is a key tool for SCAG and its member governments to plan for growth. The 2014-2021 RHNA quantifies the need for housing within each jurisdiction between 2014 and 2021. Communities then plan, consider, and decide how they will address this need through the process of completing the housing elements of their general plans. The RHNA does not necessarily encourage or promote growth, but rather allows communities to anticipate growth, so that they can grow in ways that enhance quality of life, and improve access to jobs, transportation and housing, without adversely impacting the environment. The RHNA is produced periodically by SCAG, as mandated by State law, to coincide with the region's schedule for preparing housing elements. It consists of two measurements of housing need: (a) existing need; and (b) future need.

The existing need assessment is based on data from the most recent U.S. Census to measure ways in which the housing market is not meeting the needs of current residents. These variables include the number of low-income households paying more than 30 percent of their income for housing, as well as severe overcrowding.

The future need for housing is determined primarily by the forecasted growth in households in a community, based on historical growth patterns, job creation, household formation rates, and other factors to estimate how many households will be added to each community over the projection period. The housing need for new households is then adjusted to account for an ideal level of vacancy needed to promote housing choice, maintain price competition and encourage acceptable levels of housing upkeep and repair. The RHNA also accounts for units expected to be lost due to demolition, natural disaster, or conversion to non-housing uses. The sum of these factors - household growth, vacancy need and replacement need - form the "construction need" assigned to each community. The City of Los Angeles was assigned a RHNA of 82,002 units for the 2014-2021 planning period.⁷⁵ There is no process for allocating the citywide total to City subareas, such as a Community Plan Area. Finally, the RHNA considers how each jurisdiction might grow in ways that will decrease the concentration of low-income households in certain communities. The need for new housing is distributed among income groups so that each community moves closer to the regional average income distribution.

⁷⁵ City of Los Angeles General Plan Housing Element, Housing Needs Assessment, December 3, 2013.

2016-2040 Regional Transportation Plan

The 2016-2040 RTP/SCS includes a proposed growth forecast for population, household, and employment for the City of Los Angeles in 2012 and 2040:⁷⁶

- Population: 3,845,500 persons in 2012 and 4,609,400 in 2040;
- Households: 1,325,500 households in 2012 and 1,690,300 in 2040; and
- Employment: 1,696,400 jobs in 2012 and 2,169,100 in 2040.

City

City of Los Angeles General Plan

The General Plan addresses community development goals and policies relative to the distribution of land use, both public and private, including housing. The General Plan integrates citywide elements, Community Plans, and Specific Plans and gives policy direction for planning regulations and implementation programs.

General Plan Framework Element

The General Plan Framework Element (General Plan Framework or Framework Element), adopted in December 1996 (re-adopted August 2001), is a strategy for long-term growth that sets a citywide context to guide the update of the Community Plans and citywide elements. The Framework Element provides that precise determinations regarding future growth and development will be made through the Community Planning process. The Framework Element encourages future growth and development within target areas, but does not require that future development and growth be limited to target areas. The Framework Element's central housing goal is an equitable distribution of housing opportunities by type and cost accessible to all residents of the City.

The General Plan Framework focuses on providing strategies for accommodating growth by encouraging growth in a number of higher-intensity commercial and mixed-use districts, centers, boulevards and industrial districts particularly in proximity to transportation corridors and transit stations. It is intended to be flexible and provides a Long Range Land Use Diagram recommending the creation of new land use categories for targeted growth areas in various areas of the City that will contain international centers,

⁷⁶ SCAG, 2016-2040 Regional Transportation Plan/Sustainable Communities Strategy, Current Demographics and Forecast, Table 11, page 24: http://scagrtpscs.net/Documents/2016/draft/d2016RTPSCS_DemographicsGrowthForecast.pdf.

regional centers, community centers, neighborhood districts, and mixed-use boulevards based on the planning principles, goals, objectives, and policies it discusses.

General Plan Housing Element

The Housing Element of the City's General Plan identifies as its overall goal the creation of a city of livable and sustainable neighborhoods with a range of housing types and costs in mutual proximity to jobs, infrastructure and services.

On December 3, 2013, the City Council adopted the update to the Housing Element of the General Plan for the period of 2013-2021. The Housing Element provides the number of housing units each community must plan and accommodate during the 8-year period pursuant to the RHNA allocation. The Housing Element does not alter the development potential of any site in the City, nor modify land use of the Zoning Code. It also does not undermine, in any way, neighborhood planning efforts such as Community Plans, Specific Plans, or Historic Preservation Overlay Zones. While the State requires the City to evaluate and plan for the existing capacity to accommodate future projected growth, the Housing Element does not have any material effect on development patterns, nor specify areas for increased height or density.

An objective of the Housing Element is to promote an equitable distribution of affordable housing opportunities throughout the City by providing incentives to include affordable housing in residential development. The Project would further the goals and objectives of the Housing Element by providing additional housing stock.

Existing and Forecasted Population and Housing for City of Los Angeles

According to analysis by the State's Housing and Community Development Department, prior to the recent economic downturn and foreclosure crisis, California had experienced decades of undersupply of housing, contributing to significant price escalation and the affordability crisis.⁷⁷ The factors contributing to California's continuing housing supply and affordability problems include a chronic mismatch between the existing housing stock and the demand for housing by type and location; lack of sufficient housing construction to meet demand; and persistently high housing costs relative to household incomes, even with the effects of the recent national recession.

Almost all future California population and household growth will occur in metropolitan areas, and most of that will occur in southern California. According to SCAG's 2008 growth forecast, the six-county region is projected to add about 4.6 million people and about 1.6 million households between 2010 and

⁷⁷ *State of California – Business, Transportation and Housing Agency, The State of Housing in California 2012: Affordability Worsens, Supply Problems Remain, 2012.*

2035. In Los Angeles County alone, the forecast envisions about 1.7 million people and about 646,000 households between 2010 and 2035. As the largest city in the County, the City of Los Angeles will receive most of the County's future growth.

SCAG's State-approved 5th Cycle 2014-2021 RHNA assigns 82,002 units of housing production need to the City of Los Angeles for the 2013-2021 Housing Element (which actually covers a 7.5-year planning period), or an annual average of about 15,000 new dwelling units per year.⁷⁸

The Housing Element of the City's General Plan, mentioned above, notes that for over 10 years, the City has been pursuing a sustainable approach to accommodating long-range growth. This approach is established in the Framework Element of the General Plan, first adopted in 1995, which encourages sustainable growth in higher-intensity commercial and mixed-use districts, centers and boulevards, and in proximity to transit. The goals and policies of the Framework Element establish a balanced approach to growth by linking it to the land uses and infrastructure that will support the type of infill development that incurs the least economic, environmental, and social costs.

Table IV-25 lists the 2010 and 2016 population, households, and subsequent persons/housing ratio, the SCAG forecast for 2040, as well as the number and percent change.

Table IV-25
Population and Households in the City of Los Angeles

Year	Population	Households	Person/Households
2010 ¹	3,792,621	1,412,006	2.69
2016 ²	3,957,022	1,453,271	2.74
2040 ⁴	4,609,400	1,690,300	2.72
Change 2010 to 2016			
Number Changed	+164,401	+41,265	+0.05
Change 2016 to 2040			
Number Changed	+652,378	+237,029	-0.02
¹ 2010: Census data, reported 4/1/2010. ² 2016: As of January 1, 2016, Department of Finance: http://www.dof.ca.gov/Forecasting/Demographics/Estimates/E-5/ ³ 2020: Based on the adopted 2012-2035 RTP/SCS by SCAG, page 32: http://rtpscs.scag.ca.gov/Documents/2012/final/SR/2012fRTP_GrowthForecast.pdf ⁴ 2040: Based on the adopted 2016-2040 RTP/SCS by SCAG, page 24: http://scagrtpscscs.net/Documents/2016/draft/d2016RTPSCS_DemographicsGrowthForecast.pdf			

⁷⁸ City of Los Angeles General Plan Housing Element, Housing Needs Assessment, December 3, 2013.

Existing Project Site Conditions

The Project site is an infill site that is currently not developed with any structures.

Project Impacts

The Project includes of the site with a 42 single-family homes. Based on the 2016 persons-per-household rate for the City shown on Table IV-25, the Project would generate approximately 114 residents.

As shown on Table IV-26, the Project would represent a negligible percent (less than one-half of one percent) of the estimated population and housing growth in the City. Thus, the Project's residents and housing units would fall within the estimates and RHNA allocation. Additionally, by utilizing the City's Small Lot Subdivision Ordinance for efficient single family home development, the Project would help achieve a portion of the household growth forecast for the City and the Northeast Los Angeles Community Plan area, while also being consistent with regional policies to reduce urban sprawl, efficiently utilize existing infrastructure, reduce regional congestion, and improve air quality through the reduction of VMT. Thus, the Project would not substantially induce housing growth beyond forecasted levels and would meet a portion of forecasted housing demand currently forecasted for the City. Thus, the Project would not represent a substantial or significant growth as compared to projected growth. Therefore, no significant impacts related to population and housing would occur as a result of the Project.

Table IV-26
Project Estimated Comparison

Project	Comparison Amount	% of Comparison
As compared to Growth Forecast from 2016 to 2040		
114 residents	+652,378 ¹	0.017
42 units	+237,029 ¹	0.017
As compared to City's 2014-2021 Housing Element		
42 units	6,018 (Community Plan) ²	0.69
42 units	82,002 (Citywide) ³	0.05
¹ Refer to Table IV-25.		
² City of Los Angeles, Housing Element, 2013-2021, adopted December 3, 2013, Table 3.1, page 3-4.		
³ City of Los Angeles, Housing Element, 2013-2021, adopted December 3, 2013, page 3-3.		

b) Would the project displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?

No Impact. No housing exists on the Project site. Therefore, the Project would not displace any existing housing, necessitating the construction of replacement housing elsewhere, and no impacts related to this issue would occur.

c) Would the project displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?

No Impact. No people live on the Project site. Therefore, the Project would not displace any residents, necessitating the construction of replacement housing elsewhere, and no impacts related to this issue would occur.

Cumulative Impacts

Implementation of the related projects listed on Table IV-38 could result in housing and population growth. However, as discussed previously, the Project's population growth would be consistent with the anticipated growth for the Project area and in the General Plan. The Project would not create unplanned growth, and impacts related to population and housing would be less than significant. As such, regardless of whether the related projects would result in unplanned growth, the Project would not have the potential to contribute to any potential cumulative impact.

14. PUBLIC SERVICES

a) Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered government facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objective for any of the following public services:

(i) Fire protection?

Less Than Significant Impact. The Project includes development of a 42 single-family residential homes at the Project site, increasing the need for fire protection services at the Project site. Los Angeles Fire Department (LAFD) considers fire protection services for a project adequate if a project: (1) is within the maximum response distance for the land uses proposed; (2) complies with emergency access requirements; (3) complies with fire-flow requirements; and (4) complies with fire hydrant placement. Pursuant to LAMC Section 57.09.07, the maximum response distance between a low-density residential neighborhood land use and a LAFD station that houses an engine or truck company is 1.5 miles. If this distance is exceeded, all structures shall be constructed with automatic fire sprinkler systems.⁷⁹ However, projects that fall within Very High Fire Hazard Severity Zone (as is the Project) are required to install fire sprinkler systems.

⁷⁹ LAFD website: http://lafd.com/prevention/hydrants/division_9_fc.html, accessed October 21, 2014.

The Project site is served by several fire stations, as shown on Table IV-27. As stated previously, the Project is located within a Very High Fire Hazard Severity Zone. Thus, the Project would be required to be designed and constructed in accordance with the Los Angeles Fire Code and would be required to incorporate measures, including but not limited the following:

- Ignition-resistant roofing and other building materials
- Fire-Retardant-Treated Wood or noncombustible materials
- Roof coverings, valleys, and gutters
- Attic ventilation
- Eave or cornice vents
- Sprinkler systems
- Landscaping with fire-retardant plants
- Vegetation clearance

Additionally, prior to issuance of an Occupancy Permit, the Project Applicant would be required to coordinate with LAFD to ensure that the Project incorporates all appropriate fire-prevention measures. All ingress/egress associated with the Project would be designed and constructed in conformance to all applicable City Building and Safety Department and LAFD standards and requirements for design and construction. Therefore, the Project would not result in any significant impacts related to emergency access. Approximate fire-flow requirement for the Project is 2,000 gallons per minute with a 20 pounds-per-inch residual pressure. Final fire-flow demands, fire hydrant placement, and other fire protection equipment would be determined for the Project during LAFD's plan check process. Through compliance with these requirements, Project impacts related to fire protection services would be less than significant.

Table IV-27
Fire Stations Serving the Project Site

No.	Address	Distance from Project Site
1	2230 Pasadena Avenue	3.8 miles
12	5921 North Figueroa Street	3.4miles
16	2011 North Eastern Avenue	0.7 mile
47	4575 Huntington Drive South	1.2 miles
Source: http://lafd.org/fire_stations/station_results/%2A?zipcode=90065 , accessed April 15, 2015.		

Cumulative Impacts

Implementation of the related projects on Table IV-38 could result in a net increase in the number of residents and employees in the Project area and could further increase the demand for fire protection services. Cumulative development requires the LAFD to continually evaluate the need for new or physically altered facilities in order to maintain adequate service ratios. Similar to the proposed Project, the related projects would be subject to the Fire Code and other applicable regulations of the LAMC

including, but not limited to, automatic fire sprinkler systems for high-rise buildings and/or residential projects located farther than 1.5 miles from the nearest LAFD Engine or Truck Company to compensate for additional response time, and other recommendations made by the LAFD to ensure fire protection safety. Through the process of compliance, the ability of the LAFD to provide adequate facilities to accommodate future growth and maintain acceptable levels of service would be ensured. Furthermore, the increased demands for additional LAFD staffing, equipment, and facilities would be funded via existing mechanisms (e.g., property taxes and government funding) to which the Project and related projects would contribute. Therefore, cumulative impacts related to fire protection services would be less than significant.

(ii) Police protection?

Less Than Significant Impact. The Project includes development of 42 single-family residential homes at the Project site, increasing the need for police protection services at the Project site. In accordance with the City's Standard Condition of Approval, the Project developer would be required to refer to "Design Out Crime Guidelines: Crime Prevention Through Environmental Design," published by the LAPD. Contact the Community Relations Division, located at 100 W. 1st Street, #250, Los Angeles, CA 90012; (213) 486-6000. The Project would include standard security measures such as adequate security lighting, controlled residential access, and secure parking facilities. These measures for the Project shall be approved by the LAPD prior to the issuance of building permits. Through compliance with the requirements of the LAPD, Project impacts related to police protection services would be less than significant.

Cumulative Impacts

Implementation of the related projects listed on Table IV-38 could result in a net increase in the number of residents and employees in the Project area and could further increase the demand for police protection services. Cumulative development requires the LAPD to continually evaluate the need for new or physically altered facilities in order to maintain adequate service ratios. Similar to the proposed Project, the related projects would be subject to the site plan review, recommendations of the LAPD related to crime prevention features, and other applicable regulations of the LAMC. Through the process of compliance, the ability of the LAPD to provide adequate facilities to accommodate future growth and maintain acceptable levels of service would be ensured. Furthermore, the increased demands for additional LAPD staffing, equipment, and facilities would be funded via existing mechanisms (e.g., property taxes and government funding) to which the Project and related projects would contribute. Therefore, cumulative impacts related to police protection services would be less than significant.

(iii) Schools?

Less Than Significant Impact. Los Angeles Unified School District's (LAUSD) schools that serve the Project site and area are shown on Table IV-28. As shown on Table IV-29, the Project would generate a total of approximately 11 students, including 5 elementary students, 3 middle school students, and 3 high

school students. Based on the remaining capacity shown on Table IV-28, the schools serving the Project site would have adequate capacity to serve the Project's student generation. Pursuant to the California Government Code, mandatory payment of the school fees established by the LAUSD in accordance with existing rules and regulations regarding the calculation and payment of such fees would, by law, provide full and complete mitigation for any potential direct and indirect impacts to schools as a result of the Project. Additionally, the Project Applicant would be required to implement Mitigation Measures 16-2 and 16-4 (refer to Checklist Question 16a, Transportation/Traffic) to ensure that construction-related traffic would not cause any safety issues for Farmdale Elementary School and El Sereno Middle School located near the Project site. Therefore, Project impacts to school services would be less than significant.

Cumulative Impacts

The related projects listed on Table IV-38 could result in an increase in the number students in the Project area. However, similar to the applicant of the proposed Project, the applicants of all the related projects would be required to pay the applicable school fees to the LAUSD to ensure that no significant impacts to school services would occur. Therefore, cumulative impacts to school services would be less than significant.

Table IV-28
LAUSD School's Serving the Project Area Student Capacity and Enrollment

School Type (Grade)	School Name	Capacity (students)	Actual Enrollment (students)	(-)Under / (+)Over Capacity (students)
Elementary School	Farmdale Elementary School	546	501	-45
Middle School	El Sereno Middle School	1,568	1,304	-264
High Schools	Lincoln Senior High School	1,702	1,365	-337
	Wilson Senior High School	2,077	1,748	-329
Source: LAUSD, Rena Perez, Director, March 9, 2015 (refer to Appendix H).				

**Table IV-29
Estimated Project Student Generation**

Use Type	Amount of Development	School Type	Student Generation Factor ^a	Total Students Generated
Residential	42 du	Elementary School (K-5)	0.1266/du	5
		Middle School (6-8)	0.0692/du	3
		High School (9-12)	0.0659/du	3
Total				11
<i>du = dwelling unit Number of students has been rounded to the nearest whole number.</i>				
<i>^a Los Angeles Unified School District, Student Generation Rate Calculation, February 25, 2008.</i>				

(iv) Parks?

Less Than Significant Impact. The Project would consist of 42 single-family residential homes, which would generate an estimated 114 residents. The standard minimum parkland-to-population ratio, provided in the City's General Plan Framework Element, is two acres of parkland per 1,000 residents generated. Therefore, implementation of the Project would require approximately 0.228 acre of parkland.⁸⁰ However, the Project Applicant shall pay all required parkland (i.e., Quimby) fees pursuant to the LAMC, including, in consultation with the City of Los Angeles Department of Recreation and Parks, the Project Applicant shall be required to comply with one or more of the following: 1) dedicate two acres of parkland per 1,000 residents, 2) pay in-lieu fees for any land dedication requirement shortfall, or 3) provide on-site improvements equivalent in value of the in-lieu fees, or any portion thereof. Through compliance with the LAMC, Project impacts related to parks and recreational facilities would be less than significant.

Cumulative Impacts

The related projects listed on Table IV-38 could result in an increase demand for parks and recreational services. However, employees generated by the commercial projects and the commercial portions of mixed-use projects on the related projects list would not typically enjoy long periods of time during the workday to visit parks and/or recreational facilities. Therefore these related-project-generated employees would not contribute to the future demand on park and recreational facility services. The extent to which the related residential projects include parks/recreational amenities is unknown. However, the applicants of these projects would be subject to the parkland fees pursuant to LAMC Section 17.12, ensuring that

⁸⁰ $[(114 \text{ residents}) \div (1,000)] = 0.114 \text{ thousand residents. } [(2 \text{ acres of parkland}) \times (0.114 \text{ thousand residents})] = 0.228 \text{ required acre.}$

any potential impacts to parks and recreational facilities would be less than significant, similar to the proposed Project. As stated previously, Project impacts related to parks and recreational facilities would be less than significant. Therefore, cumulative impacts to park and recreational facilities would be less than significant.

(v) Other public facilities?

Libraries

Less Than Significant Impact. The City of Los Angeles Public Library (LAPL) provides library services throughout the City. The LAPL's Branch Facilities Plan includes criteria for new libraries and recommends new size standards for the provision of LAPL facilities: 12,500 square feet for communities with a population of less than 45,000; 14,500 square feet for communities with a population of more than 45,000; and up to 20,000 square feet for a regional branch. It also recommends that when a community reaches a population of 90,000, an additional branch library should be considered for the community.

As discussed previously, the Project would introduce approximately 114 residents to the Project site. (It should be noted that some or all of the 114 residents could already live in the Project area or City with an existing demand for library services that would not be increased with implementation of the Project.) However, the number of residents generated by the Project is minimal and would not require the need for new or expanded library facilities. Therefore, Project impacts to library service would be less than significant.

Cumulative Impacts

Implementation of the related projects listed on Table IV-38 could increase the demand for library services in the Project area. The related residential projects would be subject to the standards to determine demand for library facilities used by the City, and would likely be required to implement mitigation where applicable. As such, the demand for library services created by these residential projects could be accommodated, and impacts would be less than significant. As stated previously, Project impacts related to library services would be less than significant. Therefore, cumulative impacts to library services would be less than significant.

15. RECREATION

a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?

Less Than Significant Impact. As discussed above in response to Checklist Question 14iv (Parks), the Project Applicant would be required to either dedicated approximately 0.228 acre of parkland, pay in-lieu fees, or provide on-site improvements equivalent in value to in-lieu fees (or any portion thereof). The

Project would not cause substantial deterioration of parks and recreational facilities. Therefore, impacts related to this issue would be less than significant.

b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?

Less Than Significant Impact. Refer to response to Checklist Question 14(a)(iv).

Cumulative Impacts

Refer to discussion of cumulative impacts related to parks and recreational facilities under response to Checklist Question 14(a)(iv).

16. TRANSPORTATION AND TRAFFIC

a) Would the project conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?

Less Than Significant With Mitigation Incorporated. The analysis and information in this section is primarily based on the Traffic Impact Study for El Sereno – VTTM 73531 (2520-2608) N. Eastern Avenue) Los Angeles, California, prepared by KOA Corporation and dated November 11, 2016 (refer to Appendix I).

Project Study Area

The study area included the following signalized study intersections:

1. Eastern Avenue/El Sereno Avenue and Huntington Drive
2. Eastern Avenue and Lombardy Boulevard
3. Eastern Avenue and Klamath Street
4. Eastern Avenue and Valley Boulevard

Figure IV-2 illustrates the locations of the four study intersections and the Project site.

Methodologies

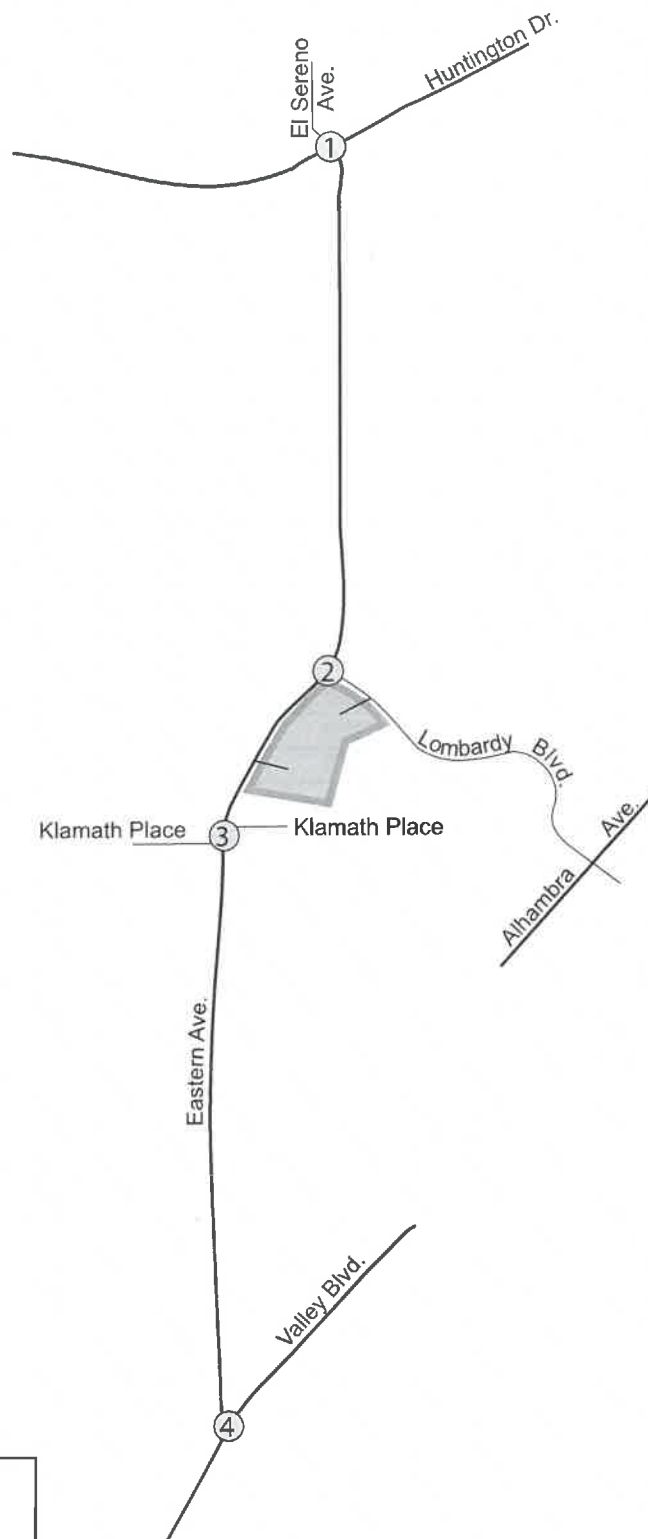
Based on LADOT's current traffic study policies, this study uses the Critical Movement Analysis (CMA) methodology for the analysis and evaluation of traffic operations at signalized intersections under their

jurisdiction, as detailed in Circular Number 212 published by the Transportation Research Board (TRB).⁸¹ This analysis technique describes the operating characteristics of an intersection in terms of the “Level of Service” (LOS) based on intersection traffic volume and other variables such as number and type of signal phasing, lane geometries, and other factors which determine both the quantity of traffic that can move through an intersection (Capacity) and the quality of that traffic flow (LOS).


“Capacity” represents the maximum total hourly volume of vehicles in the critical lanes that has a reasonable expectation of passing through an intersection under prevailing roadway and traffic conditions. Critical lanes are defined generally as those intersection movements or groups of movements which exhibit the highest “per lane” volumes, thus defining the maximum amount of vehicles attempting to travel through the intersection during a specific time period. The capacity of an intersection also varies based on the number of signal phases for the location; more signal phases generally result in more “lost” or “startup” time, as drivers exhibit slight reaction delays when signal indications change from “red” to “green.” For the CMA analysis methodology, the intersection capacities associated with the various levels of service are therefore based on the number of traffic signal phases, as shown on Table IV-30.


For the intersection evaluation and transportation planning purposes of this traffic study, LADOT policy requires that the maximum “baseline” capacity of an intersection equate to the value associated with LOS E shown on Table IV-30. This value represents the highest volume of traffic that can be adequately accommodated through urban area intersections without a breakdown in operations, resulting in unstable traffic flows, high levels of congestion, and long delays.

⁸¹ *Interim Materials on Highway Capacity, Circular Number 212, Transportation Research Board, Washington, D.C., 1980.*



LEGEND

 Project Location

 Study Intersections

Source: KOA Corporation Planning & Engineering, 2016.



Not to Scale

The “Critical Movement” indices at an intersection are determined by first identifying the sum of the critical lane traffic volumes at the intersection. This total traffic *volume* value, which represents the most critical intersection demand, is then divided by the appropriate intersection *capacity* value for the type of signal control at the intersection, to determine the “CMA value” for the intersection that is roughly equivalent to its volume-to-capacity ratio.

Table IV-30
CMA Volume Ranges per LOS*

LOS	Maximum Sum of Critical Volumes (VPH) vs. Number of Signal Phases		
	Two Phases	Three Phases	For or More Phases
A	900	855	825
B	1,050	1,000	965
C	1,200	1,140	1,100
D	1,350	1,275	1,225
E	1,500	1,425	1,375
F	NA	NA	NA
* For planning applications only. Not appropriate for operations/design applications.			

LOS describes the quality of traffic flow through the intersection. LOS A through LOS C exhibit good traffic flow characteristics, with little congestion. LOS D is typically the level for which metropolitan area street systems are designated, and represents the highest level of acceptable congestion and delay. LOS E defines conditions at or near the capacity of an intersection, and is characterized by short-duration stoppages and unstable traffic flows at its upper range. LOS F occurs when a facility is overloaded, and is characterized by stop-and-go traffic with long duration delays. Note that the LOS definitions do not represent a single operating condition, but rather correspond to a range of CMA values, as shown on Table IV-31.

Table IV-31
LOS Definitions for Signalized Intersections (CMA Method)

LOS	Intersection Capacity Utilization	Definition
A	0.000 - 0.600	EXCELLENT. No vehicle waits longer than one red light and no approach phase is fully used.
B	0.601 - 0.700	VERY GOOD. An occasional approach phase is fully utilized; many drivers begin to feel somewhat restricted within groups of vehicles.
C	0.701 - 0.800	GOOD. Occasionally drivers may have to wait through more than one red light; backups may develop behind turning vehicles.
D	0.801 - 0.900	FAIR. Delays may be substantial during portions of the rush hours, but enough lower volume periods occur to permit clearing of developing lines, preventing excessive backups.
E	0.901 - 1.000	POOR. Represents the most vehicles intersection approaches can accommodate; may be long lines of waiting vehicles through several cycles.
F	> 1.000	FAILURE. Backups from nearby locations or on cross streets may restrict or prevent movement of vehicles out of the intersection approaches. Tremendous delays with continuously increasing queue lengths.
Source: Transportation Research Board, Transportation Research Circular No. 212, Interim Materials on Highway Capacity, 1980.		

Existing Conditions

Roadway System

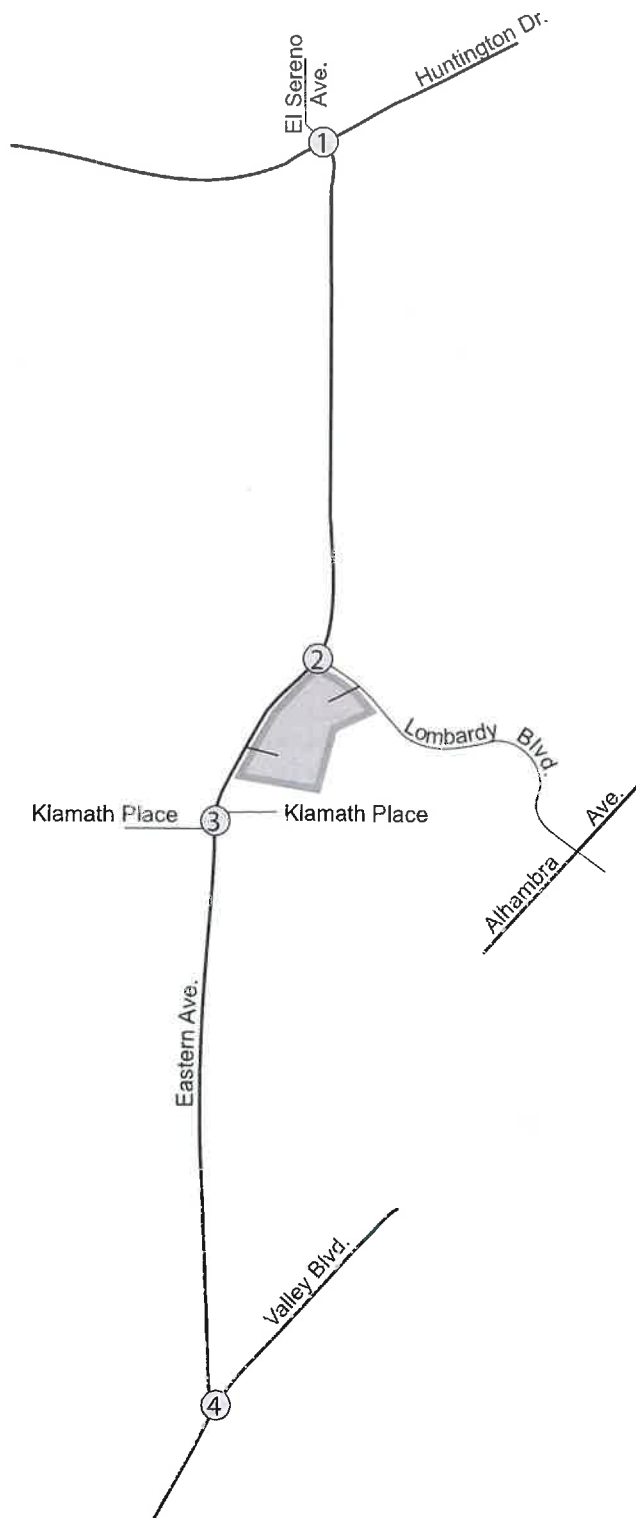
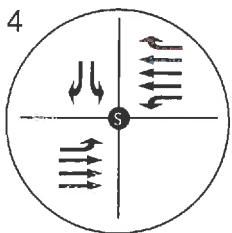
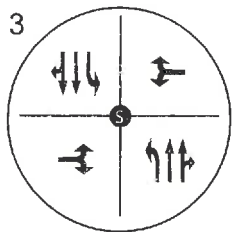
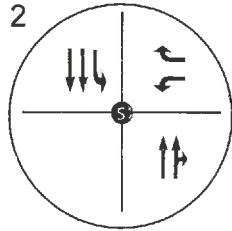
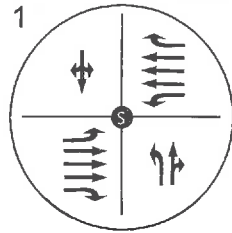
Figure IV-3 illustrates the existing traffic controls and approach lane geometries at the study intersections. The key roadways within the study area are described on Table IV-32.

Existing Transit

Table IV-33 provides a description of the bus transit lines that operate within the study area. The existing transit lines are illustrated on Figure IV-4.

Existing Traffic Volumes

Study intersection counts including vehicle, pedestrian and bicycle volumes were collected at the study intersections on Tuesday, October 25, 2016 and Tuesday, November 8, 2016 from 7:00 AM to 10:00 AM and from 2:00 PM to 5:00 PM. The highest four consecutive 15-minute vehicle counts during the two time periods were used to determine the peak hour traffic volumes at each intersection. The existing weekday AM peak hour and PM peak hour traffic turn movement volumes are shown on Figures IV-5 and IV-6, respectively.



LEGEND

- Project Location
- Study Intersection
- Signalized Intersection
- Intersection Lane Geometry

Source: KOA Corporation Planning & Engineering, 2016.



Not to Scale

Table IV-32
Study Area Roadway Descriptions

Segment	# Lanes		Median Type	Parking Restrictions		General Land Use	Posted Speed Limit (mph)
	NB/EB	SB/WB		North Side/ East Side	South Side/ West Side		
EASTERN AVENUE							
Huntington Dr. to Gambler St.	2	2	Striped	1 hr. 8am-6pm	1 hr. 8am-6pm	Commercial	35
Gambler St. to Lombardy Blvd.	2	2	Striped	Parking Permitted	15 min. 7am-5pm & NS 7am-5pm School Days	School/Residential	35
Lombardy Blvd to Klamath St.	2	2	Striped, TWLT	NSAT	NP 7am-5pm School days; NSAT	School/Open Space	35
Klamath St. to Druid St.	2	2	Striped	Parking Permitted	Parking Permitted	Church/Residential	35
Druid St. to Valley Blvd.	2	2	Striped	Parking Permitted	Parking Permitted	Residential	35
EL SERENO AVENUE							
North of Huntington Dr.	1	1	Not Striped	NP 8am-10am Friday	NP 8am-10am Thursday	Residential	Not Posted
HUNTINGTON DRIVE							
West of Eastern Ave.	3	3	Raised	Parking Permitted, No Parking	Parking Permitted, No Parking	Commercial	35
East of Eastern Ave.	3	3	Raised	1 hr. 8am-6pm	1 hr. 8am-6pm	Commercial	35
LOMBARDY BOULEVARD							
East of Eastern Ave.	1	1	Striped	Parking Permitted	Parking Permitted	Residential	Not Posted
West of Alhambra Ave.	1	1	Not Striped	Parking Permitted	Parking Permitted	Residential	Not Posted
East of Alhambra Ave.	1	1	Not Striped	Parking Permitted	Parking Permitted	Residential	Not Posted
KLAMATH PLACE							
West of Eastern Ave.	1	1	Not Striped	Parking Permitted	Parking Permitted	Residential	Not Posted
East of Eastern Ave.	1	1	Not Striped	Parking Permitted	Parking Permitted	Residential	Not Posted
VALLEY BOULEVARD							
West of Eastern Ave.	3	3	Striped	No Stopping 7am-9am	No Stopping 4pm-7pm	Light Industrial	30
East of Eastern Ave.	3	3	Striped	NSAT	NSAT	Light Industrial	Not Posted
NS = no stopping NSAT = no stopping anytime NP = no parking TWLT = two-way left turn							
Source: KOA Corporation, 2016.							

Table IV-33
Existing Transit Service Summary

Agency	Line	From	To	Via	Approx. Peak Frequency
Metro	76	Los Angeles	El Monte	Valley Boulevard	12-15 minutes
Metro	78	Los Angeles	Arcadia	Huntington Drive	6-20 minutes
Metro	79	Los Angeles	Arcadia	Huntington Drive	15-30 minutes
Metro	378	Los Angeles	Arcadia	Huntington Drive	11-28 minutes
Metro	256	Commerce	Altadena	Eastern Avenue & Valley Boulevard	45 minutes
LADOT	DASH	El Sereno/City of Terrace		Eastern Avenue Druid Street Lombardy Boulevard Alhambra Avenue	15-25minutes
Source: KOA Corporation, 2016					

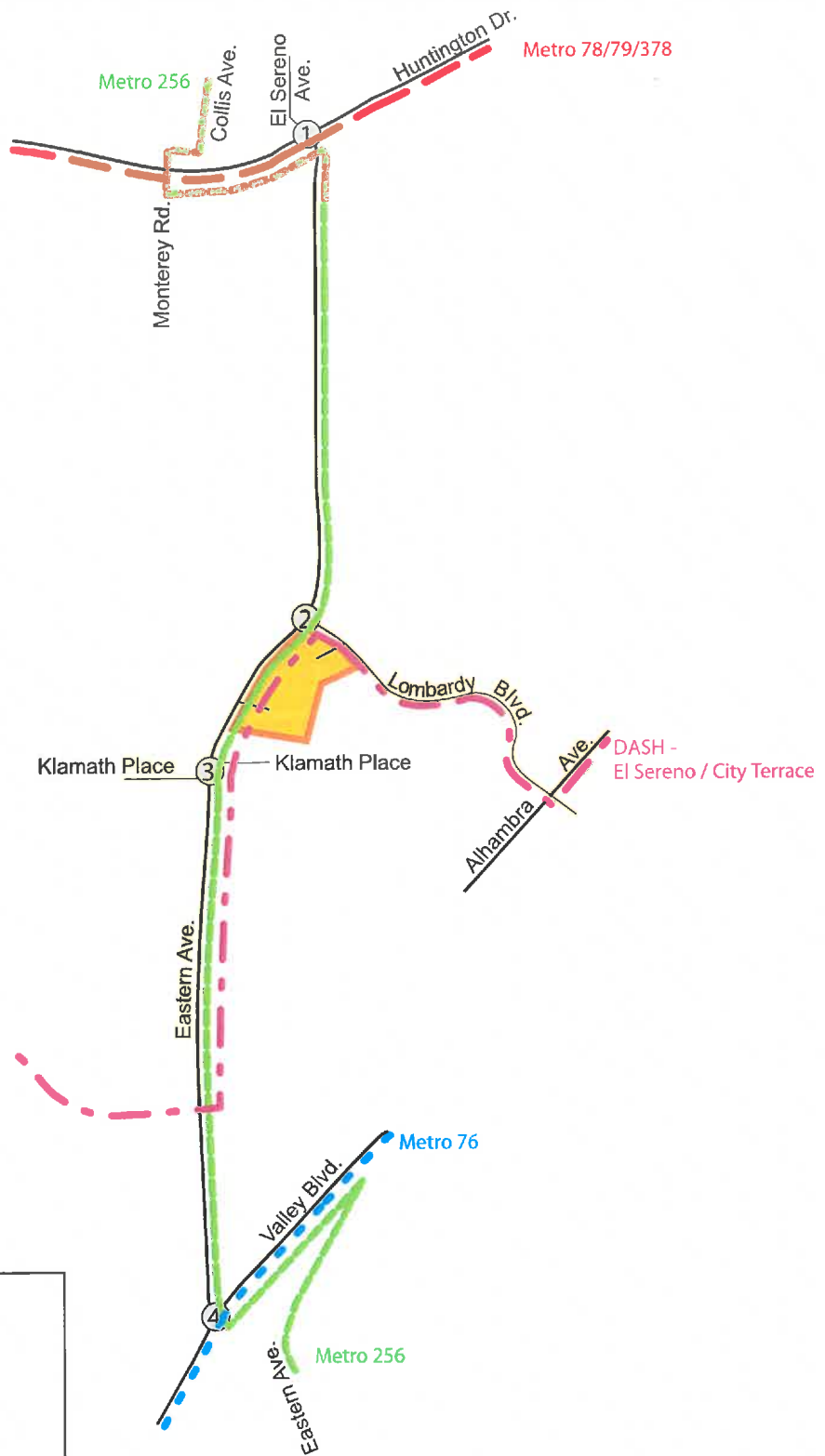
Existing Traffic Signal System

Automated Traffic Surveillance And Control (ATSAC) is a computer-based traffic signal control system whereby engineers monitor traffic conditions and system performance, selects appropriate signal timing (control) strategies, and performs equipment diagnostics and alert functions. Sensors in the street detect the passage of vehicles, vehicle speed, and the level of congestion. This information is received on a second-by-second (real-time) basis and is analyzed on a minute-by-minute basis at the ATSAC Operations Center to determine if better traffic flow can be achieved by changing the signal timing. If required, the signal timing is either automatically changed by the ATSAC computers or manually changed by the operator using communication lines that connect the ATSAC Center with each traffic signal. To supplement the information from electronic detectors, closed-circuit television (CCTV) surveillance equipment has been and continues to be installed at critical locations throughout the City.

Adaptive Traffic Control System (ATCS) is the latest enhancement to ATSAC that provides fully traffic adaptive signal control based on real-time traffic conditions. The ATCS will automatically adjust traffic signal timing in response to current traffic demands by allowing ATCS to simultaneously control all three critical components of traffic signal timing, namely cycle length, phase split and offset.

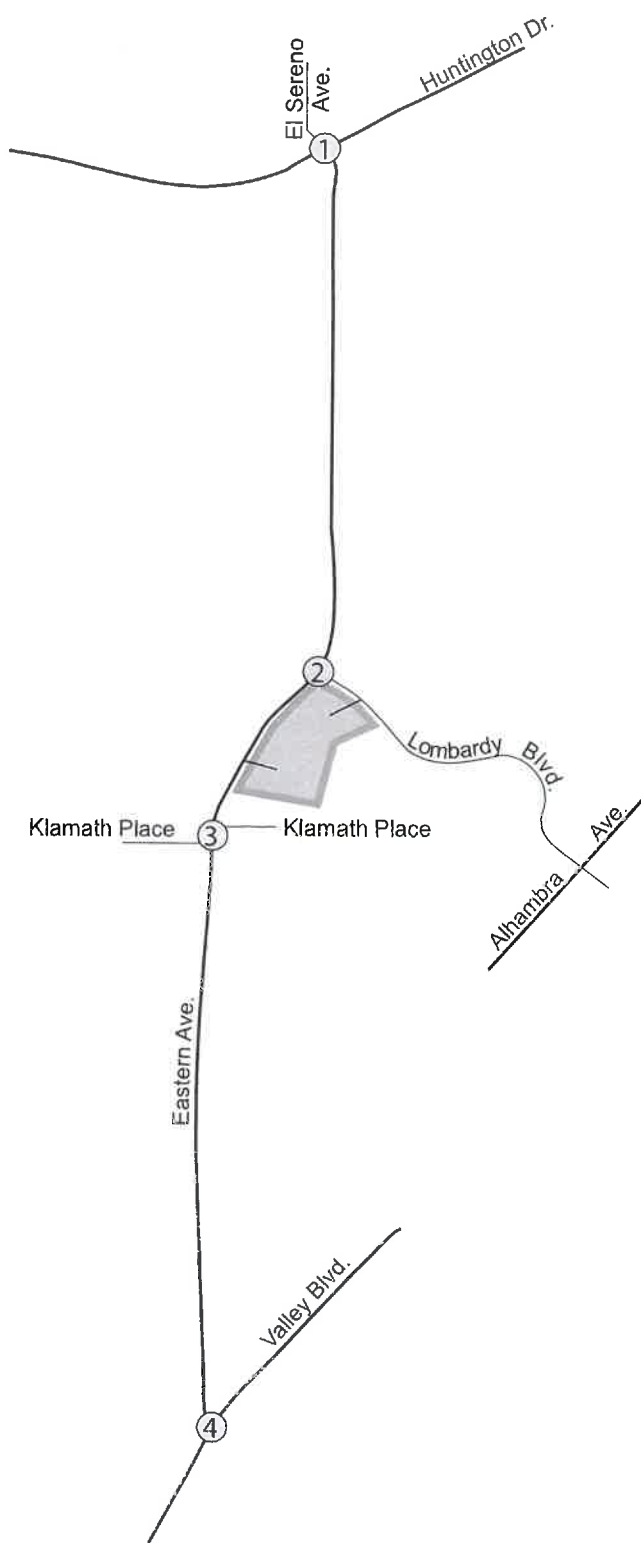
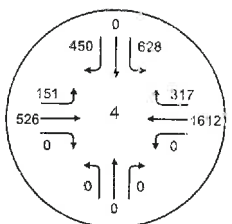
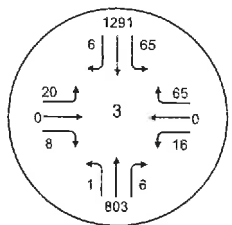
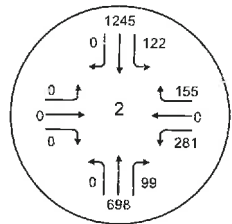
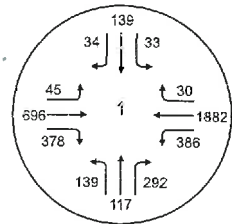
For capacity analysis, LADOT guidelines suggest a 0.07 reduction in volume-to-capacity ratio with the implementation of ATSAC and a 0.03 reduction with the implementation of ATCS, for an overall volume-to-capacity reduction of 0.10. This reduction represents field-measured benefits in flow and capacity increase by operation of this program.

Based on information obtained from LADOT, all of the study intersections are currently equipped with ATSAC/ATCS and is subjected to an overall volume-to-capacity reduction of 0.1 for both existing and future conditions to reflect the ATSAC and ATCS enhancements.



Source: KOA Corporation Planning & Engineering, 2016.





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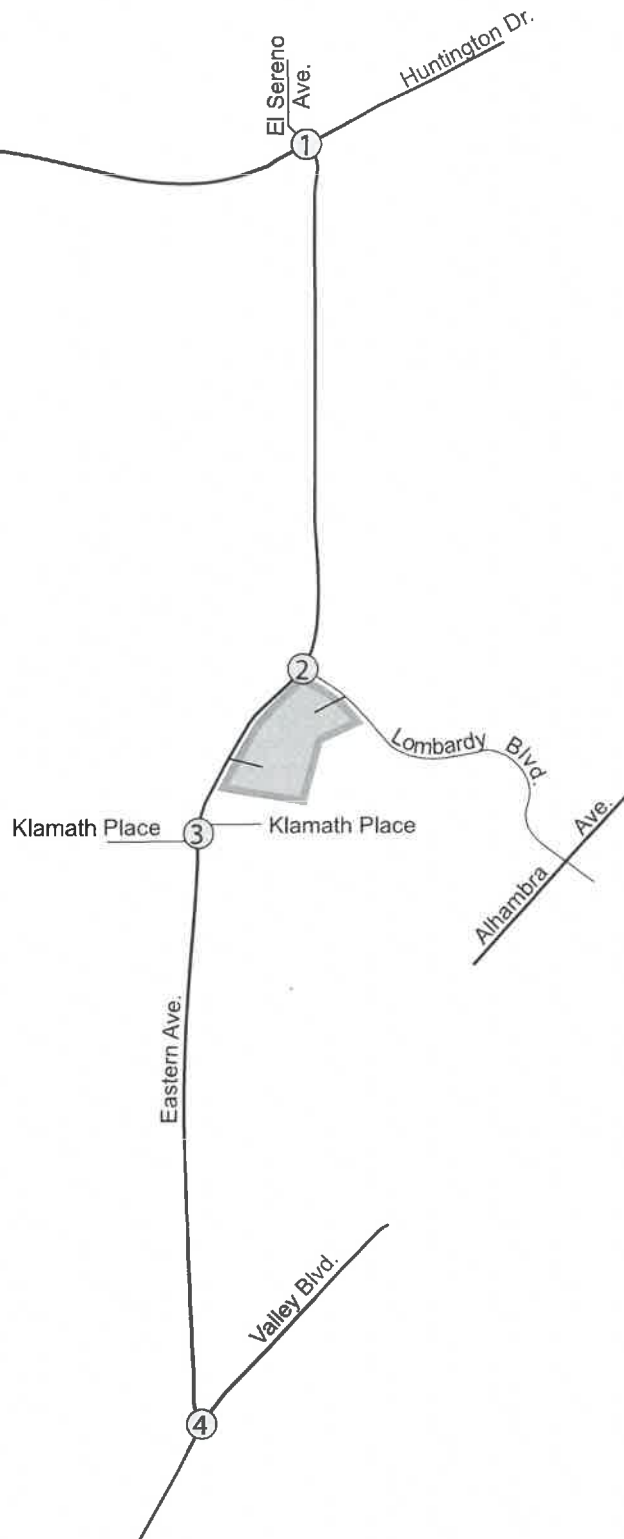
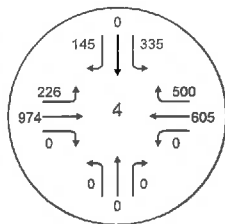
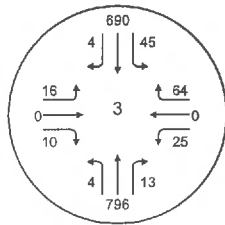
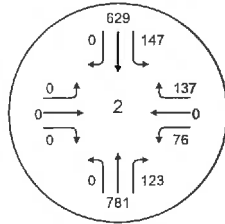
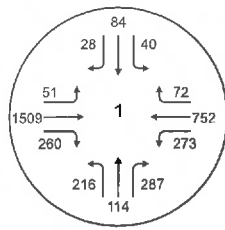
- Project Location
- Study Intersections
- Intersection Volumes

Source: KOA Corporation Planning & Engineering, 2016.

NORTH

Not to Scale

Figure IV-5
Existing Weekday AM Peak Hour Traffic Volumes



LEGEND

- Project Location
- Study Intersections
- Intersection Volumes

Source: KOA Corporation Planning & Engineering, 2016.



Existing Intersection Levels of Service

Based on the intersection lane geometries depicted on Figure IV-3 and the existing traffic volumes shown on Figures IV-5 and IV-6, volume-to-capacity (V/C) ratios and corresponding levels of service (LOS) were determined for each of the study intersections during both weekday AM and PM peak hours. Table IV-34 summarizes the volume-to-capacity ratios and LOS values for existing traffic conditions. As shown in this table, all of the study intersections are currently operating at a good level of service (LOS D or better) during the weekday AM and PM peak hours.

Threshold of Significance

LADOT's significance criteria for determining intersection LOS impacts are shown on Table IV-35.

Table IV-34
Existing LOS

Study Intersection		AM Peak Hour		PM Peak Hour	
		V/C	LOS	V/C	LOS
1	Eastern Ave./El Sereno Ave. & Huntington Dr.	0.727	C	0.785	C
2	Eastern Ave. & Lombardy Boulevard	0.534	A	0.374	A
3	Eastern Ave. & Klamath St	0.401	A	0.270	A
4	Eastern Ave. & Valley Blvd.	0.824	D	0.435	A

Source: KOA Corporation, 2016

Table IV-35
LADOT Intersection Significance Thresholds

Intersection Conditions with Project Traffic		Project-related Increase in V/C Ratio
LOS	V/C	
C	0.701 - 0.800	Equal to or greater than 0.04
D	0.801 - 0.900	Equal to or greater than 0.02
E, F	> 0.900	Equal to or greater than 0.01

Source: LADOT.

Project Impacts

Project Trip Generation

Trip generation for the Project was calculated by using rates published in ITE's *Trip Generation*, 9th Edition. The trip rates and the associated trip generation forecasts are shown on Table IV-36. As shown, the Project would generate approximately 400 daily weekday vehicle trips, including 32 trips (8 inbound

trips and 24 outbound trips) during the AM peak hour and 42 trips (26 inbound trips and 16 outbound trips) during the PM peak hour.

Table IV-36
Project Trip Generation

Land Use	ITE Code	Intensity	Daily	AM Peak Hour			PM Peak Hour		
				In	Out	Total	In	Out	Total
Trip Generation Rates									
Single-family Detached Housing	21	1 unit	9.52	25%	75%	0.75	63%	37%	1.00
Project Trips									
Single-Family Detached Housing	210	42 units	400	8	24	32	26	16	42
Source: KOA Corporation, 2016									

Project Trip Distribution

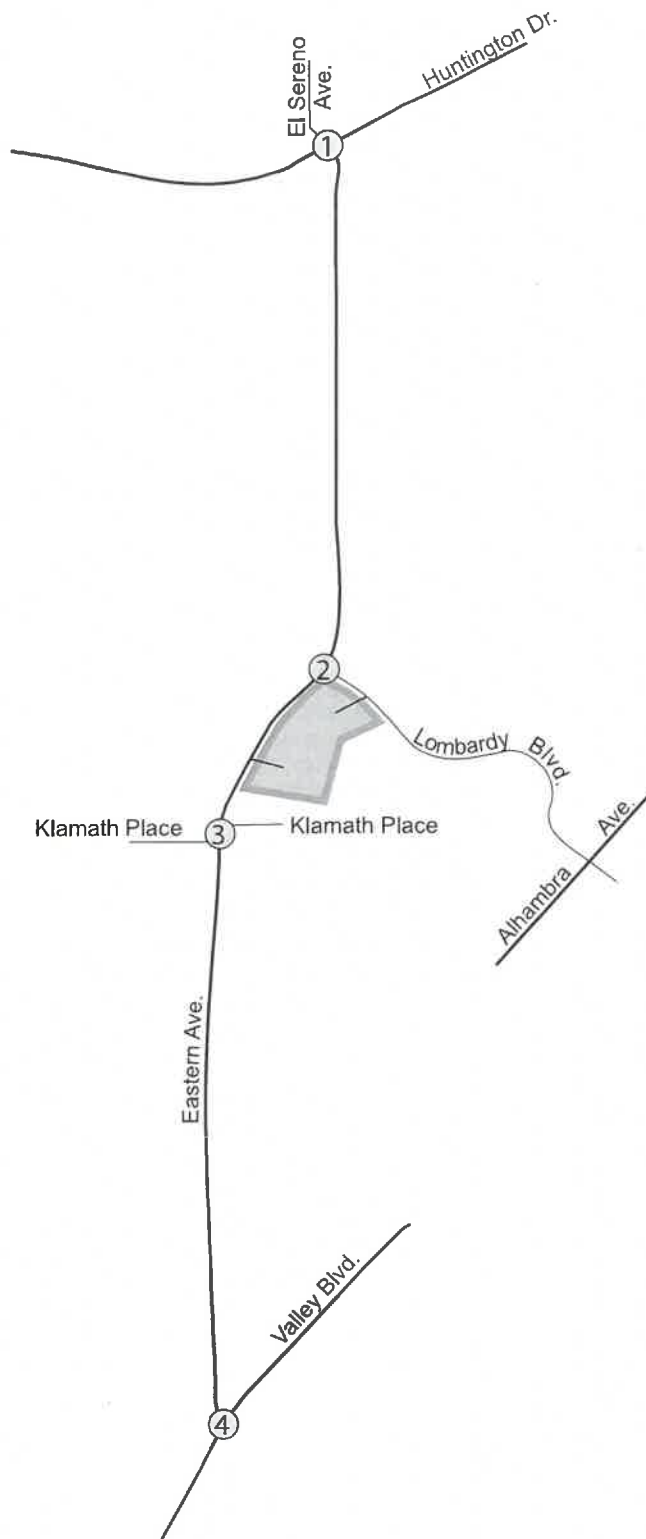
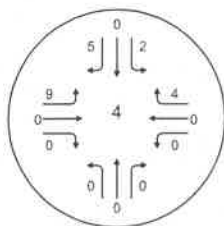
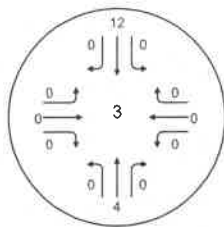
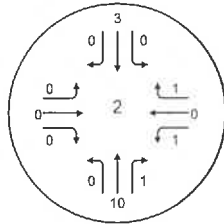
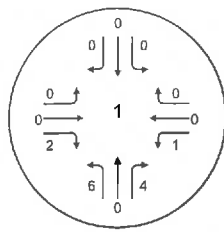
Trip distribution is the process of assigning the directions from which traffic will access a project site. Trip distribution is dependent upon the land use characteristics of the project, the local roadway network, and the general locations of other land uses to which project trips would originate or terminate. Figure IV-7 illustrates the trip distribution percentages at the study intersections that were used for the traffic impact analysis.

Project Trip Assignment

Based on the trip generation and distribution assumptions described above, project traffic was assigned to the roadway system based on site driveway locations. Figures IV-8 and IV-9 illustrate the Project trips for the weekday AM and PM peak hours, respectively.

Existing Plus Project Conditions

This section documents existing traffic conditions at the study intersections with the addition of Project-generated traffic. Traffic volumes for these conditions were derived by adding Project trips to the existing traffic volumes. The Existing Plus Project traffic volumes are provided on Figure IV-10 for the AM peak hour and on Figure IV-11 for the PM peak hour. Table IV-37 summarizes the resulting V/C and LOS values at the study intersections for the Existing Plus Project conditions. As shown, the Project would not result in any significant LOS impacts at any of the study intersections. Therefore, Project impacts under the Existing Plus Project traffic conditions would be less than significant.



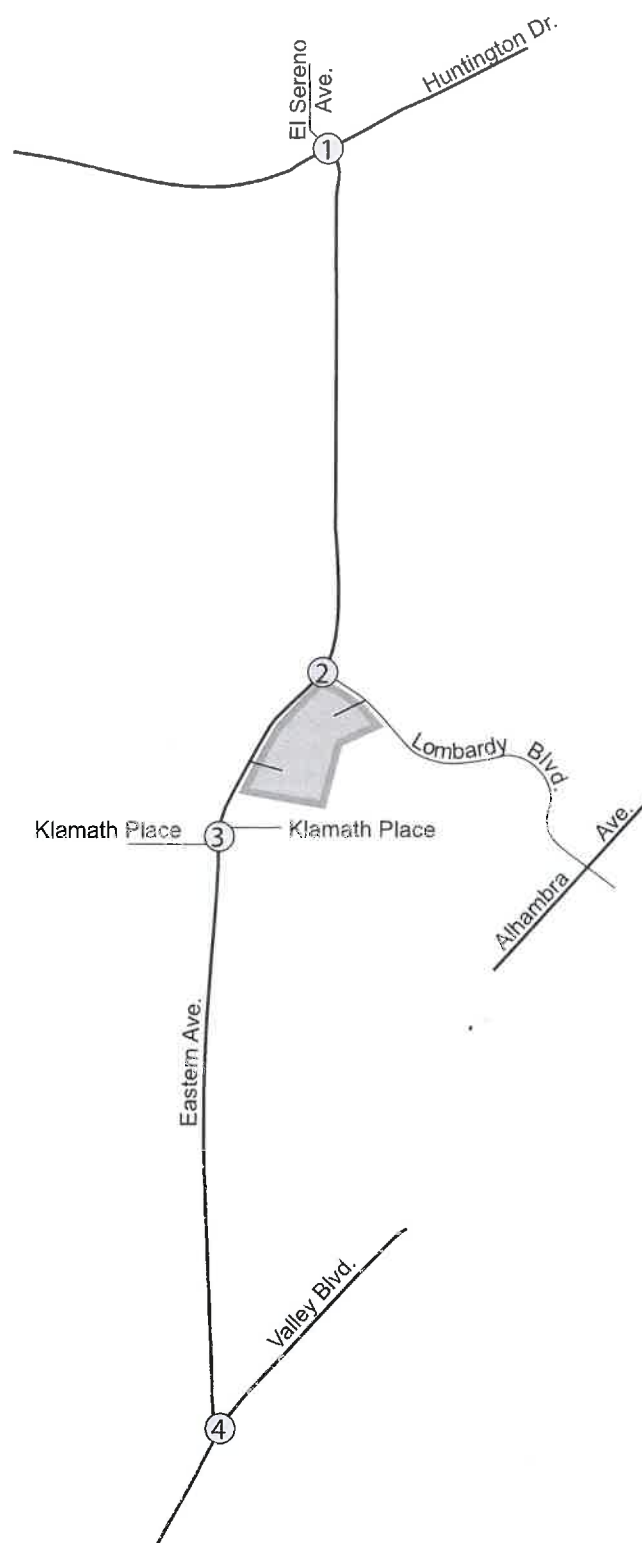
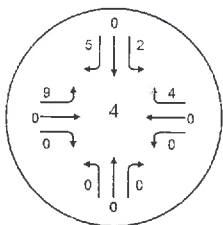
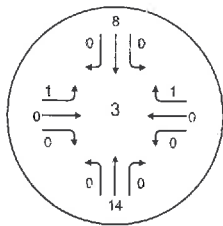
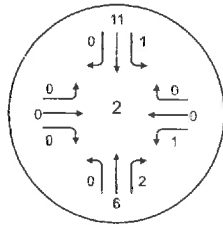
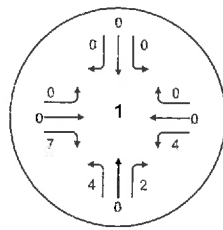
LEGEND

- Project Location
- Study Intersections
- Intersection Volumes

Source: KOA Corporation Planning & Engineering, 2016.



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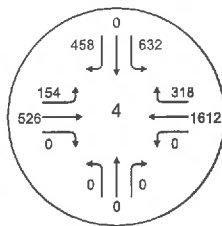
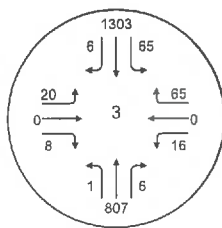
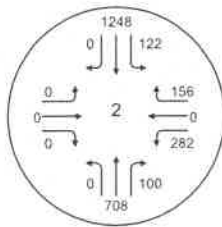
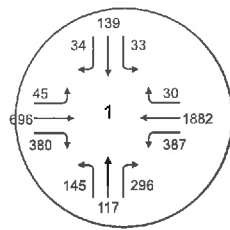


LEGEND

- Project Location
- Study Intersections
- Intersection Volumes

Source: KOA Corporation Planning & Engineering, 2016.

Not to Scale



LEGEND



Project Location



Study Intersections

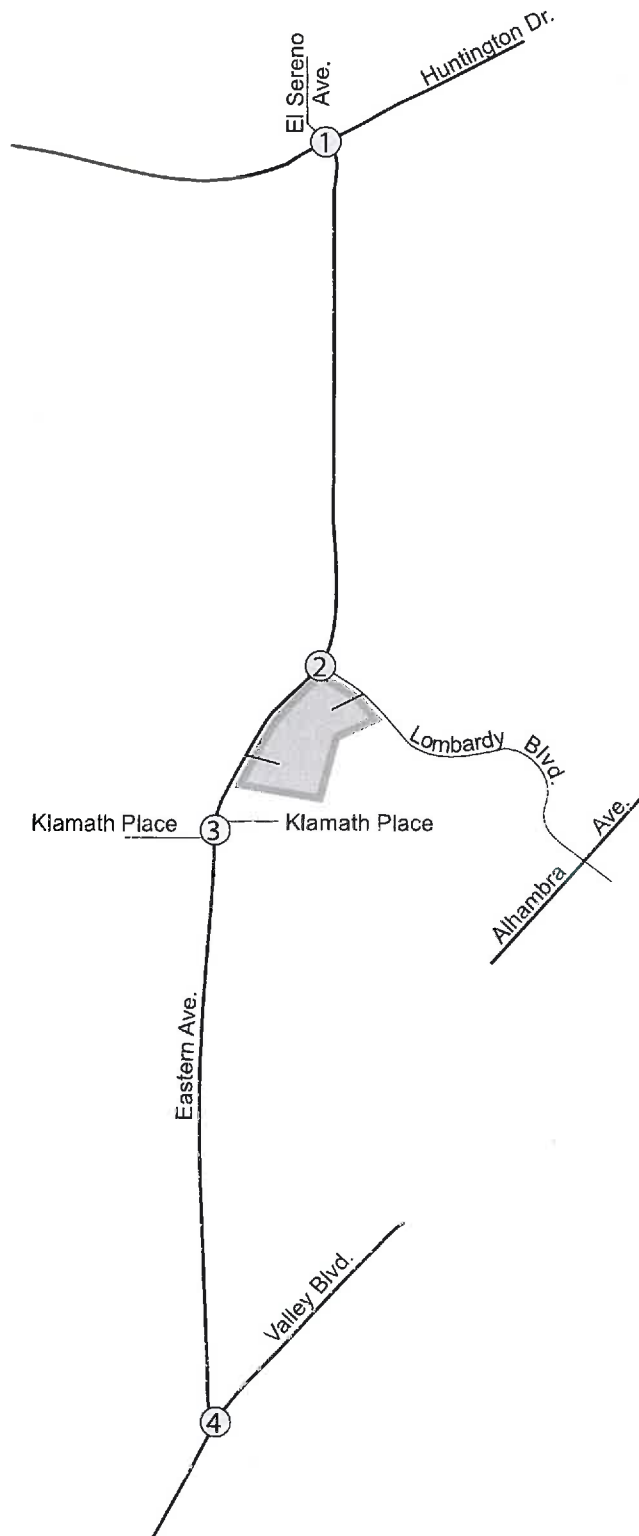
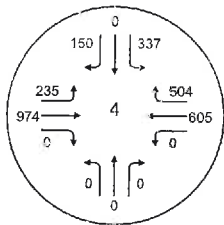
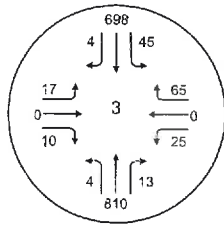
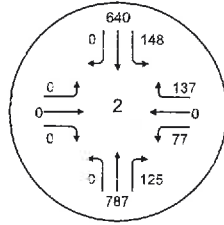
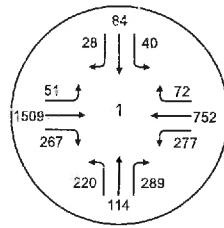


Intersection Volumes

Source: KOA Corporation Planning & Engineering, 2016.



Not to Scale



LEGEND



Project Location



Study Intersections



Intersection Volumes

Source: KOA Corporation Planning & Engineering, 2016.



Not to Scale

**Table IV-37
Existing Plus Project LOS**

Study Intersection		Existing 2016		Existing + Project		Change in V/C	Significant Impact?
		V/C	LOS	V/C	LOS		
AM PEAK HOUR							
1	Eastern Ave./El Sereno Ave. & Huntington Dr.	0.727	C	0.730	C	0.003	NO
2	Eastern Ave. & Lombardy Boulevard	0.534	A	0.536	A	0.002	NO
3	Eastern Ave. & Klamath St	0.401	A	0.405	A	0.004	NO
4	Eastern Ave. & Valley Blvd.	0.824	D	0.828	D	0.004	NO
PM PEAK HOUR							
1	Eastern Ave./El Sereno Ave. & Huntington Dr.	0.785	C	0.789	C	0.004	NO
2	Eastern Ave. & Lombardy Boulevard	0.374	A	0.378	A	0.004	NO
3	Eastern Ave. & Klamath St	0.270	A	0.276	A	0.006	NO
4	Eastern Ave. & Valley Blvd.	0.435	A	0.443	A	0.008	NO
Source: KOA Corporation, 2016							

Cumulative Impacts

Ambient Growth

The future period forecast includes an ambient growth rate to account for both population and employment growth in the project vicinity. A conservative growth factor of 1.02 was applied to the existing traffic volumes to reflect growth from year 2016 conditions to year 2020 conditions.

Related Projects

Based on a review of the area/related project lists provided by LADOT Development Review, a list of four area/related projects was identified for inclusion in the traffic analysis. These projects are located within an approximate 1.5-mile radius from the site. Table IV-38 and Figure IV-12 illustrates the locations of the area/related projects. Table IV-38 summarizes the trip generation of the included related projects. The related projects traffic was added to the surrounding street system in the study area.

**Table IV-38
Related Projects Trip Generation**

	Related Project	Daily	AM Peak Hour			PM Peak Hour		
			In	Out	Total	In	Out	Total
1	Grifols Pharmaceutical Manufacturing Facility 5557 E. Valley Blvd. -Clean Room & Lab 40,000 sf -Office 16,700 sf -Mechanical Support 37,600 sf	316	46	14	60	22	39	61
2	Mixed Commercial 5479 E. Huntington Dr. -Car Wash Other -Restaurant 1,916 sf -Retail 1,880 sf	1,155	26	24	50	58	40	98
3	Single-Family Homes 2739 N. Onyx Dr. -31 homes	295	6	17	23	20	11	31
4	Warehouse 1925 N. Marianna Ave. -Warehouse 196,000 sf	698	110	24	134	28	83	111
Source: KOA Corporation, 2016								



LEGEND



Project Location



Parks



Related Project Locations

Source: KOA Corporation Planning & Engineering, 2016.



Not to Scale

Future 2020 Without-Project Conditions

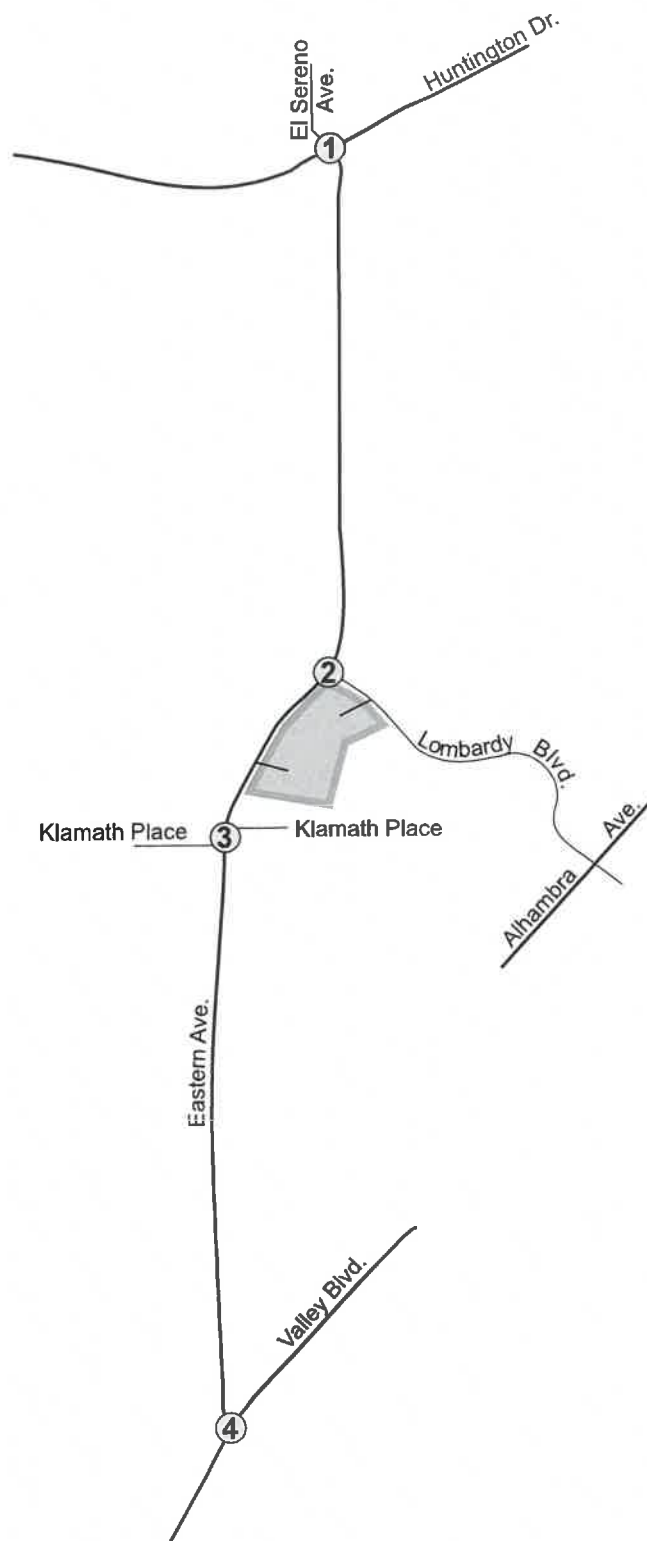
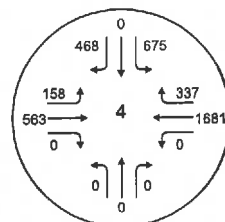
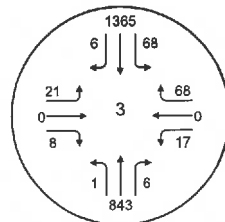
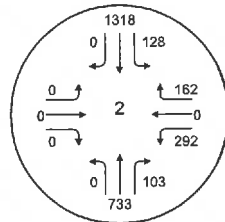
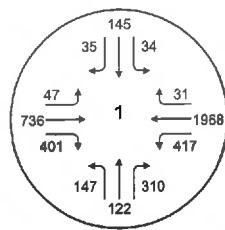
The Future 2020 Without-Project traffic volumes are shown on Figure IV-13 for the AM peak hour and on Figure IV-14 for the PM peak hour. Table IV-39 summarizes the V/C and LOS values at the study intersections for future 2020 conditions under this scenario. As shown, all of the study intersections are projected to operate at a good level of service (with some reductions to LOS D but not E or F) for the Future 2018 Without-Project conditions during the AM and PM peak hours.

Future 2020 With-Project Conditions

The Future 2020 With-Project traffic volumes are shown on Figure IV-15 for the AM peak hour and on Figure IV-16 for the PM peak hour. Table IV-39 summarizes the V/C and LOS values at the study intersections for future 2020 conditions under this scenario. As shown, the Project would not result in any significant LOS impacts at any of the study intersections. Therefore, Project impacts under the Future 2020 With-Project traffic conditions would be less than significant.

Construction Traffic

Construction of the Project would generate construction-related traffic trips associated with delivery of construction equipment and materials, construction workers, and export of approximately 78,000 cubic yards of dirt. All construction-related traffic trips would occur outside of the peak hours for traffic conditions in the Project area (7:00 AM to 9:00 AM and 4:00 PM to 6:00 PM). Construction workers would arrive at the Project site around 7:00 AM and would leave around 3:00 PM. In accordance with Mitigation Measure 16-1, all construction equipment and vehicles (including workers' cars) would be parked/staged on the Project site and not on the streets near the Project site. The roadways providing access to and from the Project site would not be blocked by construction equipment during any phase of construction. Additionally, due to the Project site's proximity to Farmdale Elementary and El Sereno Middle School, the Project Applicant would be required to comply with Mitigation Measures 16-2 and 16-4, which include coordination with the schools' administrators regarding the Project's construction schedule, activities, and hauling of dirt. With implementation of Mitigation Measures 16-1 through 16-5, the Project's construction-related traffic impacts would be less than significant.



LEGEND



Project Location



Study Intersections

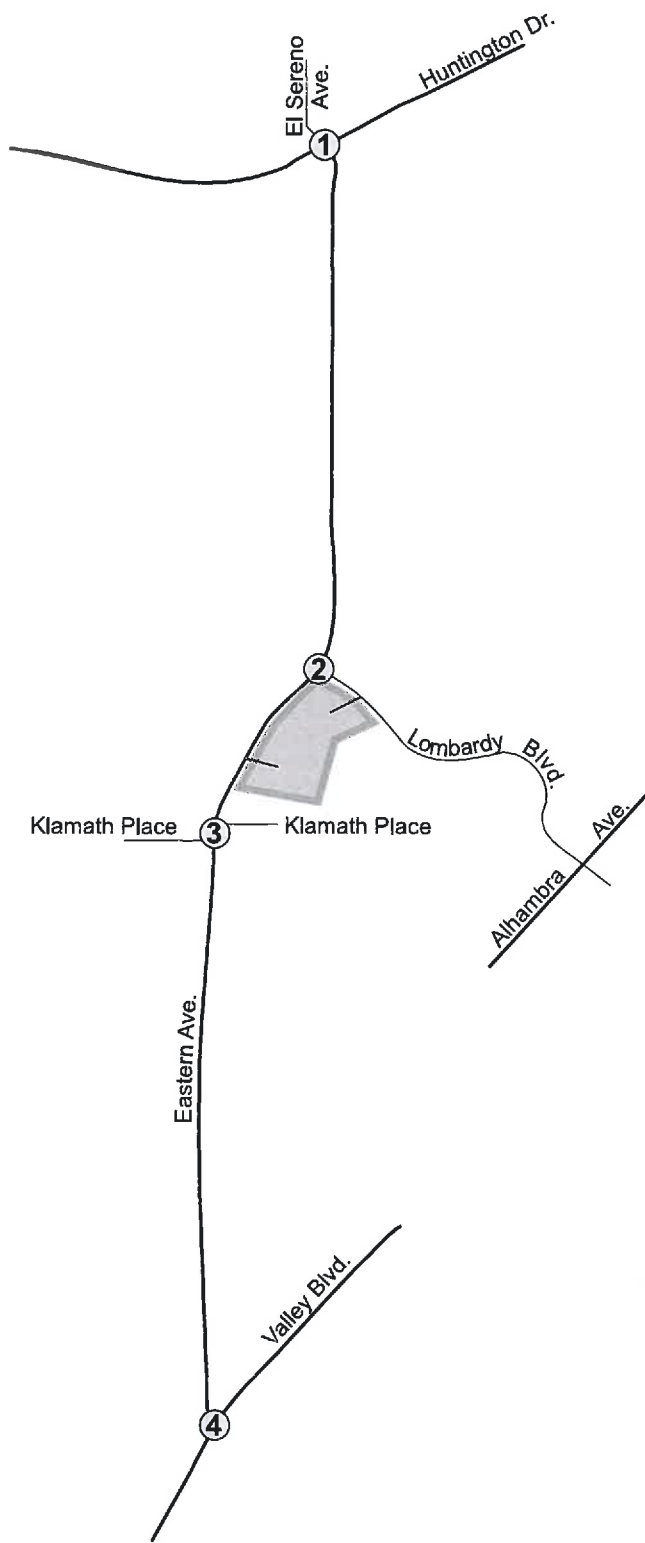
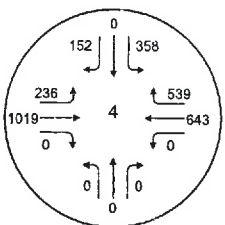
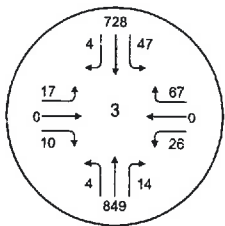
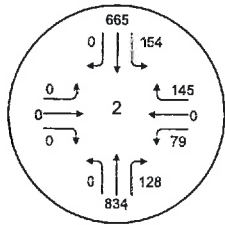
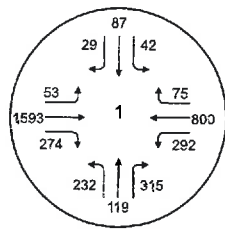


Intersection Volumes

Source: KOA Corporation Planning & Engineering, 2016.



Not to Scale



LEGEND

- Project Location
- Study Intersections
- Intersection Volumes

Source: KOA Corporation Planning & Engineering, 2016.

Not to Scale

Figure IV-14
Future 2020 Without Project-
Weekday PM PEak-Hour Turn Volumes

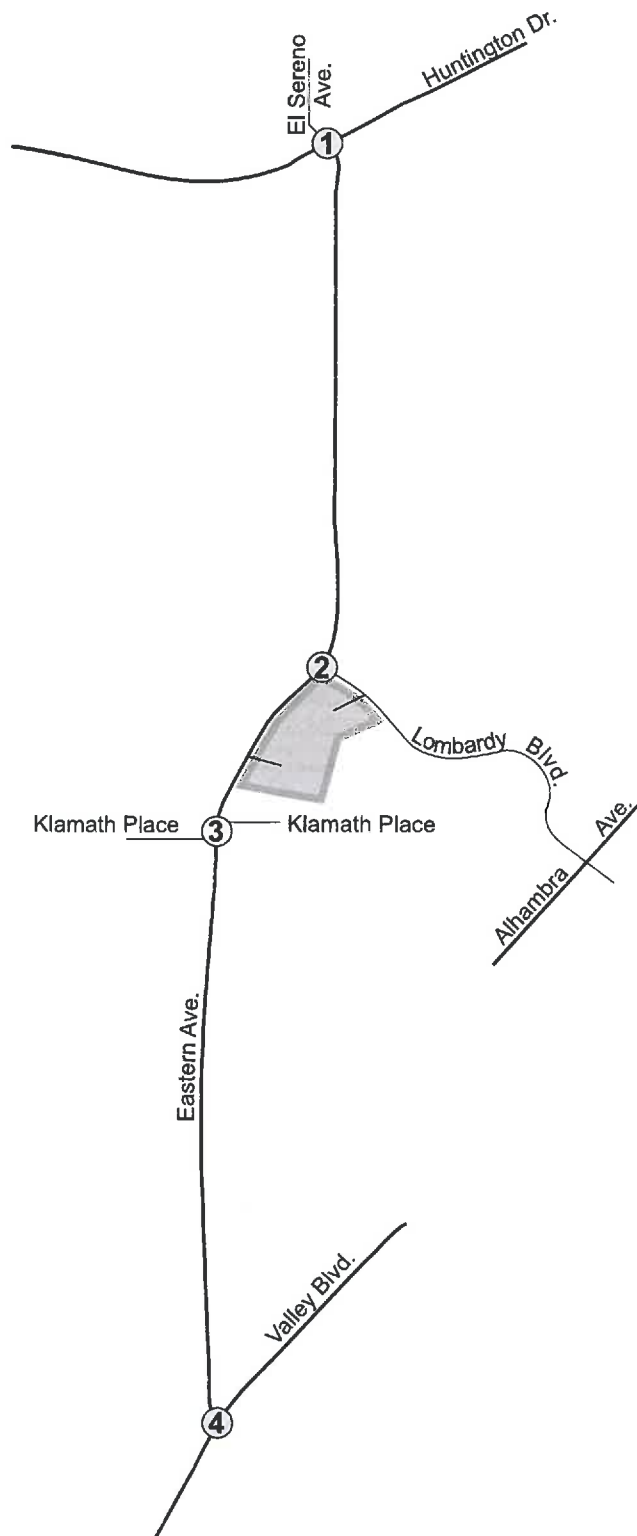
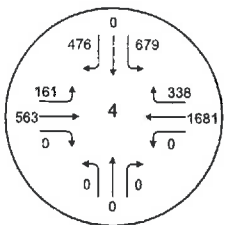
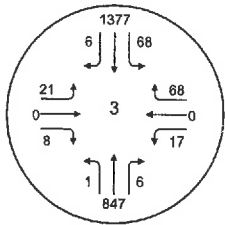
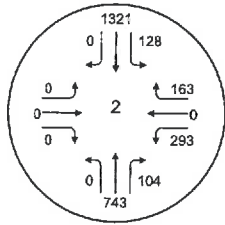
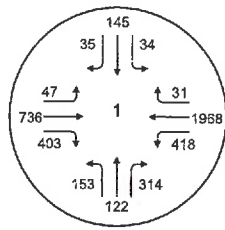
**Table IV-39
Future 2020 Without and With Project LOS**

Study Intersection		Existing 2016		Future 2020 No Project		Future 2020 With Project		Change in V/C	Significant Impact?
		V/C	LOS	V/C	LOS	V/C	LOS		
AM PEAK HOUR									
1	Eastern Ave./El Sereno Ave. & Huntington Dr.	0.727	C	0.781	C	0.784	C	0.003	NO
2	Eastern Ave. & Lombardy Boulevard	0.534	A	0.555	A	0.557	A	0.002	NO
3	Eastern Ave. & Klamath St	0.401	A	0.417	A	0.421	A	0.004	NO
4	Eastern Ave. & Valley Blvd.	0.824	D	0.859	D	0.864	E	0.005	NO
PM PEAK HOUR									
1	Eastern Ave./El Sereno Ave. & Huntington Dr.	0.785	C	0.826	D	0.830	E	0.004	NO
2	Eastern Ave. & Lombardy Boulevard	0.374	A	0.392	A	0.396	A	0.004	NO
3	Eastern Ave. & Klamath St	0.270	A	0.284	A	0.290	A	0.006	NO
4	Eastern Ave. & Valley Blvd.	0.435	A	0.456	A	0.464	A	0.008	NO
Source: KOA Corporation, 2016.									

b) Would the project conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the count congestion management agency for designated roads or highways?

No Impact. The Congestion Management Plan (CMP) is a State-mandated program that serves as the monitoring and analytical basis for transportation funding decisions in the County made through the Regional Transportation Improvement Program (RTIP) and State Transportation Improvement Program (STIP) processes. The CMP requires that a Traffic Impact Analysis (TIA) be performed for all CMP arterial monitoring intersections where a project would add 50 or more trips during either the morning or afternoon weekday peak hours and all mainline freeway monitoring locations where a project would add 150 or more trips (in either direction) during the morning or afternoon weekday peak hours.

Because the Project would not generate 50 or more peak-hour trips, the Project would not add 50 or more trips during the morning or afternoon peak hours at CMP monitoring intersections, no significant impacts would occur as a result of the Project, and no further review of potential impacts to intersection monitoring locations that are part of the CMP system is required.



LEGEND



Project Location



Study Intersections

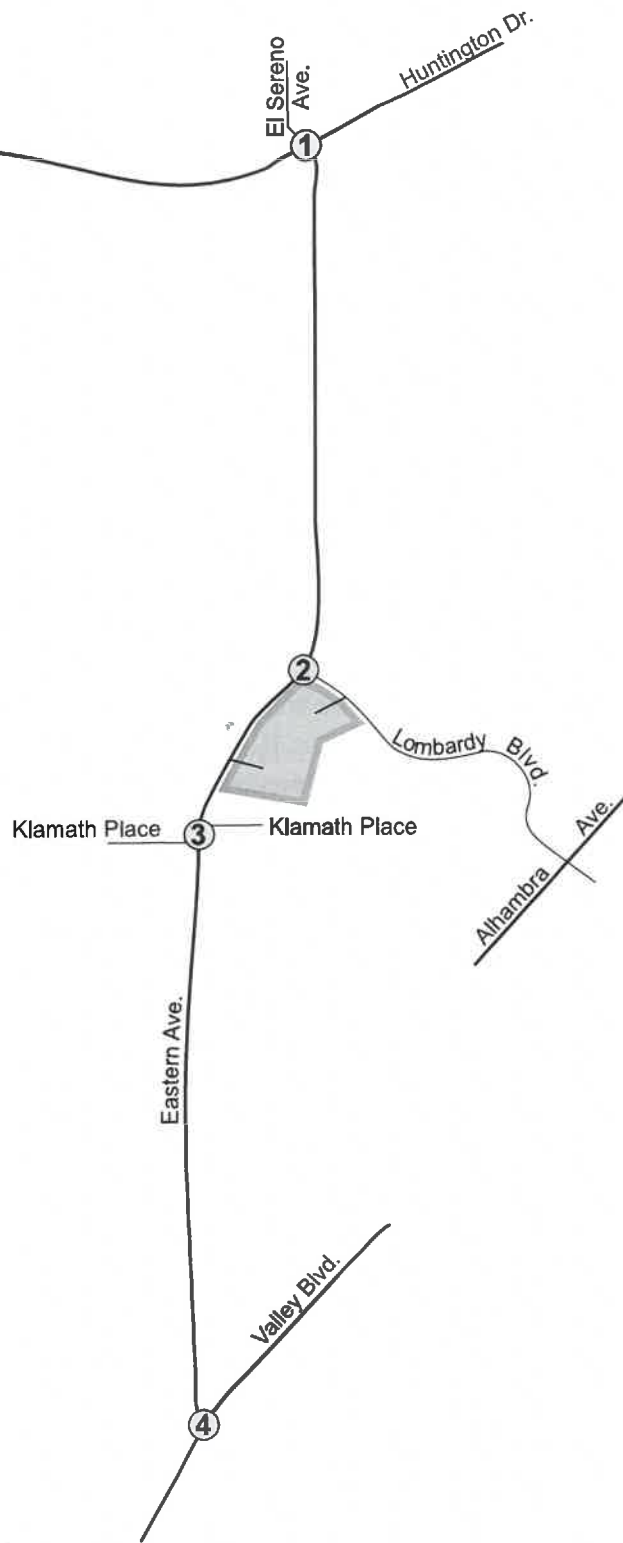
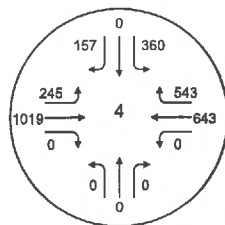
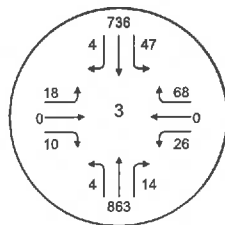
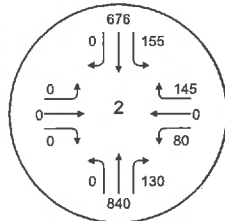
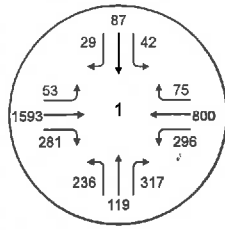


Intersection Volumes

Source: KOA Corporation Planning & Engineering, 2016.



Not to Scale



LEGEND



Project Location



Study Intersections



Intersection Volumes

Source: KOA Corporation Planning & Engineering, 2016.



Not to Scale

c) Would the project result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?

No Impact. The Project includes development of 42 single-family homes, reaching approximately 22 feet in height, a height that is within the height range of the existing buildings in the Project area. The Project site is not located near any airports. Thus, the Project would not result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks. Therefore, no impacts related to this issue would occur.

d) Would the project substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?

Less Than Significant Impact. The Project does not include development of any roadway infrastructure. The Project includes development of 42 single-family homes, similar to those found in the immediate vicinity of the Project site. The majority of the Project homes would have vehicles access to the Project site area roadway network via a driveway on Eastern Avenue. The driveway would be located between the Eastern Avenue/Klamath Street-Harmony Lane intersection on the south and the Eastern Avenue/Ruth Swiggett Drive intersection (park access) on the north.

Level of service calculations were reviewed for the Eastern Avenue Project driveway location, using the Project trip generation at the west site driveway and the thru volumes on Eastern Avenue defined by the study intersections to the north and south. The inbound/southbound left-turn movement into the Project site would use the existing striped center two-way left-turn lane within the roadway. The Project inbound vehicle movements expected during the peak hours is 7 vehicles and 22 vehicles for the a.m. and p.m. peak hours, respectively.

The calculations indicate that the queuing at the southbound left-turn movement, at the inbound flow to the Eastern Avenue Project driveway, would be less than one vehicle on average. With two to three vehicles potentially queuing, although this would be a rare occurrence, there would be adequate length within the center two-way left-turn lane to provide adequate access to both the park use on the north via a northbound left-turn movement, and access to the Project site via a southbound left-turn movement. Thus, the Project would not create any hazards, and no impacts would occur as a result of the Project.

e) Would the project result in inadequate emergency access?

Less Than Significant Impact. The Project includes development of 42 single-family homes, similar to those found in the immediate vicinity of the Project. Each home would include a driveway that extends from on-site roadways to the garages associated with the homes. All parking associated with the homes would be provided at the site of the homes and not on the roadways. Additionally, all ingress/egress associated with the Project would be designed and constructed in conformance to all applicable City Building and Safety Department and City Fire Department standards and requirements for design and

construction. Therefore, the Project would not result in any significant impacts related to emergency access.

f) Would the project conflict with adopted policies, plans, or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks)?

No Impact. The Project would generate approximately 114 people, which would not put a great demand on transit services in the Project area. No impacts related to this issue would occur as a result of the Project.

Mitigation Measures (Traffic/Transportation)

To ensure that no significant construction-related traffic impacts occur during the Project's construction phase, the following mitigation measures are required:

16-1: Hillside Construction Staging and Parking Plan

- Prior to the issuance of a grading or building permit, the applicant shall submit a Construction Staging and Parking Plan to the Department of Building and Safety and the Fire Department for review and approval. The plan shall identify where all construction materials, equipment, and vehicles will be stored through the construction phase of the project, as well as where contractor, subcontractor, and laborers will park their vehicles so as to prevent blockage of two-way traffic on streets in the vicinity of the construction site. The Construction Staging and Parking Plan shall include, but not be limited to, the following:
 - No construction equipment or material shall be permitted to be stored within the public right-of-way.
 - If the property fronts on a designated Red Flag Street, on noticed "Red Flag" days, all the workers shall be shuttled from an off-site area, located on a non-Red Flag Street, to and from the site in order to keep roads open on Red Flag days.
 - During the Excavation and Grading phases, all haul trucks shall be staged on the Project site. The drivers shall be required to follow the designated travel plan or approved Haul Route.
 - Truck traffic directed to the project site for the purpose of delivering materials, construction-machinery, or removal of graded soil shall be limited to off-peak traffic hours, Monday through Friday only. No truck deliveries shall be permitted on Saturdays or Sundays.

- All deliveries during construction shall be coordinated so that all vendor/delivery vehicles will stage and make deliveries on the project site, and that a construction supervisor is present at such time.
- A radio operator shall be on-site to coordinate the movement of material and personnel, in order to keep the roads open for emergency vehicles, their apparatus, and neighbors.
- During all phases of construction, all construction vehicle parking and queuing related to the project shall be as required to the satisfaction of the Department of Building and Safety, and in substantial compliance with the Construction Staging and Parking Plan, except as may be modified by the Department of Building and Safety or the Fire Department.

16-2: Construction Activity Near Schools

- The Project developer and contractors shall maintain ongoing contact with administrators of the Farmdale Elementary School and the El Sereno Middle School. The administrative offices shall be contacted when demolition, grading and construction activity begin on the Project site so that students and their parents will know when such activities are to occur. The developer shall obtain school walk and bus routes to the schools from either the administrators or from the LAUSD's Transportation Branch (323) 342-1400 and guarantee that safe and convenient pedestrian and bus routes to the school be maintained.
- The developer shall install appropriate traffic signs around the site to ensure pedestrian and vehicle safety.
- There shall be no staging or parking of construction vehicles, including vehicles to transport workers on any of the streets adjacent to the school.
- Due to noise impacts on the schools, no construction vehicles or haul trucks shall be staged or idled on these streets during school hours.

16-3: Schools affected by Haul Route

- LADBS shall assign specific haul route hours of operation based upon Farmdale Elementary School and El Sereno Middle School hours of operation.
- Haul route scheduling shall be sequenced to minimize conflicts with pedestrians, school buses and cars at the arrival and dismissal times of the school day. Haul route trucks shall not be routed past the school during periods when school is in session especially when students are arriving or departing from the campus.

16-4: Good Neighbor Construction Practices

- Whenever possible, construction vehicles should be parked on site to prevent congestion on streets with limited parking.
- When temporarily blocking portions of streets for deliveries of construction materials, a flag person shall be provided to assist with pedestrian and vehicular traffic.
- Street closures shall not take place during peak traffic hours. Any street, sidewalk, or other improvement work shall be conducted in conformance with the latest Manual on Work Area Traffic Control.
- Care shall be taken not to overfill concrete trucks during deliveries. If spills occur, it shall be the responsibility of the concrete company to immediately provide clean up.
- Construction noise shall be kept to a minimum with consideration of the surrounding neighbors. Unnecessary noise such as music shall be kept below legal levels.
- Streets and sidewalks adjacent to construction sites shall be swept free of construction debris at all times.
- Care shall be taken to not interfere with trash pick-up by the Bureau of Sanitation. Construction and delivery vehicles shall be subject to trash pick-up parking restrictions.
- If building materials are to be stored in public right of ways, it shall be by permit from the Department of Public Works, Bureau of Street Services, Investigations and Enforcement Division and shall conform with all applicable rules.
- All construction/demolition activities shall comply with the construction hours in Section 41.40 of the LAMC.

16-5: The Project Applicant shall plan construction and construction staging as to maintain pedestrian access to adjacent active land uses throughout all construction phases. This requires the Applicant to maintain adequate and safe pedestrian protection, including physical separation from workspace and vehicular traffic and overhead protection, due to sidewalk closure or blockage, at all times. Barriers, such as K-Rails, scaffolding, etc., shall be maintained at a height of 8 feet.

Cumulative Impacts

Similar to the Project, the applicants of the related projects shown on Table IV-38 would be required by the City to implement measures similar to Mitigation Measures 16-1 through 16-3 identified for the Project's construction traffic, if these related projects included construction activities on a hillside and/or near schools. Cumulative traffic impacts were addressed previously under "Future With-Project Conditions." As shown on Table IV-39, no significant cumulative impacts would occur.

17. TRIBAL CULTURAL RESOURCES

a) Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural

landscape that is geographically defined in terms of size and scope of the landscape, sacred place, or object with cultural value to a California Native tribe, and that is:

- i) **Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k), or**

Less Than Significant Impact. The Project site is vacant and does not contain any structures. Based on a records search conducted by the South Central Coast Information Center (refer to Appendix D), no archaeological sites have been recorded within the Project site, and no archaeological resources have been recorded within 0.5-mile radius of the Project site. However, it is possible that unknown archaeological resources could exist at the Project site, given that significant archaeological resources have been identified in the Los Angeles area. As such, prior to Project construction, the prime contractor and any subcontractor(s) shall be advised of the legal and/or regulatory implications of knowingly destroying cultural resources or removing artifacts, human remains, bottles, and other cultural materials from the Project site. In addition, in the event that buried archaeological resources are exposed during Project construction, work within 50 feet of the find shall stop until a professional archaeologist, meeting the standards of the Secretary of the Interior, can identify and evaluate the significance of the discovery and develop recommendations for treatment, in conformance with California Public Resources Code Section 21083.2. However, construction activities could continue in other areas of the Project site. Recommendations could include preparation of a Treatment Plan, which could require recordation, collection and analysis of the discovery; preparation of a technical report; and curation of the collection and supporting documentation in an appropriate depository. Any Native American remains shall be treated in accordance with state law. Through compliance with these requirements, potential Project impacts to unknown archaeological (tribal) resources would be less than significant.

- ii) **A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe?**

Less Than Significant Impact. Pursuant to AB 52, the Department of City Planning notified Native American tribes as to the Project with a 30-day comment period on December 28, 2016. A letter was received, dated January 10, 2017, from the Gabrieleno Band of Mission Indians requesting that an approved Native American Monitor(s) be present during future ground disturbance. However, the letter provided no documentation regarding the potential for significant tribal resources to exist at the Project site. As stated previously, the Project site is vacant and does not contain any structures, but has been developed in the past. Based on a records search conducted by the South Central Coast Information Center (refer to Appendix D), no archaeological sites have been recorded within the Project site. However, it is possible that unknown archaeological resources could exist at the Project site, given that significant archaeological resources have been identified in the Los Angeles area. As such, prior to

Project construction, the prime contractor and any subcontractor(s) shall be advised of the legal and/or regulatory implications of knowingly destroying cultural resources or removing artifacts, human remains, bottles, and other cultural materials from the Project site. In addition, in the event that buried archaeological resources are exposed during Project construction, work within 50 feet of the find shall stop until a professional archaeologist, meeting the standards of the Secretary of the Interior, can identify and evaluate the significance of the discovery and develop recommendations for treatment, in conformance with California Public Resources Code Section 21083.2. However, construction activities could continue in other areas of the Project site. Recommendations could include preparation of a Treatment Plan, which could require recordation, collection and analysis of the discovery; preparation of a technical report; and curation of the collection and supporting documentation in an appropriate depository. Any Native American remains shall be treated in accordance with state law. Through compliance with these requirements, potential Project impacts to unknown archaeological/tribal resources would be less than significant.

Cumulative Impacts

Refer to discussion of cumulative impacts related to parks and recreational facilities under response to Checklist Issue 5.

18. UTILITIES AND SERVICE SYSTEMS

a) Would the project exceed wastewater treatment requirements of the applicable regional water quality control board?

Less Than Significant Impact. The Project site is located within the service area of the Hyperion Treatment Plant (HTP), which has been designed to treat 450 million gallons per day (mgd) to full secondary treatment. Full secondary treatment prevents virtually all particles suspended in effluent from being discharged into the Pacific Ocean and is consistent with the Los Angeles Regional Water Quality Control Board's (LARWQCB) discharge policies for the Santa Monica Bay. The HTP currently treats an average daily flow of approximately 362 mgd. Thus, there is approximately 88 mgd available capacity.

The Project would generate approximately 8,400 gallons of wastewater per day (or 0.0084 mgd) (refer to Table IV-40.⁸² With a remaining daily capacity of 88 mgd, the HTP would have adequate capacity to serve the Project. Therefore, Project impacts related to wastewater treatment would be less than significant.

⁸² This conservatively assumes the amount of wastewater equals water consumption.

Table IV-40
Estimated Water Consumption

Residential Dwelling Units	Size	Consumption Rate	Total (gallons/day)
3-bedroom	42	200 gpd/du	8,400

Source: City of Los Angeles Bureau of Sanitation, Sewer Generation Rates Table, March 20, 2002.
Note: Water generation rates are approximately 110% of the wastewater generation rates.

Cumulative Impacts

Implementation of the related projects listed on Table IV-38 could increase the need for wastewater treatment. The remaining treatment capacity of the HTP (88 mgd) would accommodate the wastewater treatment requirements of the related projects. As discussed previously, the Project would create the need for a fraction of one percent of the remaining capacity of the HTP, and would not result in any significant impacts related to sewer treatment. No new or upgraded treatment facilities would be required. Therefore, cumulative impacts related to wastewater treatment would be less than significant.

b) Would the project require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?

Less Than Significant Impact. The Los Angeles Department of Water and Power (LADWP) owns and operates the Los Angeles Aqueduct Filtration Plant (LAAFP) located in the Sylmar community of the City. The LAAFP treats City water prior to distribution throughout LADWP's Central Water Service Area. The designated treatment capacity of the LAAFP is 600 mgd, with an average plant flow of 550 mgd during the summer months and 450 mgd in the non-summer months. Thus, the facility has between approximately 50 to 150 mgd of remaining capacity depending on the season.

As shown on Table IV-40, the Project would consume approximately 8,400 gallons of water per day. With the remaining capacity of approximately 50 to 150 mgd, the LAAFP would have adequate capacity to serve the Project. Therefore, Project impacts related to water treatment would be less than significant.

Cumulative Impacts

Implementation of the related projects listed on Table IV-38 could increase the need for water treatment. The remaining treatment capacity of the LAAFP (50 to 150 mgd) would accommodate the wastewater treatment requirements of the related projects. As discussed previously, the Project would create the need for a fraction of one percent of the remaining capacity of the LAAFP, and would not result in any significant impacts related to water treatment. No new or upgraded treatment facilities would be required. Therefore, cumulative impacts related to water treatment would be less than significant.

c) **Would the project require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?**

Less Than Significant Impact. As discussed in response to Checklist Question 9e, the Project would not exceed the capacity of the existing or planning drainage system. Therefore, Project impacts related to storm drain capacity would be less than significant.

Cumulative Impacts

Refer to the discussion of cumulative impacts under response to Checklist Question 9.

d) **Would the project have significant water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?**

Less Than Significant Impact. As shown on Table IV-40, the Project would consume approximately 8,400 gallons of water per day. According to the Los Angeles Department of Water and Power (LADWP), projects that conform to SCAG's demographic projections and are located within the City's service area have been accounted for in LADWP's water supply planning efforts.⁸³ As discussed previously in response to Checklist Question 10b, the Project is consistent with the City's General Plan land use designation for the Project site. Additionally, the Project would include a cistern system that would capture rainwater to use for landscape irrigation in order to help reduce the Project's overall water consumption. Further, the Project would be required to incorporate all of the applicable mandatory water conservation measures identified Chapter 4 of the 2016 California Green Building Standards Code. As such, the Project would not require new or additional water supply or entitlements. Therefore, Project impacts related to water supply would be less than significant.

Cumulative Impacts

As discussed previously, the Project would result in a net water consumption increase of approximately 8,400 gallons of water per day. Implementation of the related projects listed on Table IV-38 could result in a net increase in water consumption within LADWP's service area. Similar to the Project, the water supply needs of those related projects that are consistent with the City's General Plan have been accounted for in the most recently adopted UWMP. However, the applicants of all projects within LADWP's service area would be required to consult with LADWP to determine the specific water supply needs of the project, appropriate water conservation measures to minimize water usage, and LADWP's

⁸³ Los Angeles Department of Water and Power, Amir Tabakh, Chief of Energy Efficiency Engineering, correspondence, June 15, 2015. (Refer to Appendix J.)

ability to serve the project. Through this process, cumulative impacts related to water supply would be less than significant.

e) Would the project result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?

Less Than Significant Impact. As discussed in response to Checklist Question 17a, with a remaining daily capacity of 88 mgd, the HTP would have adequate capacity to serve the Project. Therefore, Project impacts related to wastewater treatment would be less than significant.

f) Would the project be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?

Less Than Significant Impact. Most of the solid waste generated in the City is disposed of at the Sunshine Canyon Landfill and Chiquita Canyon Landfill. The Sunshine Canyon Landfill is jointly operated by the City and the County (each operates separate portions of the landfill). The Sunshine Canyon Landfill currently has a remaining capacity of 64,688,021 tons, with a permitted intake of 12,100 tons per day (tpd) and currently accepts an average of 7,582 tpd and therefore, has a remaining daily intake availability of 4,518 tpd.⁸⁴ The Chiquita Canyon Landfill currently has a remaining capacity of 1,833,3553 tons, with a permitted intake of 6,000 tpd and currently accepts an average of 3,558 tpd, with a remaining daily intake availability of 2,442 tpd.⁸⁵ Thus, the Sunshine Canyon Landfill and the Chiquita Canyon Landfill have a combined remaining permitted daily intake of 6,960 tpd.

The Project is estimated to generate an increase of approximately 1,140 pounds per day (or 0.57 tons/day) of solid waste. With a remaining daily capacity of 6,690 tpd, the existing landfill capacity would be adequate to accommodate the Project's solid waste generation. Therefore, Project impacts related to solid waste would be less than significant.

Cumulative Impacts

Implementation of the related projects listed on Table IV-38 could increase the need for landfill capacity. However, all development in the City is required to comply with the City's Curbside Recycling Program and the Construction and Demolition Waste Recycling Ordinance to minimize the amount of solid waste generated by the development and the need for landfill capacity. As discussed previously, the landfills serving the Project area have available capacity. The Project would create a demand for less than a fraction of one percent of the remaining landfill capacity serving the Project area and would not result in

⁸⁴ County of Los Angeles, *Countywide Integrated Waste Management Plan, 2014 Annual Report, December 2015*.

⁸⁵ *Ibid.*

any significant impacts. Therefore, cumulative impacts related to landfill capacity would be less than significant.

g) Would the project comply with federal, state, and local statutes and regulations related to solid waste?

Less Than Significant Impact. The Project would be required to comply with the City's Curbside Recycling Program and the Construction and Demolition Waste Recycling Ordinance related to solid waste generation, and no significant impacts related to this issue would occur.

19. MANDATORY FINDINGS OF SIGNIFICANCE

a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory?

Less Than Significant With Mitigation Incorporated. For the reasons stated in this Initial Study, with incorporation of the identified mitigation measures, the Project would not have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory.

b) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?

Less Than Significant Impact. For the reasons stated in this Initial Study, the Project would not result in any significant impacts would not have the potential to contribute to significant cumulative impacts.

c) Does the project have environmental effects, which will cause substantial adverse effects on human beings, either directly or indirectly?

Less Than Significant With Mitigation Incorporated. For the reasons stated in this Initial Study, with incorporation of the identified mitigation measures, the Project would not cause substantial adverse effects on human beings, either directly or indirectly.

EXHIBIT E

**DEPARTMENT OF
CITY PLANNING**

COMMISSION OFFICE
(213) 978-1300

CITY PLANNING COMMISSION

SAMANTHA MILLMAN
PRESIDENT

VAHID KHORSAND
VICE-PRESIDENT

DAVID H. J. AMBROZ
CAROLINE CHOE
HELEN LEUNG

KAREN MACK
MARC MITCHELL

VERONICA PADILLA-CAMPOS
DANA M. PERLMAN

**CITY OF LOS ANGELES
CALIFORNIA**



ERIC GARCETTI
MAYOR

EXECUTIVE OFFICES

200 N. SPRING STREET, ROOM 525
LOS ANGELES, CA 90012-4801
(213) 978-1271

VINCENT P. BERTONI, AICP
DIRECTOR

KEVIN J. KELLER, AICP
EXECUTIVE OFFICER

SHANA M.M. BONSTIN
DEPUTY DIRECTOR

TRICIA KEANE
DEPUTY DIRECTOR

ARTHI L. VARMA, AICP
DEPUTY DIRECTOR

LISA M. WEBBER, AICP
DEPUTY DIRECTOR

Decision Date: March 3, 2020

Appeal Date: March 13, 2020

Rob Flitton (O)(A)

TTLC Los Angeles – El Sereno, LLC
2942 Century Place, Suite 121
Costa Mesa, CA 92626

Jim Ries (R)

Craig Lawson & Co., LLC
3221 Hutchison Avenue, Suite D
Los Angeles, CA 90034

Lawrence Wilson, P.L.S. 6712 (E)
Forma Engineering, Inc.
400 San Fernando Mission Blvd.
San Fernando, CA 91340

Case No. VTT-73531-SL

Incidental Cases: APCE-2015-2048-ZC-ZAD-ZAA

CEQA: ENV-2015-1918-MND

Location: 2520 North Eastern Avenue (2532,
2608, and 2668 N. Eastern Ave
and 2647 – 2651 N. Lombardy
Blvd.

Council District: 14 - Huizar

Neighborhood

Council: LA-32

Community Plan

Area: Northeast Los Angeles

Land Use

Designation: Low Residential

Zone: [Q]R1-1D; [Q]RD6-1D

Legal Description: Lot No. FR Cornwall 10 Acres arb 6,
Partition Map of Lots 3 &4 and E
1/2/of SW ¼ of Sec 18 and NE ¼ of
NW ¼ of Sec 19 T1S R12W S.B.M;
Lot No. FR 1 and Vac Ord 83066,
Farmdale Tract; Lot No. 562, TR 6900

In accordance with the provisions of CEQA Guidelines Section 15074(b), the Advisory Agency found that after consideration of the whole of the administrative record, including the Mitigated Negative Declaration (MND), Case No. ENV-2015-1918-MND, and all comments received, with the imposition of mitigation measures, there is no substantial evidence that the project will have a significant effect on the environment; found that the MND reflects the independent judgement and analysis of the City; found that the mitigation measures have been made enforceable conditions on the project; and adopted the MND and the Mitigation Monitoring Program prepared for the MND. In accordance with provisions of Section 17.03 A and 17.15 of the Los Angeles Municipal Code (LAMC), the Advisory Agency approved Vesting Tentative Tract No. 73531-SL, located generally at 2520 North Eastern Avenue, for a maximum of **42 small lots**, pursuant to LAMC Section 12.22 C.27 (Ordinance No. 176,354), as shown on **revised map stamp-dated March 2, 2020**, in the Northeast Los Angeles Community Plan. This unit density is based on the requested RD5 Zone. (The subdivider is hereby advised that the LAMC may not permit this maximum approved density. Therefore, verification should be obtained from the Department of Building and Safety which will legally interpret the Zoning Code as it applies to this particular property.) The Advisory Agency's approval is subject to the following conditions:

NOTE on clearing conditions: When two or more **agencies** must clear a condition, subdivider should follow the sequence indicated in the condition. For the benefit of the applicant, subdivider shall maintain record of all conditions cleared, including all material supporting clearances and be prepared to present copies of the clearances to each reviewing agency as may be required by its staff at the time of its review.

BUREAU OF ENGINEERING - SPECIFIC CONDITIONS

Any questions regarding this report should be directed to Mr. Georgic Avanesian of the Land Development Section, located at 201 North Figueroa Street, Suite 200, or by calling (213) 808-8588.

1. That a 3-foot wide strip of land be dedicated along Eastern Avenue adjoining the tract to complete a 43-foot wide right-of-way in accordance with Avenue II Standards of LA Mobility Plan including a 20-foot radius property line return or a 15-foot by 15-foot property cut corner at the intersection with Lombardy Boulevard.
2. That the proposed merger area along Lombardy Boulevard not to be merged and existing right-of-way be correctly shown on the final map.
3. That if this tract map is approved as "Small Lot Subdivision" then, and if necessary for street address purposes all the common access to this subdivision be named on the final map satisfactory to the City Engineer.
4. That if this tract map is approved as small lot subdivision then the final map be labeled as "Small Lot Subdivision per Ordinance No. 176354" satisfactory to the City Engineer.
5. That if necessary public sanitary sewer easements be dedicated on the final map based on an alignment approved by the Central District Engineering District Office.
6. That the owners of the property record an agreement satisfactory to the City Engineer that they will provide name signs for the common access driveways.

Note to City Engineer: For Common Access Easement width requirements see Department of City Planning Condition No. 18 (g)(ii) and (iii)

DEPARTMENT OF BUILDING AND SAFETY, GRADING DIVISION

Grading Division approvals are conducted at 221 North Figueroa Street, 12th Floor suite 1200. The approval of this Tract Map shall not be construed as having been based upon a geological investigation such as will authorize the issuance of the building permit of the subject property. Such permits will be issued only at such time as the Department of Building and Safety has received such topographic maps and geological reports as it deems necessary to justify the issuance of such building permits.

7. That prior to issuance of a grading or building permit, or prior to recordation of the final map, the subdivider shall make suitable arrangements to assure compliance, satisfactory to the Department of Building and Safety, Grading Division, with all the requirements and conditions contained in Geology and Soils Report Approval dated October 4, 2019, Log No. 109928 and attached to the case file for Tract No. 73831-SL.

DEPARTMENT OF BUILDING AND SAFETY, ZONING DIVISION

An appointment is required for the issuance of a clearance letter from the Department of Building and Safety. The applicant is asked to contact Eric Wong at (213) 482-6876 to schedule an appointment.

8. That prior to recordation of the final map, the Department of Building and Safety, Zoning Division shall certify that no Building or Zoning Code violations exist on the subject site. In addition, the following items shall be satisfied:
 - a. Provide a copy of [Q] and D condition. Show compliance with the above conditions as applicable or Department of City Planning approval is required.

- b. Provide a copy of APC case APCE-2015-2048-ZC-ZAD. Show compliance with all the conditions/requirements of the APC case as applicable.
- c. Zone Change must be recorded prior to obtaining Zoning clearance.
- d. Show all street dedication as required by Bureau of Engineering and provide net lot area after all dedication. "Area" requirements shall be re-checked as per net lot area after street dedication. Front yard requirements shall be required to comply with current code as measured from new property lines after dedication.
- e. Obtain Bureau of Engineering approval for the proposed street (Lombardy Blvd.) merger.
- f. Lots 1, 38 and 42 do not comply with the minimum 20 ft. front yard setback along Eastern Avenue and Lombardy Boulevard after required street dedication is taken as required for the **[Q]RD5-1D Zone**. Revise the Map to show compliance with the above requirement or obtain approval from the Department of City Planning for the setbacks indicated in the Setback Matrix.
- g. Dimension on the map to provide and maintain a minimum 20 ft. common access for driveway and egress/ingress purposes all the way to the public street. Revise the Map to show compliance with the above requirement or obtain approval from the Department of City Planning.

Notes:

This property is in the RD Zone within the Hillside Area and may require to comply with the Hillside Ordinance (Section 12.21A.17 LAMC).

This property is located in a Liquefaction Zone.

The submitted Map may not comply with the number of guest parking spaces required by the Advisory Agency.

The proposed building plans have not been checked for and shall comply with Building and Zoning Code requirements. With the exception of revised health or safety standards, the subdivider shall have a vested right to proceed with the proposed development in substantial compliance with the ordinances, policies, and standards in effect at the time the subdivision application was deemed complete. Plan check will be required before any construction, occupancy or change of use.

If the proposed development does not comply with the current Zoning Code, all zoning violations shall be indicated on the Map.

Backup space for parking space with less than 26'-8" shall provide sufficient parking stall width and garage door opening width to comply with the current Zoning Code requirement. Comply with the above requirement at the time of Plan Check or obtain City Planning approval.

DEPARTMENT OF TRANSPORTATION

- 9. That the project be subject to any recommendations from the Department of Transportation.

FIRE DEPARTMENT

The applicant is further advised that all subsequent contact regarding these conditions must be with the Hydrant and Access Unit. This would include clarification, verification of condition compliance and plans or building permit applications, etc., and shall be accomplished BY APPOINTMENT ONLY, in order to assure that you receive service with a minimum amount of waiting please call (213) 482-6509. You should advise any consultant representing you of this requirement as well.

10. That prior to the recordation of the final map, a suitable arrangement shall be made satisfactory to the Fire Department, binding the subdivider and all successors to the following:
 - a. Submittal of plot plans for Fire Department review and approval prior to recordation of Tract Map Action.
 - b. This project is located in the very high fire hazard severity zone and shall comply with requirements set forth in the City of Los Angeles Municipal Code 57.25.01.
 - c. Mitigating measures shall be considered. These measures shall include, but not be limited to the following:
 - i. Boxed-in eaves.
 - ii. Single pane, double thickness (minimum 1/8" thickness) or insulated windows.
 - iii. Non-wood siding.
 - iv. Exposed wooden members shall be two inches nominal thickness.
 - v. Noncombustible finishes.
 - d. Any required roadway improvement within the Hillside Ordinance shall be completed prior to the Fire Department signing off and building plans or building permit application.
 - e. Irrigated and managed greenbelts around the perimeter of all structures for a distance of 100 feet shall be considered as a buffer between the brush and the proposed project.
 - f. All landscaping shall use fire-resistant plants and materials. A list of such plants is available from the Fire Department.
 - g. All homes shall have noncombustible roofs. (Non-wood)
 - h. The brush in the area adjacent to the proposed development shall be cleared or thinned periodically by the homeowner's Association under supervision to the Los Angeles City Fire Department in order to reduce the risk of brush fires spreading to the homes.
 - i. Access for Fire Department apparatus and personnel to and into all structures shall be required.
 - j. The entrance or exit of all ground dwelling units shall not be more than 150 feet from the edge of a roadway of an improved street, access road, or designated fire lane.

- k. No building or portion of a building shall be constructed more than 150 feet from the edge of a roadway of an improved street, access road, or designated fire lane.
- l. The width of private roadways for general access use and fire lanes shall not be less than 20 feet, and the fire lane must be clear to the sky.
- m. Adequate public and private fire hydrants shall be required.
- n. Fire lanes, where required and dead ending streets shall terminate in a cul-de-sac or other approved turning area. No dead ending street or fire lane shall be greater than 700 feet in length or secondary access shall be required.
- o. Where access for a given development requires accommodation of Fire Department apparatus, overhead clearance shall not be less than 14 feet.
- p. The Fire Department may require additional vehicular access where buildings exceed 28 feet in height.
- q. On small lot subdivisions, any lots used for access purposes shall be recorded on the final map as a "Fire Lane".
- r. No proposed development utilizing cluster, group, or condominium design of one or two family dwellings shall be more than 150 feet from the edge of the roadway of an improved street, access road, or designated fire lane.
- s. No framing shall be allowed until the roadway is installed to the satisfaction of the Fire Department.
- t. Any required fire hydrants to be installed shall be fully operational and accepted by the Fire Department prior to any building construction.
- u. All parking restrictions for fire lanes shall be posted and/or painted prior to any Temporary Certificate of Occupancy being issued.
- v. Plans showing areas to be posted and/or painted, "FIRE LANE NO PARKING" shall be submitted and approved by the Fire Department prior to building permit application sign-off.
- w. Electric Gates approved by the Fire Department shall be tested by the Fire Department prior to Building and Safety granting a Certificate of Occupancy.
- x. Any roof elevation changes in excess of 3 feet may require the installation of ships ladders.
- y. Adequate off-site public and on-site private fire hydrants may be required. Their number and location to be determined after the Fire Department's review of the plot plan.
- z. The Fire Department may require additional roof access via parapet access roof ladders where buildings exceed 28 feet in height, and when overhead wires or other obstructions block aerial ladder access.

DEPARTMENT OF WATER AND POWER

11. Satisfactory arrangements shall be made with the Los Angeles Department of Water and Power (LADWP) for compliance with LADWP's Water System Rules and requirements. Upon compliance with these conditions and requirements, LADWP's Water Services Organization will forward the necessary clearances to the Bureau of Engineering. (This condition shall be deemed cleared at the time the City Engineer clears Condition No. S-1.(c).)

BUREAU OF STREET LIGHTING – SPECIFIC CONDITIONS

Street Lighting clearance for this Street Light Maintenance Assessment District condition is conducted at 1149 S. Broadway Suite 200. Street Lighting improvement condition clearance will be conducted at the Bureau of Engineering District office, see condition S-3. (c).

12. If new street light(s) are required, then prior to the recordation of the final map or issuance of the Certificate of Occupancy (C of O), street lighting improvement plans shall be submitted for review and the owner shall provide a good faith effort via a ballot process for the formation or annexation of the property within the boundary of the development into a Street Lighting Maintenance Assessment District.

BUREAU OF SANITATION

13. Wastewater Collection Systems Division of the Bureau of Sanitation has inspected the sewer/storm drain lines serving the subject tract and found no potential problems to their structure or potential maintenance problem, as stated in the memo dated November 8, 2016. Upon compliance with its conditions and requirements, the Bureau of Sanitation, Wastewater Collection Systems Division will forward the necessary clearances to the Bureau of Engineering. (This condition shall be deemed cleared at the time the City Engineer clears Condition No. S-1. (d).)

INFORMATION TECHNOLOGY AGENCY

14. To assure that cable television facilities will be installed in the same manner as other required improvements, please email cabletv.ita@lacity.org that provides an automated response with the instructions on how to obtain the Cable TV clearance. The automated response also provides the email address of three people in case the applicant/owner has any additional questions.

DEPARTMENT OF RECREATION AND PARKS

15. That the Quimby fee be based on the RD5 Zone.

URBAN FORESTRY DIVISION AND THE DEPARTMENT OF CITY PLANNING

16. The applicant shall submit a tree report and a landscape plan prepared by a Tree Expert, as required by LAMC Ordinance No. 177,404, for approval by the City Planning Department and the Urban Forestry Division, Bureau of Street Services. The Tree Report shall contain the Tree Expert's recommendation for the preservation of as many desirable (eight inches diameter or greater) trees as possible and shall provide species, health, and condition of all trees with tree locations plotted on a site survey. Any on-site 1:1 tree replacement shall be required for the unavoidable loss of any desirable on-site trees.
17. Plant street trees and remove any existing trees within dedicated streets or proposed dedicated streets as required by the Urban Forestry Division of the Bureau of Street

Services. Parkway tree removals shall be replanted at a 2:1 ratio. All street tree plantings shall be brought up to current standards. When the City has previously been paid for tree planting, the subdivider or contractor shall notify the Urban Forestry Division (213-847-3077) upon completion of construction to expedite tree planting.

Note:

Removal of Protected trees requires the approval of the Board of Public Works.

Removal or planting of any tree in the public right-of-way required approval of the Board of Public Works.

Contact Urban Forestry Division at (213)847-3077 for permit information. CEQA documents must address removal of protected trees and parkway trees.

DEPARTMENT OF CITY PLANNING - SITE SPECIFIC CONDITIONS

Clearances may be conducted at the Figueroa, Valley, or West Los Angeles Development Services Centers. To clear conditions, an appointment is required, and can be requested at planning.lacity.org.

18. Prior to the recordation of the final map, the subdivider shall prepare and execute a Covenant and Agreement (Planning Department General Form CP-6770) in a manner satisfactory to the Planning Department, binding the subdivider and all successors to the following:
 - a. Limit the proposed development to a maximum of 42 residential dwelling units.
 - b. A Certificate of Occupancy (temporary or final) for the building(s) in Vesting Tentative Tract Map No. 73531-SL shall not be issued until after the final map has been recorded.
 - c. Provide a minimum of two (2) covered off-street parking spaces per dwelling unit.
 - d. That prior to issuance of a certificate of occupancy, a minimum 6-foot-high slumpstone or decorative masonry wall shall be constructed adjacent to neighboring residences, if no such wall already exists, except in required front yard.
 - e. That a solar access report shall be submitted to the satisfaction of the Advisory Agency prior to obtaining a grading permit.
 - f. That the subdivider consider the use of natural gas and/or solar energy and consult with the Department of Water and Power and Southern California Gas Company regarding feasible energy conservation measures.
 - g. Note to City Zoning Engineer and Plan Check. The Advisory Agency has considered the following setbacks in conjunction with the approved map. Minor deviations to the map's setbacks are allowed in the event that such deviations are necessary in order to accommodate other conditions of approval as required by other City agencies. In no event shall the setback from the perimeter boundary of the subdivision measure less than the yards required pursuant to LAMC Section 12.22 C.27 (Ordinance No. 176,354):
 - (i) The project shall comply with the setbacks as indicated in the table below:

Setback Matrix									
Lot No.	North	South	East	West	Lot No.	North	South	East	West
1	22[S]	23[S]	4[R]	5[F]	22	55[R]	23[F]	4[S]	4[S]
2	26[F]	16[R]	4[S]	4[S]	23	75[R]	24[F]	4[S]	7[S]
3	26[F]	16[R]	4[S]	4[S]	24	56[F]	1[R]	4[S]	56[F]
4	24[F]	18[R]	4[S]	4[S]	25	3.5[S]	4[S]	27[R]	130[F]
5	26[F]	16[R]	4[S]	4[S]	26	4[S]	4[S]	27[R]	150[F]
6	24[F]	18[R]	6[S]	4[S]	27	4[S]	4[S]	27[R]	170[F]
7	28[F]	19[R]	4[S]	4[S]	28	4[S]	4[S]	27[R]	190[F]
8	28[F]	19[R]	4[S]	4[S]	29	4[S]	4[S]	27[R]	210[F]
9	28[F]	19[R]	4[S]	4[S]	30	15[R]	27[F]	15[S]	3[S]
10	28[F]	12[R]	4[S]	6[S]	31	15[R]	27[F]	3[S]	3[S]
11	11[F]	12[R]	4[S]	4[S]	32	15[R]	27[F]	3[S]	3[S]
12	20[F]	12[R]	18[S]	4[S]	33	15[R]	27[F]	3[S]	3[S]
13	4[S]	1.5[S]	13[R]	25[F]	34	15[R]	27[F]	3[S]	3[S]
14	4[S]	4[S]	14[R]	25[F]	35	15[R]	27[F]	3[S]	3[S]
15	6[S]	4[S]	14[R]	25[F]	36	15[R]	27[F]	3[S]	3[S]
16	4[S]	6[S]	13[R]	25[F]	37	15[R]	27[F]	3[S]	3[S]
17	4[S]	4[S]	13[R]	25[F]	38	22[F]	32[S]	5[R]	6[F]
18	4[S]	4[S]	13[R]	25[F]	39	15[F]	23[R]	3[S]	5[F]
19	9[S]	4[S]	13[R]	25[F]	40	25[F]	30[R]	3[S]	3[S]
20	36[R]	5[F]	14[S]	7[S]	41	25[F]	30[R]	4[S]	3[S]
21	35[R]	29[F]	4[S]	4[S]	42	7[F]	62[R]	23[S]	15[S]

- (ii) The common access easement which provides access from Eastern Avenue shall maintain a minimum width of 28 feet clear to the sky.
 - (iii) The common access easement which provides access from Lombardy Boulevard shall maintain a minimum width of 20 feet clear to the sky.
- h. A Community Maintenance Agreement shall be prepared, composed of all property owners, to maintain all common areas such as trees, landscaping, trash, parking, community driveway, walkways, monthly service for private fire hydrant (if required), etc. Each owner and future property owners shall automatically become party to the agreement and shall be subject to a proportionate share of the maintenance. The Community Maintenance Agreement shall be recorded as a Covenant and Agreement to run with the land. The subdivider shall submit a copy of this Agreement, once recorded, to the Planning Department for placement in the parcel map file.
- i. That copies of all recorded Covenant and Agreement(s) for all reciprocal private easements shall be submitted to the Planning Department for placement in the parcel map file.
- j. INDEMNIFICATION AND REIMBURSEMENT OF LITIGATION COSTS.

Applicant shall do all of the following:

- (i) Defend, indemnify and hold harmless the City from any and all actions against the City relating to or arising out of, in whole or in part, the City's processing and approval of this entitlement, including but not limited to, an action to attack, challenge, set aside, void or otherwise modify or annul the approval of the entitlement, the environmental review of the entitlement, or

- the approval of subsequent permit decisions, or to claim personal property damage, including from inverse condemnation or any other constitutional claim.
- (ii) Reimburse the City for any and all costs incurred in defense of an action related to or arising out of, in whole or in part, the City's processing and approval of the entitlement, including but not limited to payment of all court costs and attorney's fees, costs of any judgments or awards against the City (including an award of attorney's fees), damages, and/or settlement costs.
 - (iii) Submit an initial deposit for the City's litigation costs to the City within 10 days' notice of the City tendering defense to the Applicant and requesting a deposit. The initial deposit shall be in an amount set by the City Attorney's Office, in its sole discretion, based on the nature and scope of action, but in no event shall the initial deposit be less than \$50,000. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
 - (iv) Submit supplemental deposits upon notice by the City. Supplemental deposits may be required in an increased amount from the initial deposit if found necessary by the City to protect the City's interests. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
 - (v) If the City determines it necessary to protect the City's interest, execute an indemnity and reimbursement agreement with the City under terms consistent with the requirements of this condition.

The City shall notify the applicant within a reasonable period of time of its receipt of any action and the City shall cooperate in the defense. If the City fails to notify the applicant of any claim, action, or proceeding in a reasonable time, or if the City fails to reasonably cooperate in the defense, the applicant shall not thereafter be responsible to defend, indemnify or hold harmless the City.

The City shall have the sole right to choose its counsel, including the City Attorney's office or outside counsel. At its sole discretion, the City may participate at its own expense in the defense of any action, but such participation shall not relieve the applicant of any obligation imposed by this condition. In the event the Applicant fails to comply with this condition, in whole or in part, the City may withdraw its defense of the action, void its approval of the entitlement, or take any other action. The City retains the right to make all decisions with respect to its representations in any legal proceeding, including its inherent right to abandon or settle litigation.

For purposes of this condition, the following definitions apply:

"City" shall be defined to include the City, its agents, officers, boards, commissions, committees, employees, and volunteers.

"Action" shall be defined to include suits, proceedings (including those held under alternative dispute resolution procedures), claims, or lawsuits. Action includes actions, as defined herein, alleging failure to comply with any federal, state or local law.

Nothing in the definitions included in this paragraph are intended to limit the rights

of the City or the obligations of the Applicant otherwise created by this condition.

19. That prior to the issuance of the building permit or the recordation of the final map, a copy of the Case No. APCE-2015-2048-ZC-ZAD-ZAA shall be submitted to the satisfaction of the Advisory Agency. In the event that Case No. APCE-2015-2048-ZC-ZAD-ZAA is not approved, the subdivider shall submit a tract modification.

20. **Haul Route.** Prior to the issuance of a grading permit, the subdivider shall record and execute a Covenant and Agreement (Planning Department General Form CP-6770), binding the subdivider to the following haul route conditions:

a. Streets to be used are limited to:

(i) Loaded:

- (a) Exit job site on Eastern Avenue Southbound
- (b) Left turn onto Valley Boulevard Eastbound
- (c) Right turn onto Southbound Long Beach Freeway I-710 South to dump site

(ii) Unloaded:

- (a) Northbound Long Beach Freeway I-710 North
- (b) Exit at end of freeway and continue onto Valley Boulevard
- (c) Left turn onto Valley Boulevard Westbound
- (d) Right turn onto Eastern Avenue Northbound to job site

(iii) Staging: Hauling trucks shall be staged on the job site whenever possible.

NOTE: No interference to traffic; access to driveways must be maintained at all times.

b. Hours of operation are restricted to the hours between 9:00 a.m. and 3:30 p.m. on Mondays through Fridays and Saturdays from 8:00 a.m. to 6:00 p.m. No hauling shall be performed on Sundays and holidays.

c. Department of Transportation.

- (i) The contractor shall contact LADOT at (213)485-2298 at least five business days prior to hauling to post "Temporary Tow Away No Stopping" signs in front of the jobsite on Eastern Avenue for hauling if necessary.
- (ii) Flagger control should be provided during the hauling operations to assist with ingress and egress of truck traffic on Eastern Avenue.

Note: Questions related to LADOT questions, please call Joan Hsu at (213)928-9728.

d. Bureau of Street Services.

- (i) Required Permit Fee and Bond. Permit Fee must be paid before the Department of Building and Safety will issue a Grading Permit.

(a) Under the provisions of Section 62.201 of the Los Angeles Municipal

Code, the following permit fee shall be required:

- (1) A total of 78,000 cubic yards of material moved 2.1 miles within the hillside at a rate of \$0.29 per cubic yard per mile would exceed the maximum chargeable under the Ordinance. Therefore, the maximum fee chargeable, \$3000.00 shall be due.
- (2) The Minimum permit fee of \$150.00 is required for the (import/export).
- (b) The required permit fee shall be paid at the Street Services Investigation and Enforcement Division office, 1149 South Broadway, Suite 350, Los Angeles, CA 90015, telephone (213) 847-6000.
- (c) Under the provisions of Section 62.202 of the Los Angeles Municipal Code, a cash bond or surety bond in the amount of \$50,000.00 shall be required from the property owner to cover any road damage and/or street cleaning costs resulting from the hauling activity.
- (d) Forms for the bond will be issued by Bond Control, Bureau of Engineering Valley District Office, 6262 Van Nuys Boulevard, Suite 251, Van Nuys, CA 91401, telephone (818) 374-5090.
- (ii) Special Conditions. An authorized Public Officer may make additions to, or modifications of, the following conditions if necessary to protect the health, safety, and welfare of the general public.
 - (a) The hauling operations are restricted to the hours between 9 a.m. and 3:30 p.m. on Mondays through Fridays, and Saturdays from 8 a.m. to 6 p.m. No hauling shall be performed on Sundays, and holidays.
 - (b) The vehicles used for hauling shall be Double Bottom Dump trucks.
 - (c) All trucks are to be cleaned of loose earth at the export site to prevent spilling. The contractor shall remove any material spilled onto the public street.
 - (d) All trucks are to be watered at the export site to prevent excessive blowing of dirt.
 - (e) The applicant shall comply with the State of California, Department of Transportation policy regarding movement of reducible loads.
 - (f) Total amount of dirt to be hauled shall not exceed 78,000 cubic yards.
 - (g) "Truck Crossing" warning signs shall be placed 300 feet in advance of the exit in each direction.
 - (h) Flagpersons shall be required at the job site to assist the trucks in and out of the project area. Flagpersons and warning signs shall be in compliance with Part II of the latest Edition of "Work Area Traffic Control Handbook."
 - (i) The permittee shall comply with all regulations set forth by the State of

California, Department of Motor Vehicles pertaining to the hauling of earth.

- (j) The City of Los Angeles, Department of Transportation, telephone (213) 485-2298, shall be notified 72 hours prior to beginning operations in order to have temporary "No Parking" signs posted along streets in haul route.
- (k) A copy of the approval letter from the City, the approved haul route and the approved grading plans shall be available on the job site at all times.
- (l) Any change to the prescribed routes, staging and/or hours of operation must be approved by the concerned governmental agencies. Contact the Street Services Investigation and Enforcement Division at (213) 847-6000 prior to effecting any change.
- (m) The permittee shall notify the Street Services Investigation and Enforcement Division at (213) 847-6000 at least 72 hours prior to the beginning of hauling operations and shall notify the Division immediately upon completion of hauling operations.
- (n) The application shall expire eighteen months after the date of the Board the Department of City Planning approval. The permit fee shall be paid to the Street Services Investigation and Enforcement Division prior to the commencement of hauling operations.

DEPARTMENT OF CITY PLANNING-ENVIRONMENTAL MITIGATION MEASURES

- 21. That prior to recordation of the final map, the subdivider shall prepare and execute a Covenant and Agreement (Planning Department General Form CP-6770 and Exhibit CP-6770. M) in a manner satisfactory to the Planning Department requiring the subdivider to identify (a) mitigation monitor(s) who shall provide periodic status reports on the implementation of mitigation items required by Mitigation Condition No(s). 22 and 23 of the Tract's approval satisfactory to the Advisory Agency. The mitigation monitor(s) shall be identified as to their areas of responsibility, and phase of intervention (pre-construction, construction, postconstruction/maintenance) to ensure continued implementation of the above mentioned mitigation items.
- 22. Prior to the recordation of the final map, the subdivider will prepare and execute a Covenant and Agreement (Planning Department General Form CP-6770) in a manner satisfactory to the Planning Department, binding the subdivider and all successors to the following:

MM-1. Aesthetics.

a. Non-Protected Trees.

- i. Prior to the issuance of any permit related to development of the Project, a plot plan shall be prepared for the Project, indicating the location, size, type, and general condition of all existing trees on the Project Site and within the adjacent public right(s)-of-way.
- ii. All significant (8-inch or greater trunk diameter, or cumulative truck diameter if multi-trunked, as measured 54 inches above the ground) non-

protected trees on the Project Site proposed for removal shall be replaced at a 1:1 ratio with a minimum 240inch box tree. Net new trees located within the parkway of the adjacent public right(s)-of-way may be counted toward replacement tree requirements.

- iii. Removal of planting of any tree in the public right-of-way shall require approval of the Board of Public Works. All trees in the public right-of-way shall be provided in the current standards of the Urban Forestry Division of the Department of Public Works, Bureau of Street Services.

b. Protected Trees.

- i. All protected tree removals shall require approval from the Board of Public Works.
- ii. A Tree Report shall be submitted to the Urban Forestry Division of the Bureau of Street Services, Department of Public Works, for review and approval prior to implementation of the Report's recommended measures.
- iii. According to the City's Protected Tree Ordinance, a minimum of four protected trees (a minimum of 15 gallon in size) shall be planted for each protected tree that is removed. The size of each replacement tree shall measure at least one inch or more in diameter at a point one foot above the base, and not less than seven feet in height, measured from the base.
- iv. In consultation with the Division of Urban Forestry, twenty five-percent of the Protected trees removed shall be replaced with 15 gallon Juglans Californica.
- v. The location of trees planted for the purpose of replacing a removed protected tree shall be clearly indicated on the required landscape plan, which shall also indicate the replacement tree species and further contain the phrase "Replacement Tree" in its description.

- c. Previously Removed Trees. The 8 qualifying removed protected trees shall be replaced at a ratio of one Juglans Californica and three Quercus for each of the 8 trees, at a minimum of 15 gallons in size. The specific size and species of the trees to be planted as replacement for the protected trees being removed shall determined by the Urban Forestry Division.

d. All Trees.

- i. Protected Barrier: A protection barrier shall be installed around the construction area as shown on the map included in the Tree Preservation Report (refer to Appendix A). The barrier shall be 6-foot-high chain link fencing. Twelve-inch-high slit fence shall be attached to the base of the fence with the bottom edge buried 1-2 inches. The barrier may be placed on the line shwn on the map or closer to the construction, but not further. The fencing shall not be removed, relocated, or encroached upon without permission of the arborist involved.
- ii. Storage of materials: There shall be no storage of materials or supplies of any kind inside the area of the protection fencing. Concrete and cement materials, block, sand and soil shall not be placed within the drip-line of any

tree to remain.

- iii. Fuel Storage: Fuel Storage shall not be permitted within 150 feet of any tree to be preserved. Refueling servicing and maintenance of equipment and machinery shall not be permitted within 150 feet of protected trees.
- iv. Debris and Waste Materials: Debris and waste from construction of other activities shall not be permitted outside the construction area. Wash down of concrete or cement handling equipment, in particular, shall not be permitted within 150 feet of protected trees.
- v. Planting near Trees Designated for Protection: Any digging within designated protection zones shall done using supersonic air directly as the digging medium, by means of a nozzle, whose nominal rated input pressure (available from manufacture's literature) must not exceed 130 psig (pounds per square inch at gage) unless otherwise approved. Nozzles designed for input above 130 psig can damage fine roots. Air compressors rated between 100 to 125 psig recommended.
- vi. Grade Changes: Any grade changes within the protection radius listed should be approved by a Registered Consulting Arborist before construction begins, and precautions taken to mitigate potential injuries. Grade changes can be particularly damaging to trees. Even as little as two inches of fill can cause the death of a tree. Lowering the grade can destroy major portions of a root system.
- vii. Damages: Any tree damages or injuries should be reported to the project arborist as soon as possible. Severed roots shall be cut cleanly to healthy tissue, using proper pruning tools. Broken branches or limbs shall be pruned according to International Society of Arboriculture Pruning Guidelines and ANSI A-300 Pruning Standards.
- viii. Preventing Measures: Pruning of tree canopies and branches should be done at the direction of the project arborist to remove any dead or broken branches, and to provide any necessary clearances for the construction work or equipment.

MM-2. Biological Resources.

- a. To avoid potential significant impacts to roosting bats, one of the following shall be implemented by the Project Applicant:
 - 1. Tree removal shall occur between March 1st and July 31st or
 - 2. If tree removal would occur between August 1st and February 28th/29th, the Project Applicant shall retain a qualified bat biologist to conduct a roosting bat survey. If evidence of bats is present, then removal of occupied roost trees shall not occur until the biologist determines that the roost is no longer in use through repeated nocturnal surveys. The results of the survey(s) shall be provided to the Department of Public Works prior to removal of any protected trees.
- b. To avoid potential significant impacts to nesting birds, including migratory

birds and raptors, one of the following shall be implemented by the Project Applicant:

- (1) Conduct vegetation removal associated with construction from September 1st through January 31st, when birds are not nesting. Initiate grading activities prior to the breeding season (which is generally February 1st through August 31st) and keep disturbance activities constant throughout the breeding season to prevent birds from establishing nests in surrounding habitat (in order to avoid possible nest abandonment); if there is a lapse in activities of more than five days, pre-construction surveys shall be necessary as described in the bullet below, or
- (2) Conduct pre-construction surveys for nesting birds if vegetation removal or grading is initiated during the nesting season. A qualified wildlife biologist shall conduct weekly pre-construction bird surveys no more than 30 days prior to initiation of grading to provide confirmation on the presence or absence of active nests in the vicinity (at least 300 to 500 feet around the individual construction site, as access allows). The last survey should be conducted no more than three days prior to the initiation of clearance/construction work. If active nests are encountered, clearing and construction in the vicinity of the nests shall be deferred until the young birds have fledged and there is no evidence of a second attempt at nesting. A minimum buffer of 300 feet (500 feet for raptor nests) or as determined by a qualified biologist shall be maintained during construction depending on the species and location. The perimeter of the nest-setback zone shall be fenced or adequately demarcated with staked flagging at 20-foot intervals, and construction personnel and activities restricted from the area. Construction personnel should be instructed on the sensitivity of the area. A survey report by the qualified biologist documenting and verifying compliance with the mitigation and with applicable state and federal regulations protecting birds shall be submitted to the City and County, depending on within which jurisdiction the construction activity is occurring. The qualified biologist shall serve as a construction monitor during those periods when construction activities would occur near active nest areas to ensure that no inadvertent impacts on these nests would occur.

23. **Construction Mitigation Conditions** - Prior to the issuance of a grading or building permit, or the recordation of the final map, the subdivider shall prepare and execute a Covenant and Agreement (Planning Department General Form CP-6770) in a manner satisfactory to the Planning Department, binding the subdivider and all successors to the following:

CM-1. That a sign be required on site clearly stating a contact/complaint telephone number that provides contact to a live voice, not a recording or voice mail, during all hours of construction, the construction site address, and the tract map number. YOU ARE REQUIRED TO POST THE SIGN 7 DAYS BEFORE CONSTRUCTION IS TO BEGIN.

- Locate the sign in a conspicuous place on the subject site or structure (if developed) so that it can be easily read by the public. The sign must be

sturdily attached to a wooden post if it will be free-standing.

- Regardless of who posts the site, it is always the responsibility of the applicant to assure that the notice is firmly attached, legible, and remains in that condition throughout the entire construction period.
- If the case involves more than one street frontage, post a sign on each street frontage involved. If a site exceeds five (5) acres in size, a separate notice of posting will be required for each five (5) acres or portion thereof. Each sign must be posted in a prominent location.

CM-2. Air Quality.

- a. All off-road construction equipment greater than 50 hp shall meet U.S. EPA Tier 4 emission standards, where available, to reduce NO_x, PM₁₀, PM_{2.5} emissions at the Project Site. In addition, all construction equipment shall be outfitted with Best Available Control Technology devices certified by CARB. Any emissions control device used by the contractor shall achieve emissions reductions that are no less than what could be achieved by a Level 3 diesel emissions control strategy for a similarly sized engine as defined by CARB regulations.
- b. Require the use of 2010 and newer diesel haul trucks (e.g., material delivery trucks and soil import/export) and if the Lead Agency determines that 2010 model year or newer diesel trucks cannot be obtained, the Lead Agency shall require trucks that meet U.S. EPA 2007 model year NO_x emissions requirements.
- c. At the time of mobilization of each applicable unit of equipment, a copy of each unit's certified tier specification, BACT documentation, and CARB or SCAQMD operating permit shall be provided.
- d. Encourage construction contractors to apply for SCAQMD "SOON" funds. Incentives could be provided for those construction contractors who apply for SCAQMD "SOON" funds. The "SOON" program provides funds to accelerate clean up of off-road diesel vehicles, such as heavy duty construction equipment. More information on this program can be found at: <http://www.aqmd.gov/home/programs/business/business-detail?title=off-road-dieselelengines&parent=vehicle-engine-upgrades>
- e. Construction activities shall comply with SCAQMD Rule 403, including the following measures:
 - (1) Apply water to disturbed areas of the site three times a day.
 - (2) Require the use of a gravel apron or other equivalent methods to reduce mud and dirt trackout onto truck exit routes.
 - (3) Appoint a construction relations officer to act as a community liaison concerning on-site construction activity including resolution of issues related to PM generation.
 - (4) Limit soil disturbance to the amounts analyzed in the Final MND.

- (5) All materials transported off-site shall be securely covered.
- (6) Apply non-toxic soil stabilizers according to manufacturers' specifications to all inactive construction areas (previously graded areas inactive for ten days or more).
- (7) Traffic speeds on all unpaved roads to be reduced to 15 mph or less.

CM-3 Noise.

- a. The Project shall comply with the City of Los Angeles Building Regulations Ordinance No. 178048, which requires a construction site notice to be provided that includes the following information: job site address, permit number, name and phone number of the contractor and owner or owner's agent, hours of construction allowed by code or any discretionary approval for the site, and City telephone numbers where violations can be reported. The notice shall be posted and maintained at the construction site prior to the start of construction and displayed in a location that is readily visible to the public.
- b. Two weeks prior to commencement of construction, notification shall be provided to the off-site residential and school uses within 500 feet of the Project site that discloses the construction schedule, including the types of activities and equipment that would be used throughout the duration of the construction period.
- c. Temporary sound barriers, capable of achieving a sound attenuation of at least 10 dBA (e.g., construction sound wall with sound blankets), and capable of blocking the line-of-sight to the adjacent residences shall be installed as feasible.
- d. Noise-generating construction equipment operated at the Project Site shall be equipped with effective state-of-the-art noise control devices, i.e., mufflers, lagging, solar power or electric plug-in on-site power generators and/or motor enclosures or other shielding equipment. All equipment shall be properly maintained to assure that no additional noise, due to worn or improperly maintained parts, would be generated.
- e. All construction areas for staging and warming-up equipment shall be located as far as possible from adjacent residences.
- f. Portable noise sheds for smaller, noisy equipment, such as air compressors, dewatering pumps, and generators shall be provided where feasible.
- b. A haul route for exporting cut materials from the site to a nearby landfill that access the San Bernardino and/or Long Beach Freeways should minimize travel on residential streets with sensitive receptors.

CM-4. Transportation/Traffic.

- a. Hillside Construction Staging and Parking Plan. Prior to the issuance of a grading or building permit, the applicant shall submit a Construction

Staging and Parking Plan to the Department of Building and Safety and the Fire Department for review and approval. The plan shall identify where all construction materials, equipment, and vehicles will be stored through the construction phase of the project, as well as where contractor, subcontractor, and laborers will park their vehicles so as to prevent blockage of two-way traffic on streets in the vicinity of the construction site. The Construction Staging and Parking Plan shall include, but not be limited to, the following:

- (1) No construction equipment or material shall be permitted to be stored within the public right-of-way.
- (2) If the property fronts on a designated Red Flag Street, on noticed "Red Flag" days, all the workers shall be shuttled from an off-site area, located on a non-Red Flag Street, to and from the site in order to keep roads open on Red Flag days.
- (3) During the Excavation and Grading phases, all haul trucks shall be staged on the Project site. The drivers shall be required to follow the designated travel plan or approved Haul Route.
- (4) Truck traffic directed to the project site for the purpose of delivering materials, construction-machinery, or removal of graded soil shall be limited to off-peak traffic hours, Monday through Friday only. No truck deliveries shall be permitted on Saturdays or Sundays.
- (5) All deliveries during construction shall be coordinated so that all vendor/delivery vehicles will stage and make deliveries on the project site, and that a construction supervisor is present at such time.
- (6) A radio operator shall be on-site to coordinate the movement of material and personnel, in order to keep the roads open for emergency vehicles, their apparatus, and neighbors.
- (7) During all phases of construction, all construction vehicle parking and queuing related to the project shall be as required to the satisfaction of the Department of Building and Safety, and in substantial compliance with the Construction Staging and Parking Plan, except as may be modified by the Department of Building and Safety or the Fire Department.

- b. Construction Activity Near Schools. The Project developer and contractors shall maintain ongoing contact with administrators of the Farmdale Elementary School and the El Sereno Middle School. The administrative offices shall be contacted when demolition, grading and construction activity begin on the Project site so that students and their parents will know when such activities are to occur. The developer shall obtain school walk and bus routes to the schools from either the administrators or from the LAUSD's Transportation Branch (323) 342-1400 and guarantee that safe and convenient pedestrian and bus routes to the school be maintained.

- (1) The developer shall install appropriate traffic signs around the site to ensure pedestrian and vehicle safety.
- (2) There shall be no staging or parking of construction vehicles, including vehicles to transport workers on any of the streets adjacent to the school.
- (3) Due to noise impacts on the schools, no construction vehicles or haul trucks shall be staged or idled on these streets during school hours.

c. Schools affected by Haul Route.

- (1) LADBS shall assign specific haul route hours of operation based upon Farmdale Elementary School and El Sereno Middle School hours of operation.
- (2) Haul route scheduling shall be sequenced to minimize conflicts with pedestrians, school buses and cars at the arrival and dismissal times of the school day. Haul route trucks shall not be routed past the school during periods when school is in session especially when students are arriving or departing from the campus.

d. Good Neighbor Construction Practices.

- (1) Whenever possible, construction vehicles should be parked on site to prevent congestion on streets with limited parking.
- (2) When temporarily blocking portions of streets for deliveries of construction materials, a flag person shall be provided to assist with pedestrian and vehicular traffic.
- (3) Street closures shall not take place during peak traffic hours. Any street, sidewalk, or other improvement work shall be conducted in conformance with the latest Manual on Work Area Traffic Control.
- (4) Care shall be taken not to overfill concrete trucks during deliveries. If spills occur, it shall be the responsibility of the concrete company to immediately provide clean up.
- (5) Construction noise shall be kept to a minimum with consideration of the surrounding neighbors. Unnecessary noise such as music shall be kept below legal levels.
- (6) Streets and sidewalks adjacent to construction sites shall be swept free of construction debris at all times.
- (7) Care shall be taken to not interfere with trash pick-up by the Bureau of Sanitation. Construction and delivery vehicles shall be subject to trash pick-up parking restrictions.

- (8) If building materials are to be stored in public right of ways, it shall be by permit from the Department of Public Works, Bureau of Street Services, Investigations and Enforcement Division and shall conform with all applicable rules.
- (9) All construction/demolition activities shall comply with the construction hours in Section 41.40 of the LAMC.
- e. The Project Applicant shall plan construction and construction staging as to maintain pedestrian access to adjacent active land uses throughout all construction phases. This requires the Applicant to maintain adequate and safe pedestrian protection, including physical separation from workspace and vehicular traffic and overhead protection, due to sidewalk closure or blockage, at all times. Barriers, such as K-Rails, scaffolding, etc., shall be maintained at a height of 8 feet.

DEPARTMENT OF CITY PLANNING - STANDARD SMALL LOT CONDITIONS

SL-1. That approval of this parcel map constitutes approval of model home uses, including a sales office and off-street parking. If models are constructed under this parcel map approval, the following conditions shall apply:

- 1. Prior to recordation of the final map, the subdivider shall submit a plot plan for approval by the Development Services Center of the Department of City Planning showing the location of the model dwellings, sales office and off-street parking. The sales office must be within one of the model buildings.
- 2. All other conditions applying to Model Dwellings under Section 12.22 A.10 and 11 and Section 17.05 O of the Code shall be fully complied with satisfactory to the Department of Building and Safety.

SL-2. That a landscape plan, prepared by a licensed landscape architect, be submitted to and approved by the Advisory Agency in accordance with CP-6730 prior to obtaining any grading or building permits before the recordation of the final map. The landscape plan shall identify tree replacement on a 1:1 basis by a minimum of 24-inch box trees for the unavoidable loss of desirable trees on the site.

In the event the subdivider decides not to request a permit before the recordation of the final map, a covenant and agreement satisfactory to the Advisory Agency guaranteeing the submission of such plan before obtaining any permit shall be recorded.

BUREAU OF ENGINEERING - STANDARD CONDITIONS

- S-1. (a) That the sewerage facilities charge be deposited prior to recordation of the final map over all of the tract in conformance with Section 64.11.2 of the Los Angeles Municipal Code (LAMC).
- (b) That survey boundary monuments be established in the field in a manner satisfactory to the City Engineer and located within the California Coordinate System prior to recordation of the final map. Any alternative measure approved by the City Engineer would require prior submission of complete field notes in support

of the boundary survey.

- (c) That satisfactory arrangements be made with both the Water System and the Power System of the Department of Water and Power with respect to water mains, fire hydrants, service connections and public utility easements.
 - (d) That any necessary sewer, street, drainage and street lighting easements be dedicated. In the event it is necessary to obtain off-site easements by separate instruments, records of the Bureau of Right-of-Way and Land shall verify that such easements have been obtained. The above requirements do not apply to easements of off-site sewers to be provided by the City.
 - (e) That drainage matters be taken care of satisfactory to the City Engineer.
 - (f) That satisfactory street, sewer and drainage plans and profiles as required, together with a lot grading plan of the tract and any necessary topography of adjoining areas be submitted to the City Engineer.
 - (g) That any required slope easements be dedicated by the final map.
 - (h) That each lot in the tract complies with the width and area requirements of the Zoning Ordinance.
 - (i) That 1-foot future streets and/or alleys be shown along the outside of incomplete public dedications and across the termini of all dedications abutting unsubdivided property. The 1-foot dedications on the map shall include a restriction against their use of access purposes until such time as they are accepted for public use.
 - (j) That any 1-foot future street and/or alley adjoining the tract be dedicated for public use by the tract, or that a suitable resolution of acceptance be transmitted to the City Council with the final map.
 - (k) That no public street grade exceeds 15%.
 - (l) That any necessary additional street dedications be provided to comply with the Americans with Disabilities Act (ADA) of 2010.
- S-2. That the following provisions be accomplished in conformity with the improvements constructed herein:
- (a) Survey monuments shall be placed and permanently referenced to the satisfaction of the City Engineer. A set of approved field notes shall be furnished, or such work shall be suitably guaranteed, except where the setting of boundary monuments requires that other procedures be followed.
 - (b) Make satisfactory arrangements with the Department of Traffic with respect to street name, warning, regulatory and guide signs.
 - (c) All grading done on private property outside the tract boundaries in connection with public improvements shall be performed within dedicated slope easements or by grants of satisfactory rights of entry by the affected property owners.
 - (d) All improvements within public streets, private streets, alleys and easements shall be constructed under permit in conformity with plans and specifications approved

by the Bureau of Engineering.

- (e) Any required bonded sewer fees shall be paid prior to recordation of the final map.

S-3. That the following improvements are either constructed prior to recordation of the final map or that the construction is suitably guaranteed:

- (a) Construct on-site sewers to serve the tract as determined by the City Engineer.
- (b) Construct any necessary drainage facilities.
- (c) No street lighting improvements if no street widening per BOE improvement conditions. Otherwise relocate and upgrade street lights; nine (9) on Eastern Avenue and one (1) on Lombardy Boulevard

Notes:

The quantity of street lights identified may be modified slightly during the plan check process based on illumination calculations and equipment selection.

Conditions set: 1) in compliance with a Specific Plan, 2) by LADOT, or 3) by other legal instrument excluding the Bureau of Engineering condition S-3 (i), requiring an improvement that will change the geometrics of the public roadway or driveway apron may require additional or the reconstruction of street lighting improvements as part of that condition.

- (d) Plant street trees and remove any existing trees within dedicated streets or proposed dedicated streets as required by the Street Tree Division of the Bureau of Street Maintenance. All street tree plantings shall be brought up to current standards. When the City has previously been paid for tree planting, the subdivider or contractor shall notify the Urban Forestry Division ((213) 847-3077) upon completion of construction to expedite tree planting.
- (e) Repair or replace any off-grade or broken curb, gutter and sidewalk satisfactory to the City Engineer.
- (f) Construct access ramps for the handicapped as required by the City Engineer.
- (g) Close any unused driveways satisfactory to the City Engineer.
- (h) Construct any necessary additional street improvements to comply with the 2010 Americans with Disabilities Act (ADA) Standards for Accessible Design.
- (i) That the following improvements are either constructed prior to recordation of the final map or that the construction is suitably guaranteed:
 - (1) Improve Eastern Avenue being dedicated and adjoining the subdivision by the construction of a new concrete curb 2-foot wide concrete gutter and a full-width concrete sidewalks with tree wells including any necessary removal and reconstruction of existing improvements.
 - (2) Improve Lombardy Boulevard by reconstruction of the existing concrete curb and gutter and reconstruct the existing 5-foot sidewalk and landscaping of the parkway necessary removal and reconstruction of

existing improvements.

- (3) Construct the necessary on-site mainline sewers satisfactory to the City Engineer.
- (4) Construct any necessary retaining walls satisfactory to the City Engineer.

NOTES:

The Advisory Agency approval is the maximum number of units permitted under the tract action. However the existing or proposed zoning may not permit this number of units. This vesting map does not constitute approval of any variations from the Municipal Code, unless approved specifically for this project under separate conditions.

Any removal of the existing street trees shall require Board of Public Works approval.

Satisfactory arrangements shall be made with the Los Angeles Department of Water and Power, Power System, to pay for removal, relocation, replacement or adjustment of power facilities due to this development. The subdivider must make arrangements for the underground installation of all new utility lines in conformance with Section 17.05-N of the Los Angeles Municipal Code (LAMC).

The final map must be recorded within 36 months of this approval, unless a time extension is granted before the end of such period.

The Advisory Agency hereby finds that this tract conforms to the California Water Code, as required by the Subdivision Map Act.

The subdivider should consult the Department of Water and Power to obtain energy saving design features which can be incorporated into the final building plans for the subject development. As part of the Total Energy Management Program of the Department of Water and Power, this no-cost consultation service will be provided to the subdivider upon his request.

FINDINGS OF FACT (CEQA)

A Mitigated Negative Declaration (MND), Case No. ENV-2015-1918-MND, was prepared for the project and published for re-circulated on Thursday, April 6, 2017. The analysis in the MND found that potential negative impacts could occur from the project's implementation due to Aesthetics, Air Quality, Biological Resources, and Transportation/Traffic and that those impacts could be mitigated to a less than significant level with the implementation of the identified mitigations. The MND found that there were no potentially significant impacts that could not be mitigated to a less than significant level.

The Advisory Agency found, pursuant to CEQA Guidelines Section 15074(b), after consideration of the whole of the administrative record, including the MND, Case No. ENV-2015-1918-MND, that there were no potentially significant impacts that could not be mitigated to less than significant impact. The Advisory Agency, adopts the MND and the Mitigation Monitoring Program for the MND and finds that it reflects the independent judgment of the lead agency and determined that this project would not have a significant effect upon the environment provided the potential impacts identified in the MND are mitigated to a less than significant level through implementation of Condition No(s). 22 and 23 of the Tract's approval. Other identified potential impacts not mitigated by these conditions are mandatorily subject to existing City ordinances, (Sewer Ordinance, Grading Ordinance, Flood Plain Management Specific Plan, Xeriscape Ordinance, etc.) which are specifically intended to mitigate such potential impacts on all projects.

In accordance with Section 21081.6 of the Public Resources Code (AB3180), the Deputy Advisory Agency has assured that the above identified mitigation measures will be implemented by requiring reporting and monitoring as specified in Condition No. 21.

FINDINGS OF FACT (SUBDIVISION MAP ACT)

In connection with the approval of Vesting Tentative Tract No. 73531-SL the Advisory Agency of the City of Los Angeles, pursuant to Sections 66473.1, 66474.60, .61 and .63 of the State of California Government Code (the Subdivision Map Act), makes the prescribed findings as follows:

(a) **THE PROPOSED MAP WILL BE/IS CONSISTENT WITH APPLICABLE GENERAL AND SPECIFIC PLANS.**

The Land Use Element of the General Plan consists of the 35 Community Plans within the City of Los Angeles. The Community Plans establish goals, objectives, and policies for future developments at a neighborhood level. Additionally, through the Land Use Map, the Community Plan designates parcels with a land use designation and zone. The Land Use Element is further implemented through the Los Angeles Municipal Code (LAMC). The zoning regulations contained within the LAMC regulates, but is not limited to, the maximum permitted density, height, parking, and the subdivision of land.

The subdivision of land is regulated pursuant to Article 7 of the LAMC. Specifically, Section 17.05 C requires that the vesting tentative tract map be designed in compliance with the zoning regulations applicable to the project site. The project site is located within the Northeast Los Angeles Community Plan, which designates the site with a Low Residential land use designation. The land use designation lists the RE9, RS, R1, RU, RD6, and RD5 Zones as the corresponding zones. The Project Site is zoned [Q]R1-1D and [Q]RD6-1D, which is consistent with the land use designation. The site is subject to Qualified "Q" Conditions and Development "D" Limitations contained within Ordinance No. 180,403, which contain regulations related to the development of residential uses on parcels located within the Community Plan area. The tract map is to merge and re-subdivide the lot as a small lot subdivision pursuant to LAMC Section 12.22 C.27 as adopted pursuant to Ordinance No. 176,354. As small lot subdivisions are not permitted within the R1 Zone, the applicant has requested a zone change, incidental Case No. APCE-2015-2048-ZC-ZAD-ZAA, from [Q]R1-1D and [Q]RD6-1D to [Q]RD5-1D. The adoption of the zone change would permit the subdivision of the lot as a small lot subdivision as shown in the revised tract map stamped dated November 1, 2016.

As zoned, approximately 56,229 square feet of the site is zoned [Q]R1-1, which would permit the creation of up to eleven (11) lots with a minimum lot area of 5,000 square feet to be created through the approval and recordation of a tract map. The remaining 162,041 square feet of lot area, which is zoned [Q]RD6-1D, would permit the development of up to 27 dwelling units. Combined, the site would permit up to 38 dwelling units as currently zoned. The requested RD5 Zone is consistent with the Low Residential land use designation and would permit a maximum of 43 dwelling units, for a net increase in the maximum permitted density of five (5) dwelling units. As the tract map is for a maximum of 42 small lots in conjunction with the construction of 42 small lot homes, the revised tract map would be consistent the requested Zone Change. The tract map is approved contingent upon the approval and adoption of the Zone Change to RD5 and requires that the map be modified prior to recordation if the requested Zone Change is not approved. The applicant has also requested modification to the adopted "Q" Conditions as it relates to building design, construction, retaining walls, and grading. Additionally, the applicant

has requested the approval of a Zoning Administrator's Determination and Adjustment related to the number and height of retaining and privacy walls. In the event that the incidental case is disapproved or modified, the applicant would be required to submit a revised tract map that is consistent with the determination of Case No. APCE-2015-2048-ZC-ZAD-ZAA.

In addition to LAMC Section 17.05 C, LAMC Section 17.06 B requires that the tract map be prepared by or under the direction of a licensed surveyor or registered civil engineer. The tract map was prepared by Lawrence Wilson, P.L.S 6712, and contains information regarding the boundaries of the project site, as well as the abutting public rights-of-way, existing and proposed dedication, and improvements of the tract map. The tract map indicates the tract number, notes, legal description, contact information for the owner, applicant, and engineer, as well as other pertinent information as required by LAMC Section 17.06 B. As discussed at the public hearing, the applicant submitted a revised tract map with corrections to the setback matrix to provide setbacks from the existing property lines and not from the proposed merger along with other minor revisions. The changes do not change the overall layout of the map. Therefore, the proposed map demonstrates compliance with LAMC Sections 17.05 C, 17.06 B, and is consistent with the applicable General Plan.

(b) THE DESIGN OR IMPROVEMENT OF THE PROPOSED SUBDIVISION IS CONSISTENT WITH APPLICABLE GENERAL AND SPECIFIC PLANS.

For purposes of a subdivision, design and improvement is defined by Section 66418 and 66419 of the Subdivision Map Act and LAMC Section 17.02. Design refers to the configuration and layout of the proposed lots in addition to the proposed site plan layout. Pursuant to Section 66427(a) of the Subdivision Map Act, the location of the buildings is not considered as part of the approval or disapproval of the map by the Advisory Agency. Easements and/or access and "improvements" refers to the infrastructure facilities serving the subdivision. LAMC Section 17.05 enumerates the design standards for a tract map and requires that each map be designed in conformance with the Street Design Standards and in conformance with the General Plan. As indicated in Finding (a), LAMC Section 17.05 C requires that the tract map be designed in conformance with the zoning regulations of the project site. Approximately 56,229 square feet of the site is zoned [Q]R1-1D, which would permit up to 11 lots with a minimum lot area of 5,000 square feet. The remaining 162,041 square feet of lot area is zoned [Q]RD6-1D, which would permit up to 27 dwelling units. As the site is partially zoned [Q]R1-1D, the small lot subdivision as shown in the revised tract map would not be permitted because LAMC Section 12.22 C.27 does not permit small lot subdivisions in the R1 Zone. In conjunction with the requested tract map, the applicant has requested a zone change from [Q]R1-1D and [Q]RD6-1D to [Q]RD5-1D as part of incidental Case No. APCE-2015-2048-ZC-ZAD-ZAA. The requested zone change would permit the subdivision of the site as shown in the revised tract map and would permit a maximum of 43 dwelling units. As discussed in Finding (a), the requested zone change would be consistent with the existing land use designation and would result in a net increase of five (5) dwelling units from the existing zoning. As the revised tract map is for a maximum of 42 small lots for the construction of 42 small lot homes, the revised tract map would be consistent with the requested zone change. The tract map is approved with the condition that the map is in compliance with the approvals of Case No. APCE-2015-2048-ZC-ZAD-ZAA. As such, the proposed density and uses would be consistent the proposed land use designation, zone, and height district.

The tract map was distributed to and reviewed by the various city agencies of the Subdivision Committee that have the authority to make dedication, and/or improvement recommendations. The Bureau of Engineering reviewed the tract map for compliance with

the Street Design Standards. As part of the revised tract map, the applicant requested to merge a portion of the existing public right-of-way to tract. As indicated in the recommendation from the Bureau of Engineering, the merger has not been recommended as the area is currently improved with an existing sidewalk for public use. During the public hearing, the representative confirmed that a vacation of a portion of the public right-of-way along Lombardy Avenue was no longer being requested and that the revised map would no longer show the requested merger. The representative from the Bureau of Engineering requested clarification regarding the notation for the variable width common access easement be clarified. Planning Staff clarified that the width of the common access easement has been recommended at 28 feet for the entirety of the easement and that this does not refer to the actual improved width of the common access driveway. The representative requested clarification that the width of common access driveway from Lombardy Avenue be allowed to have a minimum width of 20 feet in lieu of 28 feet. Planning Staff concurred with the request as the common access driveway from Lombardy Avenue would only serve four (4) small lot homes and each home has frontage along an improved public right-of-way. The Bureau of Engineering has recommended dedication and/or improvements to the public right-of-way along Eastern Avenue and Lombardy Boulevard, consistent with the standards of the Mobility Element. In addition, the Bureau of Engineering has recommended the construction of the necessary on-site mainline sewers and all necessary street improvements will be made to comply with the Americans with Disabilities Act (ADA) of 2010. The Bureau of Street Lighting has not required any new street lights; however, would require the relocation and upgrading of existing lights if widening is requiring by BOE. As conditioned, the design and improvements of the proposed subdivision are consistent with the applicable General Plan.

(c) THE SITE IS PHYSICALLY SUITABLE FOR THE TYPE OF DEVELOPMENT.

The project site is an irregularly shaped site that is comprised of approximately 218,270 square feet of lot area. The site is currently vacant and contains 102 Protected Trees (Black Walnut trees), as well as 72 non-protected trees. The applicant proposes the removal of 39 Protected Trees and 59 non-protected trees and the grading and export of approximately 78,000 cubic yards of dirt for the construction of 42 small lot homes. In addition to the construction of the small lot homes, the applicant proposes the construction of a total of 178 retaining walls comprised of 129 walls which will be less than 3 feet in height, 18 walls with a height between 3 and 4 feet, and 32 walls with a height between 4 and 6 feet. Up to 103 of the retaining walls will have an additional 6-foot high privacy/security fence constructed on top of the retaining wall. The project site is located within the 0.86 km (0.53 miles) from the Upper Elysian Park Fault, but is not located within the Alquist-Priolo Fault Zone. The site is located within a designated hillside area and within the BOE Special Grading Area. The site is not located within a flood zone, landslide, liquefaction, methane, or tsunami inundation zone. The site is not located within Fire District No. 1, but is located within the Very High Fire Hazard Severity Zone. The Project would be required to comply with all applicable regulations as it pertains to development within the the Very High Fire Hazard Severity Zone. Prior to the issuance of any permits, the project would be required to be reviewed and approved by the Department of Building and Safety and the Fire Department. The site is not identified as having hazardous waste or past remediation. The site is within Flood Zone Type C, which denotes areas minimal flooding. The site is not subject to the Specific Plan for the Management of Flood Hazards (floodways, floodplains, mud prone areas, coastal high-hazard and flood-related erosion hazard areas).

The tract has been approved contingent upon the satisfaction of the Department of Building and Safety, Grading Division and compliance with the Geology and Soils Report Approval Letter dated October 4, 2019, Log No. 109928 prior to the recordation of the map

and issuance of any permits. Therefore, the site will be physically suitable for the proposed type of development.

- (d) THE SITE IS PHYSICALLY SUITABLE FOR THE PROPOSED DENSITY OF DEVELOPMENT.

The Project Site and parcels directly adjacent to the site to the east and south, and further east, across Lombardy Avenue, have a land use designation of Low Residential and are zoned [Q]RD6-1D and [Q]R1-1D and are developed primarily with one-story single-family dwellings. The parcels to the west and northwest of the site, across Eastern Avenue, have a land use designation of Open Space and Public Facilities and are zoned [Q]OS-1XLD and [Q]PF-1D, respectively. The parcels are developed with the El Sereno Park and Farmdale Elementary School. To the northeast, along Eastern Avenue, the parcels have a land use designation of Neighborhood Commercial and are zoned [Q]C4-1XL. The parcels are developed with one- to two-story residential uses and one-story commercial uses. The parcels to the northeast, across Lombardy Boulevard, have a land use designation of Low Medium I Residential and are zoned [Q]RD3-1D and are developed with one- and two-story single- and multi-family homes.

The site is currently vacant and contains 102 Protected Trees (Black Walnut trees), as well as 72 non-protected trees. In conjunction with the requested tract map, the applicant has requested a Zone Change to permit the construction of 42 small lot homes in conjunction with the revised tract map for the small lot subdivision. The requested zone change would change the existing [Q]R1-1D and [Q]RD6-1D Zone to [Q]RD5-1D, which would permit a maximum of 43 dwelling units. As discussed in Finding (a), the requested zone change would be consistent with the existing land use designation and would result in a net increase of five (5) dwelling units from the existing zoning. As the revised tract map is for a maximum of 42 small lots for the construction of 42 small lot homes, the revised tract map would be consistent with the requested zone change. Additionally, the applicant has requested modifications to the existing Q Conditions as it relates to building design, construction, retaining walls, and grading. As part of the construction of the dwelling units, the applicant has requested a Zoning Administrator's Determination and Adjustment to permit the construction of up to 178 retaining walls, with approximately 103 retaining walls having a privacy fence installed on top of them. The tract has been approved contingent upon the satisfaction of the Department of Building and Safety, Grading Division and compliance with the Geology and Soils Report Approval Letter dated October 4, 2019, Log No. 109928 prior to the recordation of the map and issuance of any permits.

- (e) THE DESIGN OF THE SUBDIVISION OR THE PROPOSED IMPROVEMENTS ARE NOT LIKELY TO CAUSE SUBSTANTIAL ENVIRONMENTAL DAMAGE OR SUBSTANTIALLY AND AVOIDABLY INJURE FISH OR WILDLIFE OR THEIR HABITAT.

The site is currently vacant and contains 102 Protected Trees (Black Walnut trees), as well as 72 non-protected trees. The tract map is conditioned to require the approval of the Board of Public Works prior to the issuance of any permits for the removal of any protected trees and those located within the public right-of-way. The surrounding area is presently developed with structures. Neither the project site nor the surrounding area provides a natural habitat for fish or wildlife. On April 6, 2017, the City Planning Department re-circulated Mitigated Negative Declaration, Case No. ENV-2015-1918-MND, which determined that the project would result in significant impacts relating to biological resources due to the proposed removal of trees; however, mitigations have been identified which would mitigate impacts to a less than significant level. Those mitigations have been incorporated as conditions of approval of this determination. As such, the proposed project

will not cause substantial environmental damage or injury to wildlife or their habitat.

- (f) THE DESIGN OF THE SUBDIVISION OR TYPE OF IMPROVEMENTS IS NOT LIKELY TO CAUSE SERIOUS PUBLIC HEALTH PROBLEMS.

There appears to be no potential public health problems caused by the design or improvement of the proposed subdivision.

The development is required to be connected to the City's sanitary sewer system, where the sewage will be directed to the LA Hyperion Treatment Plant, which has been upgraded to meet Statewide ocean discharge standards. The Bureau of Engineering has reported that the proposed subdivision does not violate the existing California Water Code because the subdivision will be connected to the public sewer system and will have only a minor incremental impact on the quality of the effluent from the Hyperion Treatment Plant.

- (g) THE DESIGN OF THE SUBDIVISION OR THE TYPE OF IMPROVEMENTS WILL NOT CONFLICT WITH EASEMENTS, ACQUIRED BY THE PUBLIC AT LARGE, FOR ACCESS THROUGH OR USE OF PROPERTY WITHIN THE PROPOSED SUBDIVISION.

As required by LAMC Section 12.03, the project site has a minimum of 20 feet of frontage along Eastern Avenue and Lombardy Boulevard, which are public streets. The project site consists of a parcel identified as Lot Nos. FR Cornwall 10 Acres Arb 6 of Tract Partition Map of Lots 3&4 and E ½ of SQ ¼ of Sec 18 and NE ¼ of NW ¼ of Sec 19 T1S R12W S.B.M. and Lot FR1 and Vac Ord 83066 of Farndale Tract, and Lot 562 of Tract TR 6900. The Site is comprised of the following APN Nos. 5216-008-016, -034, and -037. The revised tract map includes easements within the tract for vehicular access purposes; however, there are no known easements acquired by the public at large for access through or use of the property within the proposed subdivision, as identified on the tract map. Necessary easements for utilities will be acquired by the City prior to the recordation of the proposed parcel map.

Therefore, the design of the subdivision and the proposed improvements would not conflict with easements acquired by the public at large for access through or use of the property within the proposed subdivision.

- (h) THE DESIGN OF THE PROPOSED SUBDIVISION SHALL PROVIDE, TO THE EXTENT FEASIBLE, FOR FUTURE PASSIVE OR NATURAL HEATING OR COOLING OPPORTUNITIES IN THE SUBDIVISION. (REF. SECTION 66473.1)

In assessing the feasibility of passive or natural heating or cooling opportunities in the proposed subdivision design, the applicant has prepared and submitted materials which consider the local climate, contours, configuration of the parcel(s) to be subdivided and other design and improvement requirements.

Providing for passive or natural heating or cooling opportunities will not result in reducing allowable densities or the percentage of a lot which may be occupied by a building or structure under applicable planning and zoning in effect at the time the tentative map was filed.

The lot layout of the subdivision has taken into consideration the maximizing of the north/south orientation.

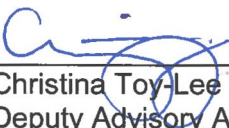
The topography of the site has been considered in the maximization of passive or natural

heating and cooling opportunities.

In addition, prior to obtaining a building permit, the subdivider shall consider building construction techniques, such as overhanging eaves, location of windows, insulation, exhaust fans; planting of trees for shade purposes and the height of the buildings on the site in relation to adjacent development.

These findings shall apply to both the tentative and final maps for Tract No. 73531-SL.

VINCENT P. BERTONI, AICP
Advisory Agency


Christina Toy-Lee
Deputy Advisory Agency
CTL:MS

Note: If you wish to file an appeal, it must be filed within 10 calendar days from the decision date as noted in this letter. For an appeal to be valid to the City Planning Commission or Area Planning Commission, it must be accepted as complete by the City Planning Department and appeal fees paid, prior to expiration of the above 10- day time limit. Such appeal must be submitted on Master Appeal Form No. CP- 7769 at the Department's Public Offices, located at:

Figueroa Plaza
201 North Figueroa Street,
4th Floor
Los Angeles, CA 90012
(213) 482-7077

Marvin Braude San Fernando
Valley Constituent Service Center
6262 Van Nuys Boulevard, Room
251
Van Nuys, CA 91401
(818) 374-5050

West Los Angeles
Development Services Center
1828 Sawtelle Boulevard,
2nd Floor
Los Angeles, CA 90025
(310) 231-2912

Forms are also available on-line at [http:// planning.lacity.org](http://planning.lacity.org)

The time in which a party may seek judicial review of this determination is governed by California Code of Civil Procedure Section 1094.6. Under that provision, a petitioner may seek judicial review of any decision of the City pursuant to California Code of Civil Procedure Section 1094.5, only if the petition for writ of mandate pursuant to that section is filed no later than the 90th day following the date on which the City's decision becomes final.

VESTING TENTATIVE TRACT MAP NO. 73531

IN THE CITY OF LOS ANGELES 42 LOT SMALL LOT SUBDIVISION AND MERGER COUNCIL DISTRICT # 14

LEGAL DESCRIPTION:
THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:

PARCEL A:
THAT PORTION OF THE LOT MARKED "CORNWALL" ON THE PARTITION MAP OF THE SMITH ESTATE, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA AS PER MAP RECORDED IN BOOK 6 PAGE 156 OF MISCELLANEOUS RECORDS, ON THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHEAST CORNER OF SAID LOT MARKED "CORNWALL", BEING ALSO THE SOUTHEAST CORNER OF LOT 1 OF THE FARMDALE TRACT, AS PER MAPS RECORDED IN BOOK 4 PAGE 37 OF MAPS, IN SAID OFFICE OF THE COUNTY RECORDER; THENCE SOUTH ALONG THE EAST LINE OF SAID "CORNWALL" LOT, 180.87 FEET; THENCE WEST PARALLEL WITH THE SOUTH LINE OF SAID FARMDALE TRACT, WHICH IS ALSO THE NORTH LINE OF SAID "CORNWALL" LOT, 538.78 FEET MORE OR LESS, TO THE EASTERLY LINE OF FARMDALE AVENUE, FORMERLY KNOWN AS BROWN ROAD, AS SAID ROAD WAS DESCRIBED IN THE DEED TO THE COUNTY RECORDER, THENCE NORTH 32°28' EAST ALONG THE EASTERLY LINE OF SAID AVENUE, 214.57 FEET, MORE OR LESS, TO THE NORTH LINE OF SAID LOT MARKED "CORNWALL"; THENCE EAST ALONG SAID LAST MENTIONED NORTH LINE 424.62 FEET TO THE POINT OF BEGINNING.

PARCEL B:
PARCEL 1:
LOT 1 OF FARMDALE TRACT, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 4 PAGE 37 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

PARCEL 2:
THAT PORTION OF EASTERN AVENUE, FORMERLY BROWN ROAD, AS SHOWN ON SAID MAP OF FARMDALE TRACT, VACATED, ADJOINING SAID LOT 1 ON THE NORTHWEST WHICH LIES SOUTHEASTERLY OF THE SOUTHEASTLY LINE OF EASTERN AVENUE 80 FEET WIDE, AS ESTABLISHED BY VACATION ORDINANCE NO. 83066, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA.

PARCEL C:
LOT 562 OF TRACT NO. 6900, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 108 PAGE 55 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

BASIS OF BEARINGS:
THE BEARINGS FOR THIS MAP ARE BASED ON THE CENTERLINE OF MALLORY STREET AS SHOWN ON TRACT 6900, FILED IN BOOK 86, PAGES 34-39, RECORDS OF LOS ANGELES COUNTY, BEING NORTH 00°00'00" WEST.

BENCH MARK:
NO. 11-05092 ELEV = 447.371 ADJ 2000
WIRE SPK W. CURB FACE EASTERN AVE; 24 FEET S/O S CURB LINE PROD. OF LOMBARDY BLVD, 1 FOOT S/O N END CB

EXISTING EASEMENTS:

- PURPOSE:** EASEMENT FOR PUBLIC STREET, ROAD, OR HIGHWAY IN FAVOR OF: CITY OF LOS ANGELES
RECORDING DATA: IN BOOK 12435, PAGE 200, OF OFFICIAL RECORDS
AFFECTS: THE EASTERLY 40 FEET OF SAID LAND
NOT PLOTTED, EASTERN AVE RIGHT-OF-WAY
- PURPOSE:** SLOPE CONSTRUCTION AND MAINTENANCE ALONG EASTERN AVE
RECORDING DATA: BOOK 16886, PAGE 99 OF OFFICIAL RECORDS
AFFECTS: VARIABLE WIDTH AS REQUIRED ALONG EASTERN AVENUE FROM VALLEY BLVD TO LOMBARDY BLVD
NOT PLOTTED, ADJACENT TO EASTERN AVE
- PURPOSE:** SLOPE CONSTRUCTION AND MAINTENANCE ALONG EASTERN AVE
RECORDING DATA: BOOK 16886, PAGE 99 OF OFFICIAL RECORDS AND BOOK 16945, PAGE 69 OF OFFICIAL RECORDS
AFFECTS: VARIABLE WIDTH AS REQUIRED ALONG EASTERN AVENUE FROM VALLEY BLVD TO LOMBARDY BLVD
NOT PLOTTED, ADJACENT TO EASTERN AVE
- PURPOSE:** PUBLIC UTILITY EASEMENT
RECORDING DATA: BOOK 15644, PAGE 194 OF OFFICIAL RECORDS.
AFFECTS: 5' WIDE AS SHOWN ON THE MAP, TO BE ABANDONED PRIOR TO FINAL MAP RECORDATION

NOTES:

- EXISTING USE: 3 VACANT PARCELS
- PROPOSED USE: SUBDIVISION OF 42 LOTS IN CONJUNCTION WITH THE CONSTRUCTION OF 42 SINGLE FAMILY HOMES, UTILIZING THE SMALL LOT ORDINANCE.
- AREA: EXISTING LOT AREA=218,354 SF (5.0127 ACRES)
DEDICATION AREA = 1,879 SF (0.0431 ACRES) - EASTERN AVE.
NET AREA = 216,475 SF (4.9696 ACRES)
GROSS AREA TO CENTERLINE = 257,716 SF (5.9163 ACRES)
- EXISTING ZONE - [O]R1-1D, [O]RD6-1D
PROPOSED ZONE - [Q]RD5-1D
- TREES:
ALL ONSITE PROTECTED TREES ARE PLOTTED ON THE TENTATIVE MAP.
- EXISTING UTILITIES:
30" SEWER MAIN LINE IN EASTERN AVENUE
10" SEWER MAIN LINE IN LOMBARDY BLVD
24" WATER MAIN LINE IN EASTERN AVENUE
12" WATER MAIN LINE IN LOMBARDY BLVD
7"x4.5" STORM WATER MAIN LINE IN EASTERN AVENUE
30" RCP MAIN LINE IN LOMBARDY BLVD
- DRAINAGE:
WHOLE SITE DRAINS TO EASTERN AVENUE, OR LOMBARDY BLVD.
- FLOOD ZONE: X
FEMA MAP #: 06037-C1635F
- THIS PROJECT IS IN A LANDSLIDE AREA.
- THIS PROJECT IS IN A LIQUEFACTION AREA.
- THIS PROJECT IS IN A VERY HIGH FIRE HAZARD AREA.
- THERE ARE NO KNOWN POTENTIALLY DANGEROUS AREAS WITHIN THIS PROPOSED DEVELOPMENT.
- ALL EXISTING STRUCTURES TO BE REMOVED.
- 28' VEHICULAR BACKUP SPACE IS PROVIDED.
- DISTRICT MAP NO. 139-54233
- NORTHEAST HILLSIDE ZONE, LOS ANGELES PLANNING AREA
- TRASH WILL BE COLLECTED ON EACH LOT.
- THERE ARE PROTECTED TREE SPECIES ON SITE AS SHOWN.
- SMALL LOT SINGLE FAMILY SUBDIVISION IN THE R05 ZONE, PURSUANT TO ORDINANCE 176.354.
- GRADING QUANTITIES:
CUT - 80,407
FILL - 1,626
R&R - 8,000 (SHRINKAGE 15%, 1,200)
EXPORT - 77,958

ASSESSOR PARCEL NUMBERS:
5216-008-016; 5216-008-034; 5216-009-037

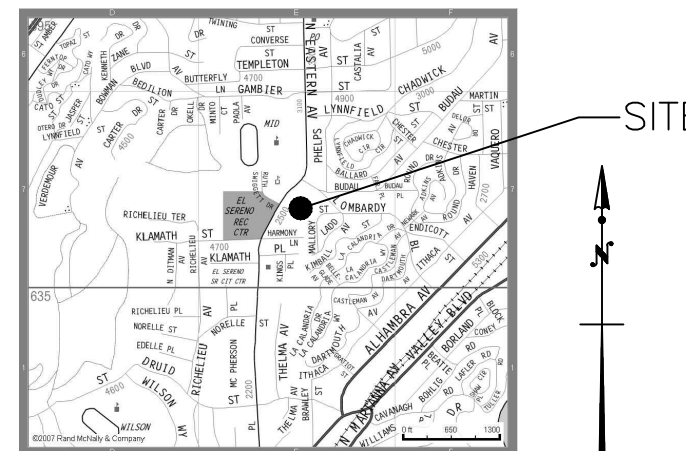
ZONING INFORMATION:
EXISTING: R05 AND RD1
PROPOSED: R05

SITE ADDRESS:
2520-2608 NORTH EASTERN AVE &
2657 LOMBARDY BLVD
LOS ANGELES, CA 90032

THOMAS GUIDE:
PAGE 595, GRID E7

SUBDIVIDER:
TTLIC LOS ANGELES - EL SERENO, LLC
ROB FLITTON
rfliton@thetrueelife.com
2942 CENTURY PLACE SUITE 121
COSTA MESA, CA 92626
(949) 500-7998

OWNER:
TTLIC LOS ANGELES - EL SERENO, LLC
ROB FLITTON
rfliton@thetrueelife.com
2942 CENTURY PLACE SUITE 121
COSTA MESA, CA 92626
(949) 500-7998



VICINITY MAP
NOT TO SCALE

SETBACK MATRIX				
LOT NO.	NORTH	SOUTH	EAST	WEST
1	22[S]	23[S]	4[R]	5[F]
2	26[F]	16[R]	4[S]	4[S]
3	26[F]	16[R]	4[S]	4[S]
4	24[F]	18[R]	4[S]	4[S]
5	26[F]	16[R]	4[S]	4[S]
6	24[F]	18[R]	6[S]	4[S]
7	28[F]	19[R]	4[S]	4[S]
8	28[F]	19[R]	4[S]	4[S]
9	28[F]	19[R]	4[S]	4[S]
10	28[F]	12[R]	4[S]	6[S]
11	11[F]	12[R]	4[S]	4[S]
12	20[F]	12[R]	18[S]	4[S]
13	4[S]	15[S]	13[R]	25[F]
14	4[S]	4[S]	14[R]	25[F]
15	6[S]	4[S]	14[R]	25[F]
16	4[S]	6[S]	13[R]	25[F]
17	4[S]	4[S]	13[R]	25[F]
18	4[S]	4[S]	13[R]	25[F]
19	9[S]	4[S]	13[R]	25[F]
20	36[R]	5[F]	14[S]	7[S]
21	35[R]	29[F]	4[S]	4[S]
22	55[R]	23[F]	4[S]	4[S]
23	75[R]	24[F]	4[S]	7[S]
24	56[F]	4[R]	4[S]	56[F]
25	35[S]	4[S]	27[R]	130[F]
26	41[S]	41[S]	27[R]	150[F]
27	41[S]	41[S]	27[R]	170[F]
28	41[S]	41[S]	27[R]	190[F]
29	41[S]	41[S]	27[R]	210[F]
30	15[R]	27[F]	15[S]	3[S]
31	15[R]	27[F]	3[S]	3[S]
32	15[R]	27[F]	3[S]	3[S]
33	15[R]	27[F]	3[S]	3[S]
34	15[R]	27[F]	3[S]	3[S]
35	15[R]	27[F]	3[S]	3[S]
36	15[R]	27[F]	3[S]	3[S]
37	15[R]	27[F]	3[S]	3[S]
38	22[S]	32[S]	5[R]	6[F]
39	15[F]	23[R]	3[S]	5[F]
40	25[F]	30[R]	3[S]	3[S]
41	25[F]	30[R]	4[S]	3[S]
42	7[F]	62[R]	23[S]	15[S]

SETBACKS ARE TO GROUND FLOOR OR SECOND FLOOR, INDICATED ON PLANS AS:
[F] = FRONT YARD
[R] = REAR YARD
[S] = RIGHT SIDE YARD

PROPOSED EASEMENTS:

- PROPOSED 20' or 28' (AS SHOWN) WIDTH PRIVATE EASEMENT FOR VEHICULAR AND PEDESTRIAN INGRESS/EGRESS, PARKING, EMERGENCY ACCESS, PUBLIC EMERGENCY SERVICES, PUBLIC UTILITIES, UTILITIES, INFRASTRUCTURE AND CROSS LOT DRAINAGE PURPOSES.
- INDICATES PROTECTED TREE TO REMAIN, TAG NUMBER, SEE TREE REPORT FOR DESCRIPTIONS.
- INDICATES PROTECTED TREE TO BE REMOVED, TAG NUMBER, SEE TREE REPORT FOR DESCRIPTIONS.
- INDICATES PROTECTED TREE REMOVED PRIOR TO MAY 8, 2015, TAG NUMBER, SEE TREE REPORT FOR DESCRIPTIONS.

MALLORY STREET

EASTERN AVENUE

GRAPHIC SCALE
0 40 80 120 160
1"=40'

VESTING TENTATIVE MAP
TRACT 73531
2520 N. EASTERN AVE
LOS ANGELES, CA 90032

DEVELOPER'S ENGINEER:

FORMA ENGINEERING INC.

400 San Fernando Mission Blvd., San Fernando, CA 91340
Phone: (818) 832-1710 • Fax: (818) 832-1740

LAWRENCE WILSON P.L.S. 6712 2/05/2020 DATE



No.	DATE	REVISION

DESIGNER:
C.K.
CHECKED BY:
W.M.W.
DATE:
2/05/2020
SHEET
1 OF
1

PREPARED FOR:
TTLIC LOS ANGELES - EL SERENO, LLC
2942 CENTURY PLACE SUITE 121 COSTA MESA, CA 92626

EXHIBIT F.1

Interested Parties

Opposition



July 7th, 2016

Gregory S. Shoop, City Planner
Planning Department
City Hall
200 N. Spring Street
Los Angeles, CA 90012

Project Title: El Sereno; Council District 14
Case Number: ENV-2015-1918-MND,VTT-73531,APCE2015-2048-ZC-ZAD

Dear Mr. Shoop,

This letter was prepared on behalf of the City's Community Forest Advisory Committee (CFAC), in reference to the above-mentioned Mitigated Negative Declaration for a proposed hillside subdivision for 42-house development.

CFAC members provide expert advice to the City Officials on tree matters within the City of Los Angeles. Our questions, comments, and concerns on this Project, in regard to the existing Black walnut woodland on this property, are given below.

In summary, we feel a full Environmental Impact Report is required for this project based on the total number of trees being removed, and that 68 of those trees are protected including the endangered *juglans californica*, California Black Walnut. This project presents a significant impact to the environment, and a full review must be completed in order to understand the impact it will have.

Below is our detailed explanation of why we feel this is justified. Please feel free to contact us with any questions or comments you have on this matter.

Thank you for taking this matter into consideration.

Ryan Allen
Chair – Community Forest Advisory Committee
rallen@kycccla.org
(213) 365-7400 x5401



1. LEVEL OF ENVIRONMENTAL REVIEW

If approved as submitted, this project would require the removal of 68 of the existing 102 Protected Trees; and, 64 of the 72 existing Non-protected trees on the subject property. However, Permits for tree removal have not been included as part of the Discretionary Approvals, as they should be.

Juglans californica (California black walnut) is a deciduous tree endemic to California, where it exists in only four counties in Southern California. Southern California walnut woodlands are severely threatened by urbanization. The Nature Conservancy, in cooperation with the state of California, is giving high priority to acquiring vegetative/habitat data on the woodland. They list it as one of California's rare and imperiled natural communities. In addition, the California Native Plant Society has rated the species as 4.2. Species with a California Rare Plant Rank of 4 are of limited distribution or infrequent throughout a broader area in California and are on a watch list.

Given the above and that the proposed project would remove **a full 76% of the existing ecosystem**, we question whether a Mitigated Negative Declaration is the appropriate environmental review for this project. Rather, in our expert opinion, we contend that it is not.

The removal of 76% of a Black walnut ecosystem cannot be mitigated to a level of insignificance; therefore, the proper review for this project is an **Environmental Impact Report (EIR)**.

Legal Standard: An EIR Is Required Where There Is A Fair Argument That The Project May Have Adverse Environmental Impacts

This Project relies on a MND, not an EIR, under CEQA. This means that the less deferential fair argument standard applies.

CEQA applies to agency projects that may have an adverse environmental impact. *Friends of Mammoth v. Board of Supervisors*, 8 Cal.3d 247, 259 (1972). CEQAs procedural and substantive requirements are interpreted . . . to afford the fullest possible protection to the environment within its reasonable scope of the statutory language." *Id.* at 259. CEQA has two broad purposes: 1) avoiding, reducing or preventing environmental damage by requiring alternatives and mitigation measures (14 Cal. Code Regs. § 15002(a)(2)-(3) (hereinafter Guidelines"); and 2) providing information to decisionmakers and the public concerning the environmental effects of



the proposed project (Guidelines § 15002(a)(1)).

The EIR is the heart of CEQA. *Dunn-Edwards v. BAAQMD*, (1992) 9 Cal.App.4th 644, 652 (1992). CEQA requires that an agency analyze the potential environmental impacts of its proposed actions in an environmental impact report except in certain very limited circumstances. A negative declaration, as proposed here for this Project, may be prepared instead of an EIR only when a lead agency determines that a project would not have a significant effect on the environment. Pub. Res. Code § 21080(c). Such a determination may be made only if [t]here is no substantial evidence in light of the whole record before the lead agency that such an impact may occur. *Id.*, § 21080(c)(1).

A negative declaration is improper, and an EIR is required, whenever substantial evidence in the record supports a fair argument that significant impacts may occur, even if other substantial evidence supports the opposite conclusion. *Pocket Protectors v. Sacramento*, 124 Cal.App.4th 903 (2005). Since [t]he adoption of a negative declaration . . . has a terminal effect on the environmental review process, by allowing the agency to dispense with the duty [to prepare an EIR], negative declarations are allowed only in cases where the proposed project will not affect the environment at all. Such a determination may be made, however, only if [t]here is no substantial evidence in light of the whole record before the lead agency that such an impact may occur. *Id.*, § 21080(c)(1) (emphasis added).

The fair argument standard, that applies here, creates a low threshold" favoring environmental review through an EIR rather than through issuance of negative declarations or notices of exemption from CEQA. *Citizens Action to Serve All Students v. Thornley*, 222 Cal.App.3d 748, 754 (1990). An agency's decision not to require an EIR can be upheld only when there is no credible evidence to the contrary. *Sierra Club v. County of Sonoma*, 6 Cal.App.4th, 1307, 1318 (1992).

2. MITIGATION MEASURES

Mitigation 1.1 calls for replacement for all significant (8" or greater trunk diameter, or cumulative trunk diameter if multi-trunked, as measured 54" above the ground) non-protected trees at a ratio of 1:1 with a minimum 24" box tree.

Mitigation measure 1.2 calls for replacement protected trees at a "minimum of two trees" (a minimum of 15-inch box size". This appears to be an error, if the intent is to



replace removed protected species for the same species, which should be the case.

Containers for trees are generally: 1 gallon; 5 gallons; 15 gallons; 24" box; 36" box, 48" box. This mitigation measure is not clear. Perhaps the intent is to require to replace each protected tree with two 15 gallon trees. However, it should be noted that it is unusual to find *Juglans californica* in sizes larger than 5 gallons, due to their sensitive root systems.

Therefore, a greater number of trees would be required for the canopy replacement of the mature protected trees.

Further, it is not made clear that the requirement is to replace the *Juglans californica* with the same species: *Juglans californica*.

There is no Plan/map that shows the **planting locations** for the Protected tree replacements. There is no requirement for inspection, monitoring and survival of the trees.

There is no stated length of time for which an Arborist must monitor the trees. We suggest a minimum of 7 years.

Mitigation measure 1.4 calls for replacement of the **Previously Removed Protected Trees** (that are noted in 1. AESTHETICS b) on Page 1V-1). This appears to be incorrectly enumerated. Its placement in the document, after Mitigation Measure 1.2 and before Mitigation Measure 1.4, indicates that it should be **Mitigation measure 1.3**.

There is no Arborist's report, nor other documentation of the 8 (eight) Protected trees that were removed illegally, ie: without a permit.

There is no report, nor documentation, as to any process by which the property owner and City arrived at the agreement that these trees were cut down and removed illegally. Nor that there should be reparation and that the only consequence of the illegal act would be to replace the eight Protected trees with 8 x 48" box trees of an undetermined species.

This is entirely inadequate. The California black walnut trees have been protected since 2006; they were removed illegally and a much greater consequence is warranted.

Executive Committee

JEN MARKEWYCH
PRESIDENT

MIGUEL VARGAS
VICE-PRESIDENT

MICHELLE CORZANTES
TREASURER

ROLANDO LOPEZ
RECORDING SECRETARY

CYNTHIA SANDOVAL
CORRESPONDING SECRETARY

CITY OF LOS ANGELES CALIFORNIA



LA-32 HISTORIC NEIGHBORHOODS

EL SERENO

EMERY PARK

HILLSIDE VILLAGE

ROSE HILLS

SIERRA PARK

UNIVERSITY HILLS

July 12, 2016

Mr. Gregory Shoop
Los Angeles City Planning Department
200 N. Spring Street, 6th Floor
Los Angeles, CA 90012

RE: Case Numbers: ENV-2015-1918 MND, VTT-73531, APCE-2015-2048-ZC-ZAC

Dear Mr. Shoop:

I am reaching out on behalf of the Los Angeles Thirty-Two Neighborhood Council and the El Sereno Community to provide you with our public comments related to the project known as Vista El Sereno (case number referenced above). After much review and deliberation on this proposed project and the applicant's proposed environmental mitigations, the community has determined that this project will indeed have a significant impact on the environment. I respectfully request that the Environmental Analysis Section of the City's Planning Department take into consideration the below comments and require that a full environmental impact report be prepared for this project.

The specific environmental factors the community believes require more study are listed below:

- **Aesthetics** – The hillsides of El Sereno are a cultural treasure and a scenic resource. The loss of this particular hillside will be devastating to the community as it will degrade the visual character of this site and degrade the geographic identity of our community.
- **Air Quality** – The construction activity related to this project will absolutely increase emissions and degrade the air quality for the surrounding community. Construction vehicles and the vehicles of construction workers will contribute to unnecessary carbon emissions near the project site.

- **Biological Resources** – This project site is home to many trees, some potentially protected by California law. The proposed excavation and relocation of these mature trees places their health at unnecessary risk. Additionally, our hillside would be reduced in size and its current function, serving as open space for local wildlife, would be eliminated.
- **Noise** –The surrounding single-family residential homes are older construction with limited sound insulation. Despite any attempts by the applicant to mitigate noise associated with their construction activity, the neighbors will be exposed to excess levels of sound.
- **Public Services** – Directly across the street from the project site sits El Sereno Regional Recreational Facility and El Sereno Middle School. Additionally, Sierra Park Elementary school is located just east of the project site. Construction related activity would impact access to these public facilities, particularly during school and park peak hours. These public facilities are utilized by many community organizations and so usage times vary. The full extent of the construction impacts to these public-service facilities are unknown and more investigation is required.
- **Transportation and Traffic** – The proposed project site sits on Eastern Ave., an essential artery for the community, and is located in a very congested area of El Sereno. The daily influx of construction workers parking on surface streets, the staging of construction related vehicles, and haul routes will unnecessarily impede the flow of traffic and adversely affect traffic patterns for our community.

The stakeholders of El Sereno support any land development project that is consistent with the goals of the North East Los Angeles Community Plan and consistent with the character of the community, so long as they do not adversely impact the surrounding community and do not degrade the environment. The proposed reduction of our hillside and the proposed relocation of several mature trees at the project site on Eastern and Lombardy would destroy environmental resources that are essential to the quality of life of El Sereno residents. It is my hope that you consider the public comments enumerated above while evaluating this project site, as they are from a wide spectrum of residents who will be directly impacted by your decision. If I can be of any further assistance, please feel free to e-mail me directly at jmarkewych@la32nc.org.

Regards,

Jen Markewych
President
LA32 Neighborhood Council

SANTA MONICA MOUNTAINS CONSERVANCY

RAMIREZ CANYON PARK
5750 RAMIREZ CANYON ROAD
MALIBU, CALIFORNIA 90265
PHONE (310) 589-3200
FAX (310) 589-3207
WWW.SMMC.CA.GOV



May 22, 2017

Vince Bertoni, Director
Attention: Greg Shoop
City Planning Department
City of Los Angeles
200 N. Spring Street
Los Angeles, California 90012

**El Sereno - Eastern Avenue 42-Unit Small Lot Subdivision
Revised Mitigated Negative Declaration Comments ENV-2015-1918-MND
Vesting Tentative Tract Map 73531 – APCE 2015-2048-ZC-ZAD**

Dear Mr. Bertoni:

The Santa Monica Mountains Conservancy reaffirms its opposition to the redesigned 42-unit Small Lot Subdivision project in the shadow of Ascot Hills adjacent an elementary school, recreation center and senior center. The Conservancy finds that the proposed Mitigated Negative Declaration (MND) remains deficient in its analysis of visual, biological, and land use impacts. The subject site is a unique hilltop open space area chocked with a topography of protected native and non-native trees.

The 42-unit project would result in unavoidable significant biological and visual impacts. The significant public controversy over the proposed Zone Change and Variances warrant preparation of a focused Environmental Impact Report to analyze less damaging alternative projects mitigated with actual public benefits. These comments are additive to the Conservancy's June 27, 2016 letter.

The City needs both more housing and more public open space. In exchange for a significant up zoning, the proposed project provides no public or protected natural open space. The need to chop thirty feet (three stories) off of a standalone hilltop, eliminate scores of mature trees, and hold it all together with scores of retaining walls means the project is incongruous with the constraints of the site. This one of a kind five-acre property is not a site on which to jam 42 single family residences via a discretionary Small Lot Subdivision and permanently rob the residents of El Sereno of forested natural area. This a property where the zoning should remain as it currently is and any development should be consistent with the zoning. Any development-given the breadth of the site—should also avoid the removal of protected California black walnut trees. The subject natural land form

Vince Bertoni

El Sereno - Eastern Avenue Project -ENV-2015-1918-MND

May 22, 2017

Page 2

and its natural resource values located in an urban area is not the location to address the housing shortage by leveling a large forested knoll in perfect proximity to public facilities and public access.

The City has no obligation to approve a cookie cutter subdivision on a hilltop. As contained in the Small Lot Subdivision Ordinance; however, if the City goes in that approval direction, it owes the public a substantial, onsite fee simple open space dedication from this project and future small lot subdivisions. It also owes the public an imbedded permanent revenue stream from the homeowner's association in the new development to assist in maintaining the safety and appearance of the open space dedication. Shy of obtaining such public benefits, the public is better served by private open space on the subject property. When will the loss of North East Los Angeles open space be a significant impact in the eyes of the City Planning Department? The conclusion of the MND, inaccurately arrived at, is that as long as it is not the last piece of open space, it is not a significant public loss.

The project is inconsistent with Northeast Los Angeles Community Plan Policy 4-1.1 which states:

Encourage the retention of passive and visual open space which provides a balance to the urban development of the Plan Area.

Please contact Paul Edelman, Deputy Director of Natural Resources and Planning, at 310-589-3200, ext. 128 with any questions and future correspondence.

Sincerely,



IRMA MUÑOZ
Chairperson



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Fwd: Eastern/Lombardy

2 messages

Annamarie Reza <rezaannamarie77@gmail.com>

Thu, Jan 23, 2020 at 12:11 AM

To: May.sirinopwongsagon@lacity.org

----- Forwarded message -----

From: **Annamarie Reza** <rezaannamarie77@gmail.com>

Date: Wednesday, January 22, 2020

Subject: Eastern/Lombardy

To: may.sirinopwongsagon@lacity.org

I am writing this email to let you know that I am totally against this development. My family and I have lived across from the hill since 1962. As I toddler and until now..I have looked at that hill--right now it is so beautiful..all green..birds and animals. Yes..it can become unattractive in the summer. I remember the businesses that were there until approximately 1970..on Lombardy Blvd.

My input is more realistic than emotional.

Living across and down from Farmdale Elementary..I have seen accidents..near misses due to that intersection. The curve is very misleading. Cars speed up to avoid oncoming cars to turn onto Lombardy. Children crossing. I can't tell how many times I have heard tires screeching.

The proposed development has 4 homes on the Lombardy side. The space between the corner to the proposed driveway is very conducive to foreseeable accidents/injuries. How? Just as I mentioned earlier..cars turning onto Lombardy while the residents come out of the driveway.

The proposed residents could possibly try to go left out of the driveway towards Eastern and t-bone accidents..along with other types of possible accidents--not to mentioned risk to pedestrians.

When the schools are in session..the back up of cars trying to go left or right onto Eastern causing the line of cars to go back up Lombardy while waiting for the green lights as there are 3 stops to Druid or 4 to Valley Blvd. The congestion will be horrendous. I am not exaggerating. Those mornings we have difficulty getting out of our driveway. So imagine trying to get into line from your new home's driveway to get unto Eastern.

Don't get me wrong..I am not opposed to development--if done proportional to the area-- and not over development.

The prospect of approximately 80-100 more cars to the intersection will slow down traffic for those who use Eastern either north or south. Plus there is very limited parking on Lombardy as it is now. The overflow of cars moving/parked through the adjoining streets will also be impacted.

I had the pleasure to speak before your group when the first project was proposed..cafe..charter school..apartments.

The second project with more homes.

Now this project..

Please consider more than this project and the negative ramifications to the current neighbors.

I know housing is needed desperately but not this project.

Thank you for taking the time to read my input and consider the facts and the greater picture.

Sincerely,

Annamarie Reza

[2644 Lombardy Blvd](#)

Los Angeles, CA 90032
(323) 222-3690

May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>
To: Annamarie Reza <rezaannamarie77@gmail.com>

Thu, Jan 23, 2020 at 6:41 AM

Hello,

Your comments have been received and included into the administrative record.

May Sirinopwongsagon
(213)978-1372
Department of City Planning
Central Project Planning Bureau
200 N. Spring Street, Room 621
Los Angeles, CA 90012

[Quoted text hidden]



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Opposition to Vista El Sereno, Project #2015-1918-MND

2 messages

Ansley Bleu Davies <ansleybleu@yahoo.com>

Wed, May 8, 2019 at 9:01 AM

To: "may.sirinopwongsagon@lacity.org" <may.sirinopwongsagon@lacity.org>

Good Morning Ms. Sirinopwongsagon,

I am writing to you to express my great concern an opposition to the subdivision project proposed for the southeast corner of Eastern and Lombardy, planning case #2015-1918-MND, vesting tentative tract map #73531, and currently known as Vista El Sereno.

My primary concern is that, when it comes to neighborhood parks, El Sereno is a park poor area. We do not come close to the 10-min walkability to a park ideal that is currently being discussed among many planning circles. This hillside land is ideal for a pocket playground below and a dog park above with other amenities, and exercise stairs and pathways up the hillsides.

I am currently working with a group of neighborhood stakeholders to have the LA-32 Neighborhood Council write a letter of opposition regarding this project to you. We have already obtained over the 50 signature minimum to request that the letter be sent, but the have been issues attaining a quorum at meetings to hear the agenda item. The LA-32 Neighborhood Council has already voted in opposition to this project.

The problems with this proposed subdivision are as follows:

1. Will create unsafe traffic conditions at peak driving times. The stop sign at Lombardy, Phillips, Budau streets will end up becoming a roundabout, and others trying to exit the property and turn left onto Eastern will be sabotaged by all the oncoming traffic heading to the elementary and middle school, the park, and the freeway.
2. This project should not be re-zoned for small-lot because it is not warranted. This development plan is based upon old car-culture technology. There are no-sidewalks or stairways in this plan, so there will be NO walkability. There is no transit nearby. This is merely an expansion of urban sprawl into one of the few remaining open spaces we have left. This project will easily create divisiveness in our neighborhood, a us vs. them mentality. The proposed project would start a wave of gentrification and all the independently owned businesses in the area would likely be forced out, similar to what occurred in Highland Park.
3. A re-zone for small lot development would set a bad precedent for El Sereno. The neighborhood is mainly made up-of independently constructed homes and residents would prefer if it remained that way. There are at least a dozen buildable open lots in a 1/4 mile radius of my home that are waiting for single family units to be built upon. This is the type of development we want to see in El Sereno.
4. This open space currently contains many native walnut trees and acts as a habitat for the Hory bat. A subdivision would wipe these sensitive species away and change the ecosystem of the area. A park would preserve these qualities. In the last two or three years, the property owner had cut down many of these walnut trees without a permit. It is my understanding that, under the City's protection

ordinance for this endangered habitat, a builder can lose their right to build on the property for such an offense.

5. When reviewing the MND, there seemed to be a number of other issues with the proposed Vista El Sereno project as well such as the excessive removal of soils, excessive retaining walls, and an issue with paleontology. The MND does not clearly state how mitigation will occur to protect any sensitive paleontologist resources. El Sereno is built on sedimentary rocks, so there is a high likelihood of uncovering sensitive resources. Additionally, doesn't this project require an EIR rather than a mere MND?

I strongly urge the City of Los Angeles to reject the Vista El Sereno project. Our group hopes to engage a community partner to pursue a Measure A parkland grant and purchase this land to sensitively develop it for park purposes. Usable neighborhood parkland will be a true benefit to the community.

I look forward to hearing when your offices will hold a hearing for this project so we may coordinate our opposition forces for the betterment of the City of Los Angeles.

Thank you for your time.

Sincerely,
Ansley Davies, El Sereno resident
[2753 Ballard Street](#)
[Los Angeles, CA 90032](#)
626-590-4159 cel

May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>
To: Ansley Bleu Davies <ansleybleu@yahoo.com>

Mon, May 13, 2019 at 8:05 AM

Hello,

Thank you for your letter. It has been added to the administrative record for consideration.

May Sirinopwongsagon
(213)978-1372
Department of City Planning
Central Project Planning Bureau
200 N. Spring Street, Room 621
Los Angeles, CA 90012

[Quoted text hidden]



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

2520 N. Eastern Ave Hillside Development Project - Gor Family

2 messages

leongor@juno.com <leongor@juno.com>

Wed, Jan 22, 2020 at 4:37 PM

To: may.sirinopwongsagon@lacity.org

Ms. May Sirinopwongsagon, City Planner
Via email: may.sirinopwongsagon@lacity.org

Re: [2520 North Eastern Avenue](#) (Eastern Avenue & Lombardy Boulevard)
Case Nos. VTT-73531-SL; APCE-2015-2048-ZC-ZAD-ZAA

Dear Ms. Sirinopwongsagon,

My house is very close to the property referenced above and received your letter to notify what a developer wants to do to this hillside. I have lived here many years and raised my children here. El Sereno is my home and I want the best for its future development.

My family and I do not think it is a good location to build 42 houses, it is just too many. 20 houses with room to park cars on the street(s) on the hill is a better solution. We do not have enough parking already. Public transportation is not good enough that people will give up their cars. Overflow of parking will impact the surrounding streets.

I also am concerned that the hillside soil is not stable enough to build so many tall houses. My house was built in the 60's and there is always cracking because the soil keeps swelling when it rains and shrinks when it is dry. It doesn't matter if the house is built when my house was built or in more recent times like the ones on Kings Place down the street. Can the hillside hold up so much weight without causing damage to the homes? My neighbor says our homes are built on liquefaction soil and that is why we have new cracks every year, why our retaining walls are leaning. What happens when there is cracking and sinking on the hill with 42 homes?

Thank you for your time and we hope you can do better for our neighborhood than what this developer wants.

Sincerely,

Connie Gor and family
[4837 Klamath Place](#)
[Los Angeles, CA 90032](#)

May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Thu, Jan 23, 2020 at 6:44 AM

To: "leongor@juno.com" <leongor@juno.com>

Hello,

Your comments have been received and included into the administrative record.

May Sirinopwongsagon
(213)978-1372
Department of City Planning
Central Project Planning Bureau
200 N. Spring Street, Room 621
Los Angeles, CA 90012

[Quoted text hidden]



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

El sereno hilltop

delmimolina@aol.com <delmimolina@aol.com>

Wed, Jan 29, 2020 at 3:57 PM

To: May.Sirinopwongsagon@lacity.org

There is no reason to grant modifications to allow the hilltop to be removed. If the project can not be built with existing ordinances, it should not be built. The Hillsides Ordinance is there for a reason. In El Sereno we value the hilltops and the laws that protect them, we support smart development, and we will continue to reject opportunistic nonsense like this project.

Thank You
Delmi Molina



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Letter Opposing - Case No: ENV-2015-1918-MND "Vista El Sereno"

2 messages

e V <ezmerv@yahoo.com>

Thu, Jan 23, 2020 at 1:27 AM

To: may.sirinopwongsagon@lacity.org

Dear May Sirinopwongsagon:

Please accept this email and the corresponding letter (PDF attached) as a formal objection to the above referenced project.

I object to the planned construction of ENV-2015-1918-MND "Vista El Sereno" development project at the intersection of Eastern Avenue and Lombardy Blvd. in El Sereno for the following reasons:

- **Traffic Congestion / Air pollution / Street Safety (pedestrian and driver):** Eastern Avenue and neighboring streets will experience excessive vehicle traffic each day. Traffic gridlock and congestion will become a daily problem. The entrance and exit from Lombardy Boulevard and Eastern Ave will increase vehicular traffic substantially.
- **Noise Pollution / Disturbance of the Peace:** The proposed location is a small raised hill at the center surrounded by hills that amplify the slightest sounds to all residents.
- **Light Pollution:** Lights at night from the proposed structures will emit excessive light pollution disturbing ALL residents.
- **Loss of Privacy:** Proposed development 45-46 feet in height on a hillside at higher elevation than the current homes in the area will oversaturate the area and cause a severe loss of privacy.
- **Inadequate Parking:** Surrounding streets will be used for parking by residents visiting or living in the development.
- **Detrimental Impact on the Character of the Area and Overcrowding:** The proposed development of 42 homes is at the center of a small hillside residential area. The property is surrounded by single family homes in accordance to the character of the hillside area. The proposed development essentially decimates the existing hill, reshapes, recharacterizes and oversaturates the only green area within this central area. The population density will increase substantially creating overcrowding.
- **Ground Stability and Drainage:** The property is located in a liquefaction and landslide area. With the stress of the proposed buildings, the size proposed may cause instability for the proposed structures and damage to the current homes on the hillside.
- **Scenic Vistas Obscured:** Buildings 45 feet high built atop the highest point of the property will obscure scenic vistas and severely diminish the quality of life, as well as the character and values of homes in our hillside community.

I would like express my OBJECTION to the proposed General Plan Amendment, Zone/Height District change and the Adoption of the previously declared Mitigated Negative Declaration so that it is recorded as part of the official public record at the Department of City Planning's Public Hearing. Please retain the current zoning and height restrictions.

Thank you for your attention and consideration,

Esmeralda Vasquez

Resident, El Sereno

2372 N. Eastern Avenue

Los Angeles, CA 90032

(323)393-5774

ezmerv@yahoo.com

Additional Comment:

I am a current and lifelong resident of El Sereno and have inherently strong concerns regarding the resubdivision, rezoning and all subsequent exemptions to the current building permissions for the planned construction Case No: ENV-2015-1918-MND "Vista El Sereno".

I would submit the following personal comments on selected items from the above noted reasons:

- **Noise Pollution:** Barring a sound-proof bubble around the site, the loud sounds of construction will extend well past a 500 feet range from the land parcels. Sounds in the dale, along Eastern Avenue and neighboring streets, amplify as they echo throughout the area.

From my home, I can hear the sounds of playing on the field at El Sereno Park. I can also hear the PA system that conducts announcements at Wilson High School. Neighboring residents, both next door and those more than 500 feet all around these parcels, as well as students attempting to learn at the neighboring schools, will also be impacted by the noise pollution of such an extensive construction project.

- **Traffic Congestion / Street Safety:** The proposed development will surge the already crowded street and its nearby precarious intersections. The added influx of people to the area will undoubtedly decrease the safety of all students, pedestrians and commuters, not only during school day mornings and afternoons and "rush hour," but anytime, day or night, all week long.

As a resident on Eastern Avenue, I can attest to the increase of vehicles traveling on the street. Many pedestrians (students, adults, teens, seniors, families, and pets) use the street's sidewalks for walking and running for leisure and as form of commute to school or work.

- **Aesthetics of Landscape:** The current natural aesthetics of the landscape, including and around the hill will be reshaped, stepped and essentially destroyed to make way for this proposed development. The hill will be distressed to a degree affecting the environment and changing the ecosystem for plants, trees, as well as migratory birds and other animals.
- **Air Pollution:** Among the extreme requests, the most exorbitant that I have noted is the appeal to excavate and remove tens of thousands cubic feet of soil. The reported soil will very likely be transported along Eastern Avenue, the only thoroughway to the I-10, which is about one mile south, in order to reach its final destination. The large-sized trucks needed to move so much dirt would haul the soil across our community, passing my own home and those of hundreds of others for this single part of the construction.

I respectfully request the City Planning Commission to take into consideration the true interest of the properties' neighbors and community members of [El Sereno](#) as a whole. In all communication and action regarding this property, past and present, our community has expressed great concern for the current and future impact on the well-being of our community and environment. I have joined my neighbors in voicing our concerns, ever constant, to our general neighborhood council, neighborhood land use committee, and directly to the Los Angeles City Planning Commission.

It is unacceptable to rezone, apply any resubdivision, and permit the developer to construct in exception of what is currently allowed for structures on the properties. It is pertinent that the city protect our community's voice. —EV.

#####



Letter to City Planner 2020-01-23.pdf

29K

May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>
To: e V <ezmerv@yahoo.com>

Thu, Jan 23, 2020 at 6:36 AM

Hello,

Your comments have been received and included into the administrative record.

Sincerely,

May Sirinopwongsagon
(213)978-1372
Department of City Planning
Central Project Planning Bureau
200 N. Spring Street, Room 621
Los Angeles, CA 90012

[Quoted text hidden]

[Quoted text hidden]

Additional Comment:

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#####

Esmeralda Vasquez
2372 N. Eastern Avenue
Los Angeles, CA 90032

January 22, 2020

May Sirinopwongsagon, City Planner
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles, CA 90012

RE: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

Dear May Sirinopwongsagon:

Please accept this email and the corresponding letter (attached) as a formal objection to the above referenced project.

I object to the planned construction of ENV-2015-1918-MND "Vista El Sereno" development project at the intersection of Eastern Avenue and Lombardy Blvd. in El Sereno for the following reasons:

- **Traffic Congestion / Air pollution / Street Safety (pedestrian and driver):** Eastern Avenue and neighboring streets will experience excessive vehicle traffic each day. Traffic gridlock and congestion will become a daily problem. The entrance and exit from Lombardy Boulevard and Eastern Ave will increase vehicular traffic substantially.
- **Noise Pollution / Disturbance of the Peace:** The proposed location is a small raised hill at the center surrounded by hills that amplify the slightest sounds to all residents.
- **Light Pollution:** Lights at night from the proposed structures will emit excessive light pollution disturbing ALL residents.
- **Loss of Privacy:** Proposed development 45-46 feet in height on a hillside at higher elevation than the current homes in the area will oversaturate the area and cause a severe loss of privacy.
- **Inadequate Parking:** Surrounding streets will be used for parking by residents visiting or living in the development.
- **Detrimental Impact on the Character of the Area and Overcrowding:** The proposed development of 42 homes is at the center of a small hillside residential area. The property is surrounded by single family homes in accordance to the character of the hillside area. The proposed development essentially decimates the existing hill, reshapes, recharacterizes and oversaturates the only green area within this central area. The population density will increase substantially creating overcrowding.
- **Ground Stability and Drainage:** The property is located in a liquefaction and landslide area. With the stress of the proposed buildings, the size proposed may cause instability for the proposed structures and damage to the current homes on the hillside.

- **Scenic Vistas Obscured:** Buildings 45 feet high built atop the highest point of the property will obscure scenic vistas and severely diminish the quality of life, as well as the character and values of homes in our hillside community.

I would like express my OBJECTION to the proposed General Plan Amendment, Zone/Height District change and the Adoption of the previously declared Mitigated Negative Declaration so that it is recorded as part of the official public record at the Department of City Planning's Public Hearing. Please retain the current zoning and height restrictions.

Thank you for your attention and consideration,

Esmeralda Vasquez
Resident, El Sereno
2372 N. Eastern Avenue
Los Angeles, CA 90032
(323)393-5774
ezmerv@yahoo.com

Additional Comment:

I am a current and lifelong resident of El Sereno and have inherently strong concerns regarding the resubdivision, rezoning and all subsequent exemptions to the current building permissions for the planned construction Case No: ENV-2015-1918-MND "Vista El Sereno".

I would submit the following personal comments on selected items from the above noted reasons:

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From my home, I can hear the sounds of playing on the field at El Sereno Park. I can also hear the PA system that conducts announcements at Wilson High School. Neighboring residents, both next door and those more than 500 feet all around these parcels, as well as students attempting to learn at the neighboring schools, will also be impacted by the noise pollution of such an extensive construction project.

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As a resident on Eastern Avenue, I can attest to the increase of vehicles traveling on the street. Many pedestrians (students, adults, teens, seniors, families, and pets) use the street's sidewalks for walking and running for leisure and as form of commute to school or work.

- **Aesthetics of Landscape:** The current natural aesthetics of the landscape, including and around the hill will be reshaped, stepped and essentially destroyed to make way for this proposed development. The hill will be distressed to a degree affecting the environment and changing the ecosystem for plants, trees, as well as migratory birds and other animals.
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I respectfully request the City Planning Commission to take into consideration the true interest of the properties' neighbors and community members of El Sereno as a whole. In all communication and action regarding this property, past and present, our community has expressed great concern for the current and future impact on the well-being of our community and environment. I have joined my neighbors in voicing our concerns, ever constant, to our general neighborhood council, neighborhood land use committee, and directly to the Los Angeles City Planning Commission.

It is unacceptable to rezone, apply any resubdivision, and permit the developer to construct in exception of what is currently allowed for structures on the properties. It is pertinent that the city protect our community's voice. –EV.



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

2520 North Eastern Avenue (Eastern Avenue & Lombardy Boulevard) Case Nos. VTT-73531-SL; APCE-2015-2048-ZC-ZAD-ZAA

2 messages

jade wong <jw15260@gmail.com>
To: may.sirinopwongsagon@lacity.org

Tue, Jan 21, 2020 at 8:51 PM

Dear Ms. Sirinopwongsagon,

I am a resident of El Sereno and reside within 500 feet of the proposed development.

I am strongly opposed to the idea of this location, which is a hillside, being developed under the small lot development template. The surrounding homes in this area are mainly modest single-story homes and as such the proposed development will not be compatible with the surrounding homes and will overwhelm due to the massing of so many homes (42) on this hillside. The impact will not be as alarming from Eastern Avenue but the homeowners on surrounding streets (Harmony Lane and Mallory Street) will feel the intrusion keenly. My house faces a hillside and I can see up the hill the last house on the street Ladd Avenue. The house is modest in size and the lot is not large by any standards. A couple years ago, a developer purchased two lots on the street above on Kimball Street and built two multi-story homes utilizing as much area on each lot as was permitted. These homes loom over the backyard of that house on Ladd Avenue and as a result lost all its outdoor privacy because of the over-development of the lots above. Massing 42 homes on the Eastern & Lombardy hillside will produce the exact same outcome for the current surrounding homes

A neighbor of mine has pointed out to me that the developer has asked for zone changes and modifications to the existing permanent Qualified "Q" Conditions in order for this hillside to be flattened to such massive degree that would require 78,000 cubic yards of dirt be removed, when under current zoning and if the lots were subdivided into 42 lots, a maximum of 42,000 cubic yards of dirt would be permitted to be removed. Is this not trying to fit a square peg into a round hole?

Let this property be developed but to a much gentler degree. Please do not allow for the zone change from [Q]R1-1D & [Q]RD6-1D to [Q]RD5-1D and please do not remove the permanent Qualified "Q" Conditions, which would allow so much of this hillside to be destroyed. At a minimum, require the current zone [Q]R1-1D be retained to offer the surrounding single-family homes protection from infringement. This site is not right for a small lot development and definitely not one of this magnitude.

Sincerely,

Jade Wong

[4841 Klamath Place](#)

[Los Angeles, CA 90032](#)

jw15260@gmail.com



Letter to May Sirinopwongsagon - Eastern & Lombardy.pdf
320K

May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Thu, Jan 23, 2020 at 6:49 AM

To: jade wong <jw15260@gmail.com>

Hello,

Your comments have been received and included into the administrative record.

May Sirinopwongsagon
(213)978-1372
Department of City Planning
Central Project Planning Bureau
200 N. Spring Street, Room 621
Los Angeles, CA 90012

[Quoted text hidden]

Jade Wong
4841 Klamath Place
Los Angeles, CA 90032
jw15260@gmail.com

January 21, 2020

Ms. May Sirinopwongsagon, City Planner
Via email: may.sirinopwongsagon@lacity.org

Re: 2520 North Eastern Avenue (Eastern Avenue & Lombardy Boulevard)
Case Nos. VTT-73531-SL; APCE-2015-2048-ZC-ZAD-ZAA

Dear Ms. Sirinopwongsagon,

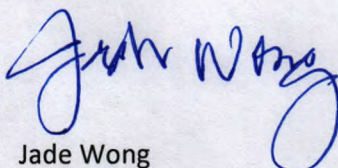
I am a resident of El Sereno and reside within 500 feet of the proposed development.

I am strongly opposed to the idea of this location, which is a hillside, being developed under the small lot development template. The surrounding homes in this area are mainly modest single-story homes and as such the proposed development will not be compatible with the surrounding homes and will overwhelm due to the massing of so many homes (42) on this hillside. The impact will not be as alarming from Eastern Avenue but the homeowners on surrounding streets (Harmony Lane and Mallory Street) will feel the intrusion keenly. My house faces a hillside and I can see up the hill the last house on the street Ladd Avenue. The house is modest in size and the lot is not large by any standards. A couple years ago, a developer purchased two lots on the street above on Kimball Street and built two multi-story homes utilizing as much area on each lot as was permitted. These homes loom over the backyard of that house on Ladd Avenue and as a result lost all its outdoor privacy because of the over-development of the lots above. Massing 42 homes on the Eastern & Lombardy hillside will produce the exact same outcome for the current surrounding homes

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Let this property be developed but to a much gentler degree. Please do not allow for the zone change from [Q]R1-1D & [Q]RD6-1D to [Q]RD5-1D and please do not remove the permanent Qualified "Q" Conditions, which would allow so much of this hillside to be destroyed. At a minimum, require the current zone [Q]R1-1D be retained to offer the surrounding single-family homes protection from infringement. This site is not right for a small lot development and definitely not one of this magnitude.

Sincerely,



Jade Wong



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Project Site: 2520 N. Eastern Ave / Case no. VTT-73531-SL

2 messages

K Williams <williamskeshia@gmail.com>
To: may.sirinopwongsagon@lacity.org

Tue, Feb 4, 2020 at 4:24 PM

TO: May Sirinopwongsagon
City Planner
[200 N. Spring Street, Room 621](#)
[Los Angeles, CA 90012](#)
Via email to: may.sirinopwongsagon@lacity.org

Hello,

As a resident of and registered voter in the El Sereno neighborhood of CD-14, I am writing **to OPPOSE the new construction of 42 small lot homes located at:**
Project Site: [2520 North Eastern Avenue](#) (and 2532, 2608, and [2668 N. Eastern Avenue](#) and 2647-2651 N. Lombardy Boulevard)
Case Nos.: VTT-73531-SL; APCE-2015-2048-ZC-ZAD-ZAA
CWQA No.: ENV-2015-1918-MND

I OPPOSE pursuant to Los Angeles Municipal Code Section 17.03 and 17.15, Vesting Tentative Tract Map No. 73531-SL for the merger and resubdivision into 42 small lots pursuant to LAMC Section 12.22 C27 (Ordinance No. 176.354) for the construction of 42 small lot homes and pursuant to LAMC section 17.13, the approval of a haul route.

The proposed project requires the removal of 78,000 cubic yard of earth as well as 39 Protected Trees in order to secure the site
for physical development. This is outside of the norm. The local hillside areas contain more restrictive land use designations and
development standards because of the varied topography, potential geologic instability and restricted access.

I OPPOSE the Zone Change from [Q]R1-1D and [Q]RD6-1D to [Q]RD5-1D

The area is designated for Low density residential development. This zone change is not consistent with public necessity, convenience,
general welfare and good zoning practice of existing neighborhood..

I OPPOSE modifications to the existing permanent qualified "Q" conditions contained within ordinance no. 180,403

The project site is zoned [Q]R1-1D and [Q]RD6-1D (Low Density and Restricted Density Residential Zones). The project must adhere to
the associated Qualified "[Q]" Conditions and Development "D" limitations which restrict development on the site to the requirements of
City Ordinance No. 180,403 - Northeast Hillside Ordinance.

I OPPOSE pursuant to LAMC Section 12.24 X.26, a Zoning Administrators determination to permit a maximum of 178 retaining walls varying in height from less than 3 feet to 6 feet with approximately 103 of these walls including an additional six (6) foot high privacy/security fence on top of the retaining walls.

The additional 6 feet are in excess of what is currently permissible and not in character with the neighborhood.

I OPPOSE pursuant to LAMC Section 12.28 a Zoning Administrator's adjustment to permit retaining walls varying in height from less than 3 feet to 6 feet with an additional maximum of six (6) foot privacy/security fence on top of the retaining walls within the required front, side, and rear yards in lieu of the maximum 42 inches in the front yard and 6 feet in the side and rear yards pursuant to the LAMC Section 12.21 C.1(g)

The additional 6 feet are in excess of what is currently permissible in the Northeast Hillside ordinance and not in keeping with the character of the neighborhood.

I OPPOSE the removal of 39 Protected Trees (Black Walnut trees) as well as the existing green space, trees, grass and shrubbery that play an important role in maintaining the air quality in the El Sereno neighborhood of Los Angeles.

The project must also receive approval from the Bureau of Street Services Urban Forestry Division pursuant to LAMC

City Ordinance No. 177,404 (Protected Trees).

Excessive traffic will be caused by potential homeowners entering and exiting the project site on busy Eastern Avenue. Eastern Avenue is a designated, improved Secondary Highway with two lanes of traffic in both the southbound and northbound directions. Both Farmdale Elementary and El Sereno Middle school are located on Eastern Avenue directly across the street from the proposed project site at the corner of Lombardy Avenue. The addition of cars and congestion caused by the proposed project will pose grave danger to the children who walk to and from school every day. Additionally, the location of the entry to the proposed housing development, which will sit very near to the traffic light intersection of Eastern & Lombardy and will cause traffic issues, including blocking the sidewalk making it inaccessible for people to use. Furthermore, there is a bus stop located on the corner of Eastern Avenue and Lombardy Avenue - directly in front of the proposed project site. The car traffic from the proposed project site will have a negative impact on bus accessibility.

The applicant The True Life Companies/Rob Flitton has built their business completing entitlements in a community and then quickly flipping the land to other real estate developers. The True Life Companies (aka TTLC), headquartered in Colorado, has taken advantage of the California real estate market and the need for affordable housing in order to reap extreme profits for their company while overusing land in California. As noted on The True Life Companies website, their mission statement is to "provide economic well being for all stakeholders".

Since 2015, The True Life Companies has purchased land throughout the state of California, extracted changes to land use regulations, variances and completed entitlements and then sold the land to other housing developers for a hefty financial profit. TTLC does not invest in neighborhoods to build viable, sustainable and affordable communities.

Here are examples of The True Life Companies most recent projects in California:

GALT, CA

- 2019: The True Life Companies (aka TTLC) bought land site they branded as the "Cardoso I" in 2019, then **immediately sold to KB Homes in 2019**

SANTA CLARA, CA

- 2015: The True Life Companies (TTLC) bought a land site they branded as "Kifer" in 2015; **TTLC sold the land to Toll Brothers in 2017**

SAN JOSE, CA

- 2015: The True Life Companies acquired land site branded as "Dobbin Drive" in 2015, they **sold to Pulte Homes in 2015. TTLC demanded variances etc to increase the number of homes permitted on the site from 125 to 131.**

- 2015: The True Life Companies acquired land site they branded as "Mahuron" in 2015; they **immediately sold to Pulte Homes in 2015**

BRENTWOOD, CA

- 2015: The True Life Companies acquired land site branded as "Sellers Avenue" in 2015; they **immediately sold to Heritage Homes in 2015**

FREMONT, CA

- 2014: The True Life Companies acquired land site they branded as "Mission Stevenson" in 2014; TTLC **sold to TRI Point Homes in 2016**

I thank you for your attention to this matter and hope you will **not approve** Case Nos.: VTT-73531-SL; APCE-2015-2048-ZC-ZAD-ZAA plans for El Sereno.

Thank you,
Keshia Williams
Resident and Registered Voter in Council District 14

May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>
To: K Williams <williamskeshia@gmail.com>

Tue, Feb 11, 2020 at 1:50 PM

Hello,

Your comments have been received and included into the record.

Sincerely,

May Sirinopwongsagon
(213)978-1372
Department of City Planning
Central Project Planning Bureau
200 N. Spring Street, Room 621
Los Angeles, CA 90012

[Quoted text hidden]



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Case ENV-2015-1918-MND "Vista El Sereno"

1 message

Moncada, Patricia (A&I-MRB)@DHCS <Patricia.Moncada@dhcs.ca.gov>
To: "may.sirinopwongsagon@lacity.org" <may.sirinopwongsagon@lacity.org>

Wed, Jan 22, 2020 at 2:59 PM

Good afternoon,

Please see my attached notice.

Thank you very much.

Best Regards,

Patricia Moncada

El Sereno Resident.



ENV-2015-1918-MND Vista El Sereno.pdf

445K

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

Dear Ms. Sirinopwongsagon:

I object to the planned construction of ENV-2015-1918-MND "Vista El Sereno" development project at the intersection of Eastern Avenue and Lombardy Blvd. in El Sereno for the following reasons:

- **Traffic Congestion / Air pollution / Street Safety (pedestrian and driver):** Eastern Avenue and neighboring streets will experience excessive vehicle traffic each day. Traffic gridlock and congestion will become a daily problem. The entrance / exit from Lombardy Blvd. and Eastern Ave will increase vehicular traffic substantially.
- **Noise Pollution /Disturbance of the Peace:** the proposed location is a small raised hill at the center surrounded by hills that amplify the slightest sounds to all residents.
- **Light Pollution:** Lights at night from the proposed structures will excessive light pollution disturbing ALL residents.
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- **Detrimental impact on the character of the area and Overcrowding:** The proposed development of 42 Homes is at the center of a small hillside residential area. The property is surrounded by single family homes in accordance to the character of the hillside area. The proposed development essentially decimates the existing hill, reshapes, recharacterizes and oversaturates the only green area within this central area. The population density will increase substantially creating overcrowding.
- **Ground Stability and Drainage:** The property is located in a liquefaction and landslide area and with the proposed stressed of buildings, the size proposed may cause instability for the proposed structures and damage to the current homes on the hillside.
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I would like express my OBJECTION to the proposed General Plan Amendment, Zone/Height District change and the Adoption of the previously declared Mitigated Negative Declaration so that it is recorded as part of the official public record at the Department of City Planning's Public Hearing. Please retain the current zoning and height restrictions.

Sincerely,

Resident

Patricia Moncada

Printed Name

2562 Lombardy Blvd.

Address

Los Angeles, CA 90032

My Additional Comments:

The children in my family have very bad asthma and allergies. The increased air pollution due to the proposed development
Would contribute to our children's health issues. Our local streets are already congested. Additional housing will also make
Traffic congestion worse. Please do not approve the proposed development. Our area is congested enough. Thank you for
Your time and consideration.



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Case No: VTT-73531-SL;APCE-2015-2048-ZC-ZAD-ZAA (3) Letters for Objection to Development Plans

2 messages

Reina Gomez <reinag@commscapeinc.com>

Wed, Jan 22, 2020 at 4:47 PM

Reply-To: reinag@commscapeinc.com

To: may.sirinopwongsagon@lacity.org

Cc: carmen.c.tata@gmail.com

Hello,

Please see the letters attached, Objecting to Case No. VTT-73531-SL;APCE-2015-2048-ZC-ZAD-ZAA.
Thank you.

Sincerely,

Reina Gomez

Secretary & Treasurer



Tel. (323) 225-5699 Ext. 10 Fax. (323) 225-5952

5315 Valley Blvd., Los Angeles, CA 90032

reinag@commscapeinc.com

Please Visit us at: www.commscapeinc.com

3 attachments



Richard Padilla Objection Letter for Case No.VTT-73531-SL;APCE-2015-2048-ZC-ZAD-ZAA.pdf

100K



Amanda Sandoval Objection Letter for Case No. VTT-73531-SL;APCE-2015-2048-ZC-ZAD-ZAA_012220.pdf

103K



Reina Gomez Objection Letter for Case No. VTT-73531-SL;APCE-2015-2048-ZC-ZAD-ZAA_012220.pdf

97K

May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Thu, Jan 23, 2020 at 6:43 AM

To: reinag@commscapeinc.com

Hello,

The comments have been received and included into the administrative record.

May Sirinopwongsagon
(213)978-1372
Department of City Planning
Central Project Planning Bureau
200 N. Spring Street, Room 621
Los Angeles, CA 90012

[Quoted text hidden]

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

January 22, 2020

Re: **Letter Opposing** Case No: VTT-73531-SL;APCE-2015-2048-ZC-ZAD-ZAA

Dear Ms. Sirinopwongsagon:

I object to the planned construction of VTT-73531-SL;APCE-2015-2048-ZC-ZAD-ZAA development project at the intersection of Eastern Avenue and Lombardy Blvd. in El Sereno for the following reasons:

- **Traffic Congestion / Air pollution / Street Safety (pedestrian and driver):** Eastern Avenue and neighboring streets will experience excessive vehicle traffic each day. Traffic gridlock and congestion will become a daily problem. The entrance / exit from Lombardy Blvd. and Eastern Ave will increase vehicular traffic substantially.
- **Noise Pollution /Disturbance of the Peace:** the proposed location is a small raised hill at the center surrounded by hills that amplify the slightest sounds to all residents.
- **Light Pollution:** Lights at night from the proposed structures will excessive light pollution disturbing ALL residents.
- **Loss of Privacy:** Proposed development 45-46 feet in height on a hillside at higher elevation than the current homes in the area will over-saturate the area and cause a severe loss of privacy.
- **Inadequate Parking:** Surrounding streets will be used for parking by residents visiting or living in the development.
- **Detrimental impact on the character of the area and Overcrowding:** The proposed development of 42 Homes is at the center of a small hillside residential area. The property is surrounded by single family homes in accordance to the character of the hillside area. The proposed development essentially decimates the existing hill, reshapes, recharacterizes and oversaturates the only green area within this central area. The population density will increase substantially creating overcrowding.
- **Ground Stability and Drainage:** The property is located in a liquefaction and landslide area and with the proposed stressed of buildings, the size proposed may cause instability for the proposed structures and damage to the current homes on the hillside especially when 78,000 cubic yards of dirt will be exported.
- **Scenic Vistas Obscured:** Buildings 45 feet high built atop the highest point of the property will obscure scenic vistas and severely diminish the quality of life as well as the character and values of homes in the hillside community. If anyone would stop thinking about MONEY and just take a look around they would see that the last thing our already overcrowded and over congested community needs is anymore housing. Especially "Boxes in the Sky" which we know are not affordable housing. We have a serious homeless crisis, just take a drive on Huntington Dr.
- **Environment:** This proposed development requires the removal of 39 of 102 Protected Trees (Black Walnut Trees) located on the site which are protected for a reason, to not be removed or developed on. Not to mention the Development company is trying to change the zoning for this area.

I would like express my OBJECTION to the proposed General Plan Amendment, Zone/Height District change and the Adoption of the previously declared Mitigated Negative Declaration so that it is recorded as part of the official public record at the Department of City Planning's Public Hearing. Please retain the current zoning and height restrictions.

Sincerely,



Resident

REINA GOMEZ

Printed Name

2718 BUDDA AVE.

Address

Los Angeles, CA 90032

My Additional Comments:

- This proposal is just another Property Developer based in Newport Beach no-less, and they have no idea what it is like to be a resident here; that these "Developments" are less a development and more of a destruction of the City's oldest community. These kinds of "Developments" often lead to Gentrification; rent increases which can lead to displacement of current residents and now the Rich outsiders have moved in. If the argument is that this is "adding value"; adding value for who? Certainly not the current residents of El Sereno. The "adding value" ploy is purely a gimmick to push for luxury housing on a hilltop with a view and to charge good money for it. My research has also led me to the fact that there is a tax credit for anyone who invests in an Enterprise Zone (which I believe this land is) again; a monetary benefit for Mr. Rob Flitton and his Development Co. I live within a 500 foot radius of this hill and I will be one of the many residents negatively impacted by this kind of development. I strongly oppose building "Money Maker/Boxes in the Sky" on our open hills; we love them, and sometimes that is the only pretty/peaceful part of our day. Please leave our hill be we have enough housing, too much in fact. Check your values at the end of the day, because this is not in the best interest of your/my people of El Sereno, this is all for the benefit of Mr. Rob Flitton and The True Life Development Company's bank accounts, not for the old Historic town of El Sereno.

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Resident


Amanda Sandoval

Printed Name

3314 Minto Ct.

Address

Los Angeles, CA 90032

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Sincerely,

Resident

RICHARD PADILLA

Printed Name

5245 HUNTINGTON DR. S.

Address

Los Angeles, CA 90032

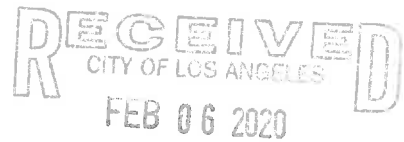
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Thank you.

January 28, 2020

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012



Re: Letter Opposing Case No: ENV-2015-1918-MND "Vista El Sereno"

CITY PLANNING
PROJECT PLANNING

Ms. Sirinopwongsagon,

The following reasons are why I **oppose** the major development on Eastern Ave and Lombardy Blvd.:

If approved this means the CITY OF LOS ANGELES will be going AGAINST its own Ordinances and recommendations:

Against - NELA Hillside ordinance # 184802, stating that it is OKAY to remove rare trees, greenery and animal species that currently live on this site.

Against the letter of recommendation from THE SANTA MONICA CONSERVANCY - that recommends saving the natural habitat area, submitted in 2016.

Against - Opposition letter from Empower LA 90032 – the VOICE of 90032, submitted in 2016.

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Against – CITY OF LOS ANGELES (Land Development Engineering, Edmund Yew inter correspondence, dated April 10, 2016 – stating the developer will take the public right of way if approved and not recommended.

Against -- AFFORDABLE HOUSING.... This is what the city is asking for !!!

The City of Los Angeles, will allow the developer to take 2 of 5 public parking spot on the Eastern Avenue side, so the new construction can access their homes. You will be adding to the limited parking for existing residence.

The city of Los Angeles, will be allowing 42 new homes to be built, with no public parking on their site!!! BIG PROBLEM and adding to the problem on this street.

Sincerely,

X

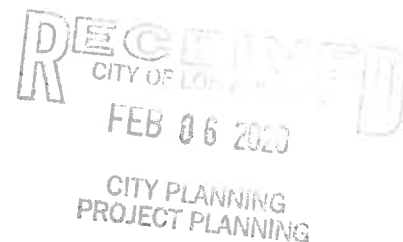

Rosemarie Gonzales

(Print Name and address)

2428 N. Eastern Ave., Los Angeles, CA 90032

January 28, 2020

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012



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The city will be approving the removal of 72,000 cubic feet of dirt....the entire hillside!

The city will be going against the stakeholders of El Sereno, as you see we provided letters to the city signed by the 90032 Board. In 2016 and 2019 those were forwarded to you. The development will go against the city recommendation of no....because they will be taking Public Right of Way.

The development will take at least 2 of the 5 public parking spots on the Eastern Avenue side, according to the plans. (this is how residents will enter their property)

You will approve the use of 120 + retaining walls (?) , where the max is 5 or less. The selected area of Eastern Avenue is an extremely dangerous curved main street.

Green open space will be taken by this move. The area of 90032 is currently in a shortage, regarding parks, park-lets, dog parks or green space.

You will be approving a development that is NOT AFFORDABLE housing!! That's what the city says we need.

Once again I am highly **opposed** to this project for the above stated reasons. Please keep these in mind while working on this case, and as you know this is the same project that's been trying for many years, to change the face of our community!

Sincerely,

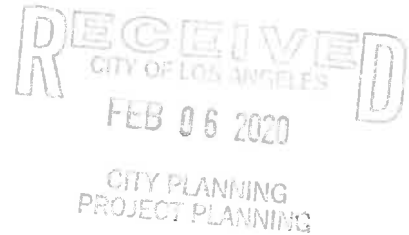
X

Print Name Deionse Hernandez

Address 2841 Chadwick Cir

h.t. 10032
90032

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Los Angeles Department of City Planning
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Los Angeles CA 90012



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Sincerely,

Resident

Celia Mata

Printed Name

3515 Winchester Ave

Address

Los Angeles, CA 90032

My Additional Comments:

January 28, 2020

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

RECEIVED
CITY OF LOS ANGELES
FEB 06 2020

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Sincerely,

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Zenaida Romero

(Print Name and address)

3906 Rambos Dr. LA . 90063

January 28, 2020

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Los Angeles Department of City Planning
200 N. Spring Street, Room 621
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Once again I am highly **opposed** to this project for the above stated reasons. Please keep these in mind while working on this case, and as you know this is the same project that's been trying for many years, to change the face of our community!

Sincerely,

X

Print Name Raquel Garcia

Address 3519 Winchester Ave. # Los Angeles,
CA 90032



January 28, 2020

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Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

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Sincerely,

X Margarita Thymiakes

Print Name

Margarita Thymiakes

Address

4644 Paula St.

Los Angeles, CA. 90032



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X Leticia G. Garcia

Print Name Leticia G. Garcia

Address 3319 Fithian Ave.

Los Angeles, CA 90032



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Sincerely,

Resident

Rosio Vazquez

Printed Name

3029 Pyrites St

Address

Los Angeles, CA 90032

My Additional Comments:



January 28, 2020

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Once again I am highly **opposed** to this project for the above stated reasons. Please keep these in mind while working on this case, and as you know this is the same project that's been trying for many years, to change the face of our community!

Sincerely,

X

Print Name

Address

Angélica Mata
Angélica Mata
3515 Winchester Ave
Los Angeles, Ca 90032

RECEIVED
CITY OF LOS ANGELES
FEB 06 2020
CITY PLANNING
PROJECT PLANNING

January 28, 2020

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: Letter Opposing Case No: ENV-2015-1918-MND "Vista El Sereno"

Ms. Sirinopwongsagon,

The following reasons are why I **oppose** the major development on Eastern Ave and Lombardy Blvd.:

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The city will be approving the removal of 72,000 cubic feet of dirt....the entire hillside!

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Sincerely,

X

Print Name Cristal Garcia

Address 4798 Converse St.

Los Angeles CA 90032



Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

Dear Ms. Sirinopwongsagon:

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I would like express my OBJECTION to the proposed General Plan Amendment, Zone/Height District change and the Adoption of the previously declared Mitigated Negative Declaration so that it is recorded as part of the official public record at the Department of City Planning's Public Hearing. Please retain the current zoning and height restrictions.

Sincerely,

My

Resident

Michael Alex Garcia
Printed Name

4798 Converse St
Address
Los Angeles, CA 90032

My Additional Comments:



January 28, 2020

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: Letter Opposing Case No: ENV-2015-1918-MND "Vista El Sereno"

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Against -- AFFORDABLE HOUSING.... This is what the city is asking for !!!

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The city of Los Angeles, will be allowing 42 new homes to be built, with no public parking on their site!!! **BIG PROBLEM** and adding to the problem on this street.

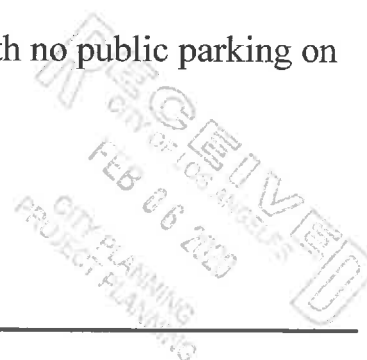
Sincerely,

X Rebecca Sapp

Rebecca Sapp

(Print Name and address)

2505 Lombardy Blvd 90032



January 28, 2020

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

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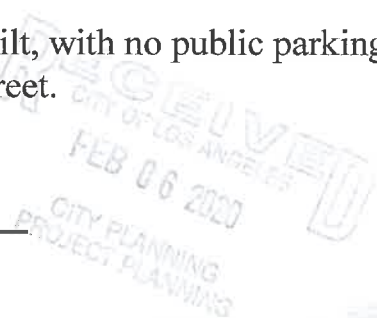
X



Guadalupe Villacinda

(Print Name and address)

3948 Guardia Ave. LA. 90032



January 28, 2020

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

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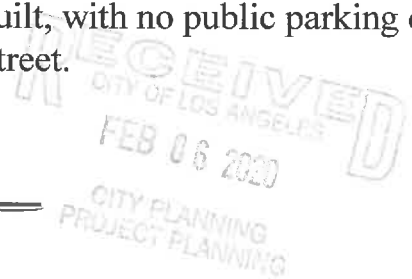
Sincerely,

X


Isabella Chamorro

(Print Name and address)

3419 Galeana St. Los Angeles, CA 90032



January 28, 2020

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: Letter Opposing Case No: ENV-2015-1918-MND "Vista El Sereno"

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Sincerely,

X

Herman Luna

(Print Name and address)

1336 Huntington Drive Los Angeles 90032



January 28, 2020

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012



Re: Letter Opposing Case No: ENV-2015-1918-MND "Vista El Sereno"

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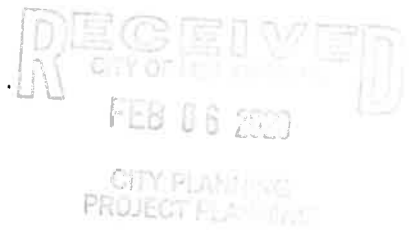
SILVIA MATA

3515 Winchester Ave LA CA 90032

(Print Name and address)

January 28, 2020

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012



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Sincerely,

X Consuelo Huerta

Print Name Consuelo Huerta

Address 2580 N. Soto St. Los Angeles
CA 90032

January 28, 2020

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012



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Sincerely,

X Rosa E. Enriquez de Garcia

Print Name Rosa E. Enriquez de Garcia

Address 3519 Winchester Ave.

Los Angeles, CA 90032

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

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Sincerely,

Resident

ANSLEY DAVIES

Printed Name

2753 Ballard St.

Address

Los Angeles, CA 90032

My Additional Comments:

We need parkland and walkability. There are
no sidewalks!

SUBMITTED AT
PUBLIC HEARING

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

SUBMITTED AT
PUBLIC HEARING

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

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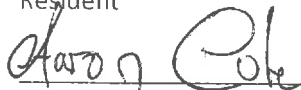
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Sincerely,

Resident



Printed Name

4618 Klamath st

Address

Los Angeles, CA 90032

My Additional Comments:

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

SUBMITTED AT
PUBLIC HEARING

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

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Sincerely,

Resident

Annamarie Riza

Printed Name

2644 Lombardy Bl.

Address

Los Angeles, CA 90032

My Additional Comments:

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

SUBMITTED AT
PUBLIC HEARING

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Resident

Printed Name

Address

Los Angeles, CA 90032

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Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

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Sincerely,

Resident

Ruben + Marleny Riera
Printed Name

5071 Kimball St Los Angeles CA 90032
Address
Los Angeles, CA 90032

My Additional Comments:

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

Dear Ms. Sirinopwongsagon:

I object to the planned construction of ENV-2015-1918-MND "Vista El Sereno" development project at the intersection of Eastern Avenue and Lombardy Blvd. in El Sereno for the following reasons:

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Sincerely,

Resident

Irma Rivera Alvarez
Printed Name

5071 Kimball St. Los Angeles, CA 90032
Address
Los Angeles, CA 90032

My Additional Comments:

Please leave our city alone. We have worked hard to make it what it is
and we do not need more pollution or loss of privacy.

SUBMITTED AT
PUBLIC HEARING

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

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Sincerely,

Resident

Dalila Mera
Printed Name

5065 Kimball st.
Address
Los Angeles, CA 90032

SUBMITTED AT
PUBLIC HEARING

My Additional Comments:

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

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Sincerely,

Resident

Daniel Melendez

Printed Name

Daniel Melendez

Address

Los Angeles, CA 90032

5065 Kimball ST.
Los Angeles CA, 90032

My Additional Comments:

SUBMITTED AT
PUBLIC HEARING

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

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Sincerely,

Resident

William R Chriskey

Printed Name

5060 Kimball Ave

Address

Los Angeles, CA 90032

My Additional Comments:

SUBMITTED AT
PUBLIC HEARING

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

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Sincerely,

Resident

2525 Lombardy Blvd.
Printed Name

Hector Celaya
Address

Los Angeles, CA 90032

SUBMITTED AT
PUBLIC HEARING

My Additional Comments:

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

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Sincerely,

Resident

YARO Celis

Printed Name

2570 Malloy St

Address

Los Angeles, CA 90032

My Additional Comments:

SUBMITTED AT
PUBLIC HEARING

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

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Sincerely,

Resident

Joseph Cruz
Printed Name

2567 Lombardy Blvd
Address
Los Angeles, CA 90032

**SUBMITTED AT
PUBLIC HEARING**

My Additional Comments:

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

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Sincerely,



Resident

Joshua Kleiner

Printed Name

2474 N. Ditman Ave

Address

Los Angeles, CA 90032

**SUBMITTED AT
PUBLIC HEARING**

My Additional Comments:

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

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Sincerely,

Resident

Junli Wang

Printed Name

4851 Klamath Pl

Address

Los Angeles, CA 90032

My Additional Comments:

**SUBMITTED AT
PUBLIC HEARING**

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

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Sincerely,

Resident

DYLAN TAYLOR

Printed Name

2417 N DITMAN AVE

Address

Los Angeles, CA 90032

My Additional Comments:

SUBMITTED AT
PUBLIC HEARING

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

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Sincerely,

Resident

Adam Wires
Printed Name

4853 Klanath St
Address
Los Angeles, CA 90032

My Additional Comments:

SUBMITTED AT
PUBLIC HEARING

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

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Sincerely,

Resident

Carlos Martinez

Printed Name

2471 N Azusa Ave LA CA, 90032

Address

Los Angeles, CA 90032

**SUBMITTED AT
PUBLIC HEARING**

My Additional Comments:

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

Dear Ms. Sirinopwongsagon:

I object to the planned construction of ENV-2015-1918-MND "Vista El Sereno" development project at the intersection of Eastern Avenue and Lombardy Blvd. in El Sereno for the following reasons:

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Sincerely,

Resident

Gumaro Rosales

Printed Name

4601 Klamath St

Address

Los Angeles, CA 90032

My Additional Comments:

**SUBMITTED AT
PUBLIC HEARING**

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

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Sincerely,

Resident

Maria Granda
Printed Name

2475 N Dittman
Address
Los Angeles, CA 90032

SUBMITTED AT
PUBLIC HEARING

My Additional Comments:

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

Dear Ms. Sirinopwongsagon:

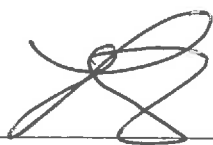
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Sincerely,

Resident

 Laura Lopez

Printed Name

2435 N Ditman Ave

Address

Los Angeles, CA 90032

SUBMITTED AT
PUBLIC HEARING

My Additional Comments:

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

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Sincerely,



Resident

Keshia Williams

Printed Name

2444 N. Ditman Ave

Address

Los Angeles, CA 90032

My Additional Comments:

**SUBMITTED AT
PUBLIC HEARING**

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

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Sincerely,

Resident

ALVIN CASTRO
Printed Name

2422 N. BITMAN AVE.
Address
Los Angeles, CA 90032

SUBMITTED AT
PUBLIC HEARING

My Additional Comments:

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

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Sincerely,

Resident

Robert Deacon

Printed Name

2429 N. Dibun Ave.

Address

Los Angeles, CA 90032

SUBMITTED AT
PUBLIC HEARING

My Additional Comments:

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

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Sincerely,

Resident

Esperanza Bandera

Printed Name

2453 N. Pitman Ave

Address

Los Angeles, CA 90032

SUBMITTED AT
PUBLIC HEARING

My Additional Comments:

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

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Sincerely,

Resident

ANTHONY REZA
Printed Name

2644 LOMBARDOY BLVD.
Address

Los Angeles, CA 90032

SUBMIT
PUBLIC H

My Additional Comments:

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

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Sincerely,

Resident

Alice Rozz

Printed Name

2644 Lombardy Blvd

Address

Los Angeles, CA 90032

SUBMITTED AT
PUBLIC HEARING

My Additional Comments:

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

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Sincerely,

Dm gnerro

Resident

Dmarie Gnerro

Printed Name

5020 Bndan Pl. - L.A. 90032

Address

Los Angeles, CA 90032

My Additional Comments:

*goes against neighbor hood views,
break up environment of hillside - vegetation
wild life*

SUBMITTED AT
PUBLIC HEARING

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

Re: **Letter Opposing** Case No: ENV-2015-1918-MND "Vista El Sereno"

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Sincerely,

Resident

FERNANDO GUERRERO

Printed Name

5020 BUDAU PLACE

Address

Los Angeles, CA 90032

My Additional Comments:

SUBMITTED AT
PUBLIC HEARING

Ms. May Sirinopwongsagon
Los Angeles Department of City Planning
200 N. Spring Street, Room 621
Los Angeles CA 90012

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- **Ground Stability and Drainage:** The property is located in a liquefaction and landslide area and with the proposed stressed of buildings, the size proposed may cause instability for the proposed structures and damage to the current homes on the hillside.
- **Scenic Vistas Obscured:** Buildings 45 feet high built atop the highest point of the property will obscure scenic vistas and severely diminish the quality of life as well as the character and values of homes in the hillside community.

I would like express my OBJECTION to the proposed General Plan Amendment, Zone/Height District change and the Adoption of the previously declared Mitigated Negative Declaration so that it is recorded as part of the official public record at the Department of City Planning's Public Hearing. Please retain the current zoning and height restrictions.

Sincerely,

Ignacio Guerrero
Resident

Ignacio Guerrero
Printed Name

5020 Budan Place - 90032
Address
Los Angeles, CA 90032

SUBMITTED AT
PUBLIC HEARING

My Additional Comments:

cutting down hillside - against codes
NE Hillside Ordinance

Sun Oi Kwan
4831 Klamath Place
Los Angeles, CA 90032

January 23, 2020

Ms. May Sirinopwongsagon, City Planner
Los Angeles City Hall
200 N. Spring Street, Room 1070
Los Angeles, CA 90012
Via: Hand delivery

Re: 2520 North Eastern Avenue (Eastern Avenue & Lombardy Boulevard)
Case Nos. VTT-73531-SL; APCE-2015-2048-ZC-ZAD-ZAA

Dear Ms. Sirinopwongsagon,

I live very close to the property proposed for major change. There is much need according to the news for more homes to be built but, this location is not the right place to build small lot homes. I have seen the pictures of the boxy houses squished so close together that sunlight will not shine on the ground sitting high above lower elevation homes. These houses are too tall and will be bad neighbors for the people that live next door. The homes surrounding this property are single story and what is built next should also be low-level in height, so neighbors are not looking into other neighbor's backyards.

We have two other problems in our area, and they not enough parking and noise.

There is not enough parking. Half my street has "NO Parking from 8 pm to 8 am" but there's not enough parking so people park anyways and hope to not get a ticket. Building so many homes on a property that cannot offer enough parking does not make people get rid of their cars. It makes it hard for families to have friends and family over. People will not want to spend so much money to buy a house where they don't have enough parking. More parking is needed on the hill because there is not enough parking on the streets around the hill.

Sound carries so far in this area. I can hear the whistles that the referees blow during soccer at the El Sereno Recreation Center across the street on Eastern Avenue and I can hear the clack as skateboards hit the cement ramps at the skateboard park. I can hear my neighbors on their balconies two streets up on Kimball. Removing so much dirt to flatten the hill will make the houses they build that much closer. How do you not destroy the peace and quality of life for the current homeowners when 42 full size homes are packed on a hillside in your backyard?

I want to thank you if you can stop this development for small lot homes. It is just too much.

Sincerely,

Sun Oi Kwan



**SUBMITTED AT
PUBLIC HEARING**

January 23, 2020

Ms. May Sirinopwongsagon, City Planner
Los Angeles City Hall
200 N. Spring Street, Room 1070
Los Angeles, CA 90012
Via: Hand delivery

Re: 2520 North Eastern Avenue (Eastern Avenue & Lombardy Boulevard)
Case Nos. VTT-73531-SL; APCE-2015-2048-ZC-ZAD-ZAA

Dear Ms. Sirinopwongsagon,

My house is very close to the property referenced above and received your letter to notify what a developer wants to do to this hillside. I have lived here many years and raised my children here. El Sereno is my home and I want the best for its future development.

My family and I do not think it is a good location to build 42 houses, it is just too many. 20 houses with room to park cars on the street(s) on the hill is a better solution. We do not have enough parking already. Public transportation is not good enough that people will give up their cars. Overflow of parking will impact the surrounding streets.

I also am concerned that the hillside soil is not stable enough to build so many tall houses. My house was built in the 60's and there is always cracking because the soil keeps swelling when it rains and shrinks when it is dry. It doesn't matter if the house is built when my house was built or in more recent times like the ones on Kings Place down the street. Can the hillside hold up so much weight without causing damage to the homes? My neighbor says our homes are built on liquefaction soil and that is why we have new cracks every year, why our retaining walls are leaning. What happens when there is cracking and sinking on the hill with 42 homes?

Thank you for your time and we hope you can do better for our neighborhood than what this developer wants.

Sincerely,



Connie Gor and family

4837 Klamath Place

Los Angeles, CA 90032

**SUBMITTED AT
PUBLIC HEARING**



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Urgent: Please help, harassment and nuisances from Builders in El Sereno 90032

1 message

Carmen Tata <carmen.c.tata@gmail.com>

Fri, Apr 12, 2019 at 9:43 AM

To: cpc@lacity.org, info@da.lacounty.gov, may.sirinopwongsagon@lacity.org, lynda.smith@lacity.org

Attention: Planning Commission, District Attorney, May,

I am writing this email as a matter of utmost urgency to inform and investigate a serious and deeply concerning issue in regards to the project being pursued at the corner of Eastern Ave and Lombardy Boulevard in El Sereno. Although this project was rejected, considered detrimental to this area in terms of livability, environmental impact, etc and defeated at city hall (in 2014-2015, Lynda Smith Case No: CPC-2011-3128-GPA-ZC-HD / Env. Case No: ENV-2011-3129-MND) after hundreds of hours of effort by the residents and other stakeholders, a petition and letters from hundreds of residents citing issues with this plan, it seems to continue to be pursued to a point of harassment. The most recent effort was on March 20th 2019.

I am one of ~500 stakeholders who signed the petition that brought us to City Hall in 2013 to stop the developers plan (Al Banegas) from saturating a small hill with houses that sits bang at the center of the neighborhood in El Sereno that is surrounded by hills with homes. This, in an area where there were 3 lots and 3 houses. This developer has managed to merge the plots and produce a plan that would decimate the hill and surroundings for everybody living here. This is by no means an understatement. There was no transparency from the beginning to the end and the developer had resorted to unethical and aggressive behavior to push it through at that time.

The same plan is repackaged with a new wrapper every few months with minor changes, fronted by a new name and then represented as a new Plan again to the local planning committee in El Sereno. On Wednesday 20, 2019 a "Special Meeting" was held for the second time in one month at a Valley Towing Center. This time with new people and they had bussed special interest groups voice their opinions in their favor and approval from the committee (7 stakeholder/residents and 12 outsiders).

I request you too please investigate this issue and provide assurance that the city planning department is aware of this in its true nature (greed) and will not provide any rezoning permits, review the environmental report and understand that there is no mitigation for that level of destruction to this environment, or any other such exceptions that would allow them to pursue any development that requires rezoning or changing any part of the natural landscape that exists today.

All prior emails, documentation, environmental report, etc has been maintained online by our community group. We will escalate this issue, if not remediated immediately, city planning committee, the city attorney and district attorneys office, the mayor and the councilman's office and the then further up the judicial and legislative panels until it is resolved.

The developers being fronted over the years are:

- 2011-present Al Banegas-City Terrace LLC.
- 2011-2013 Bancomer Construction & Development
- 2017-Clearwater Communities
- 2019-Vista El Sereno Project-Rob Flitton

Respectfully,

Mrs. Tata
El Sereno resident

I would like OBJECT on behalf of myself
and my community to the proposed
General Plan Amendment, ZONE/Height District
change and the Adoption of the previously declared
Mitigated Negative Declaration so that is
recorded as a part of the official public record
at the Department of City Planning.
PLEASE RETAIN THE CURRENT ZONING AND
HEIGHT RESTRICTIONS

Thank You,
Carmen Toota



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Submission of signatures re VTT-73531-SL, APCE-2015-204-ZC-ZAD-ZAA and ENV-2015-1918-MND

Melissa Kellogg <emailmkellogg@gmail.com>

Wed, Jan 22, 2020 at 2:27 PM

To: May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

I'm sorry, May, I should have specified that in-person signatures were obtained in 2015 and 2016.

Thank you and please let me know if you have any other questions.

Melissa

[Quoted text hidden]



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Submission of signatures re VTT-73531-SL, APCE-2015-204-ZC-ZAD-ZAA and ENV-2015-1918-MND

2 messages

Melissa Kellogg <emailmkellogg@gmail.com>

Fri, Jan 17, 2020 at 10:19 AM

To: May Sirinopwongsagon <MAY.SIRINOPWONGSAGON@lacity.org>

RE: VTT-73531-SL, APCE-2015-204-ZC-ZAD-ZAA and ENV-2015-1918-MND

Hello May,

Please accept for your case file the attached petition submitted in opposition to VTT-73531-SL, APCE-2015-204-ZC-ZAD-ZAA and ENV-2015-1918-MND on behalf of 517 El Sereno stakeholders who signed to indicate their opposition to development of a master planned community on the parcels located at the corner of Eastern Avenue and Lombardy Boulevard (2520-2668 N Eastern Avenue and [2647 Lombardy Blvd.](#))

Thank you and please let me know if you have any questions.

Melissa Kellogg



Petition Eastern Lombardy.pdf

4150K

May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Tue, Jan 21, 2020 at 7:31 AM

To: Melissa Kellogg <emailmkellogg@gmail.com>

Thank you Melissa, I have included it into the case file.

Sincerely,

May Sirinopwongsagon
(213)978-1372
Department of City Planning
Central Project Planning Bureau
200 N. Spring Street, Room 621
Los Angeles, CA 90012

[Quoted text hidden]

La Petición

Nosotros, los abajo firmantes participantes en El Sereno llamamiento a la Comisión de Planificación de Los Angeles y el Consejo de la ciudad de Los Ángeles a:

1. Propuestas de para crear una comunidad planificada maestra (o desarrollo descomunal similar) sobre la propiedad en 2520-2668 North Eastern Avenue.
2. Trabaje en colaboración con las oficinas locales, del condado, estatales y federales, fideicomisos de tierras organizadas, organizaciones de conservación, los encargados de la subvención, los filántropos y la comunidad circundante para preservar esta parcela de tierra y dedicarla a uso público.
3. 3. Participar directamente con la comunidad para desarrollar un activo relevante y duradero para las generaciones presentes y futuras.

The Petition

We the undersigned stakeholders of El Sereno call upon the Los Angeles Planning Commission and the Los Angeles City Council to:

1. Bypass proposals to create a Master Planned Community (or similar outsized development) on the property at 2520-2668 North Eastern Avenue.
2. Work collaboratively with local, county, state and federal offices, organized land trusts, conservancy organizations, grant makers, philanthropists and the surrounding community to preserve this parcel of land and dedicate it to public use.
3. Engage directly with the community to develop a relevant and lasting asset for present and future generations.

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
1. RONALD L. TWEAGLE	2224 RICHELIEU AVE L.A. 90032	562-618-6516	SIKAJUNA1@yahoo.com
2. FLORA FLORES	2230 Richelieu Ave		
3. Veronica Castillo	2236 Richelieu Ave.		vcastillo1217@yahoo.com
4. Raul Castillo	2236 Richelieu Ave.		

Signatures

	Name (Required)	Address with ZIP (Required)	Phone	Email
	MARGARIT MORALES			
5.	Margaret Morales	2240 Richelieu Ave		
	Michael Garcia	4569 Zane Street (213)		michaelmichee/07@gmail.com.
6.	Michael Garcia	LA, 90032 458-3178		
	Andrea Garcia	4569 Zane St		alg807e@laUSD.net
7.	AM	LA, CA, 90032		
	Arianna Covarrubias	3323 Edloft ave		arianna.covarrubias@gmail.com
8.	Lupe Delgado	LA, CA, 90032		
	LUPE Delgado	4569 Zane St.		
9.	Lupe Delgado	Los Angeles, 90032		
	Lidia Garcia	3323 Edloft		
10.		LA, CA, 90032		
	Vincent Nieto	2037 Cyril Ave		tripeetri@yahoo.com
11.	Vincent David Nieto	Los Angeles, CA 90032		
	Memo Carreon	5018 Ladd Ave.		
12.		LA, CA 90032		
	Patricia Juarez	5018 Ladd Ave		PJ7788@laUSD.net
13.		LA, CA 90032		
	LEONARDO MONTOR	2763 BALLARD ST		
14.		LA 90032		
	James Martinez	2586 Lombardy BLVD. LA. CA. 90032	(323) 247-8412	clubjdm@yahoo.com
15.				

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

16. Andres Longoria Jr Lombardy 90032

17. Santiago Cuevas 2406 Rickelton Pl. LA CA 90038 (323) 907-4219
Santi2echo@Yahoo.com

18. Frank Salazar 4789 Academy St. La Ca 90032 (323) 907-4219

19. Maritza Armstrong 5151 Castleman Ave. LA CA 90032 (213) 446-7793

20. Edmond Barzcki 2755 Chadwick Circle 90032 441-2195
transdlight@earthlink.net (323)

21. GARY BERCIK 4737 NORRILL ST LA 323 559 1183

22. Joe Ramirez 2543 Walling ST 90032
call (606) 617-5726
wing

23. Rosalio Garcia 9323 CAYMAN AVE LA CA 91352

24. Elizabeth Cain 4447 Sierra St LA 90032

25. JAIME SANCHEZ
4441 Sierra St LA 90032

26. Margaret Fuentes 4410 E. Sierra St. L.A. CA. 90032

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

27. Thomas To. 4740 Klamath St. 90032

28. Vickie To 4740 Klamath Street 90032

29. Sean Lopez 4716 Klamath St. 90032

30. Fernando Rodriguez 2486 Richelieu Ave. 90032

31. Nelson Tobar - 2482 Richelieu Ave. 90032

32. RAYMOND O. CHIANDE 2422 RICHELIEU AVE L.A 90032

33. Rodolfo Lopez 2426 Richelieu Ave. 90032

34. 2326 Richelieu Ave 90032

35. 2320 Richelieu Ave 90032

36. Veronica Castillo 2236 Richelieu Ave 90032

37. 5137 Templeton St. Apt #4 90032

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

38. Catherin Reyes 2214 Richelieu Ave CATREYB5@gmail.com

39. Heber Martinez 2210 Richelieu hmartine-67@yahoo.com

40. NIKOLIS PEREZ 2211 RICHELIEU AVE

41. May Estrada 4622 Edelle Pl

42. Paul Johnson 4607 EDELLE PL 90032

43. Ron Lopez 4638 NORVILLE ST 90032
Cdm Trm

44. Art Tavera 4632 Norelle st 90032, arttavera@gmail.com
Linda Mendez

45. Linda Mendez 4632 Norelle st 90032

46. Alan Soday 4621 Edelle Pl 90032

47. Xie 4612 Norille St 90032

48. Car 11555 216th St. Lakewood, Ca 90715

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

49. Michael Diaz 3736 Harriman (Flup) 90032

50. JESSE ZEPEDA 12747 HERRICK AVE SALMAR 91342

51. CINDY KWAN 4831 K^LAMATH PL. 90032

52. CONNIE GOR 4837 KLAMATH PL 90032
(323) 225-48-43

53. Joanna Tinoco 4847 Klamath Pl. 90032

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La Petición

Nosotros, los abajo firmantes participantes en El Sereno llamamiento a la Comisión de Planificación de Los Angeles y el Consejo de la ciudad de Los Ángeles a:

1. Propuestas de para crear una comunidad planificada maestra (o desarrollo descomunal similar) sobre la propiedad en 2520-2668 North Eastern Avenue.
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The Petition

We the undersigned stakeholders of El Sereno call upon the Los Angeles Planning Commission and the Los Angeles City Council to:

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3. Engage directly with the community to develop a relevant and lasting asset for present and future generations.

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
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1. <i>Ramon Hernandez</i>	<i>4763 Templeton St LA CA 90032</i>		
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2. <i>Lu Sanchez</i>	<i>4627 Richelieu Ter LA 90032</i>		
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3. <i>Liliana Sanchez</i>	<i>2631 Lombardy Blvd Los Angeles CA 90032</i>		
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4. <i>Ariana Sandoval</i>	<i>4711 Butterfly Ln. LA CA 90032</i>		
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Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
5. Gracie S. Sanchez	5243 N. Huntington Dr. Apt 5 LA CA, 90032		
6. Marcos Perez	5243 N. Huntington Dr. Apt 5 LA CA, 90032		
7. Arv Sanchez	4637 Richlenten Ln 90032		DANNY DARKSIDE1@YAHOO.COM
8. DANIEL MATA	4915 LA CALANDRIA WAY LA, CA 90032		
9. NIKKI TAN	4915 LA CALANDRIA WAY, LA CA 90032		
10. SHAWN EHRHEART	4933 LACALANDRIA WAY LA 90032		
11. JONATHAN ROJAS	4933 LACALANDRIA WAY LA 90032		
12. DENISE ROJAS	4933 LACALANDRIA WAY LA 90032		
13. Jennifer Gutierrez	5161 Huntington Dr N 5 LA 90032		
14. Arturo Gutierrez	5161 Huntington Dr N 5 LA 90032		
15. Jeannette Cedano	4940 La Calandria Way LA CA 90032		

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

16. Lorge Cedano 4940 La Calandria Way LA CA 90032
17. ALBERTO ARREDONDO 4950 LA CALANDRIA WAY 90032
18. FABIENNE BONGARD 4962 LA CALANDRIA WAY 90032
fabienne @ oxygentango.com
19. CALEB NOSE 4962 LA CALANDRIA WAY LA CA 90032
Samuel Garcia casabianca = painting @ yahoo.com
20. [Signature] 4970 La Calandria Way L.A. CA 90032
21. Alabely Garcia 4970 La Calandria Way L.A. CA 90032
22. Haete Aguirre 4980 La Calandria Way LA, CA 90032
23. Archie McDonald DANIEL@VENSALES015 LA CALANDRIA WAY
.COM
24. DANIEL CEBALLOS LOS ANGELES 90032
25. Enrique Zamudio and family 2424 N Eastern Ave
LA CA 90032
26. GERARDO VASQUEZ 2428 1/2 N Eastern Ave LA 90032

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
27. Shanell Gonzales	2428 N Eastern Ave LA, CA 90032		
28. Kitty Gomez	2449 N EASTERN AVE LA, CA 90032		
29. Edward Gomez	2449 N EASTERN LA, CA 90032		
30. Rosalina Gonzalez	2434 N. Eastern Ave LA CA 90032		
31. Bertha Rivera	4720 Klamath Pl. LA CA 90032		
32. Guadalupe Rivera	4716 Klamath Pl. LA CA 90032		
33. Esther Rivera	4716 Klamath Pl. LA CA 90032		
34. Karla Monarrez	4727 Klamath Pl LA CA 90032		
35. Alma L. Quintero	4741 Klamath Pl, L.A, 90032		
36. Irma Robles	4741 1/2 Klamath Pl, L.A 90032.		
37. Natalie Robles	4741 Klamath Pl. LA, Ca. 90032		

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

38. Alfredo León 474 Klamath Pl. LA. 90032.

39. Rafael Roldes, Jr. 474 Klamath Pl. LA. 90032.

40. Jonathan Peña 474 Klamath Pl. LA 90032

41. Angie Cruz 4751 Klamath Pl LA 90032

42. Uma Gladys Martinez 2471 N Eastern

43. Jose 1910 N. Main Ave #107

44. 1910 N Main Ave 107

45. Melinda Rosas 3711 Collis Ave L.A. Ca 90032

46. Isabel Rodriguez 4836 Edison St. LA. CA 90032

47. Frank Torres 2006 N INDIANA AVE L.A. CA 90032

48. Jacqueline Aguirre 5037 Templeton St #9 LA 90032
aguirrejackie@yahoo.com

Signatures

	Name (Required)	Address with ZIP (Required)	Phone	Email
49.	MARIO GUTIERREZ	5137 Templeton St LA 90032	823 542-4447	
50.	Joe D. Perez	3770 Lowell Ave Los Angeles, Ca 90032	(323) 637-9157	
51.	Claudia Puga	3314 Thelma Ave Los Angeles CA 90032	(213) 519-7274 BigChula 33@yahoo.com	
52.	Gabriela Ramirez	3829 Portola Ave #1 LA CA 90032	(323) 237-5367 gramirez_85@icloud.com	
53.	Rosario Gusman.	3314 Thelma Los Angeles CA 90032		
54.	Alc Kinkory	2220 N. Eastern Ave L.A Ca 90033	323-223-6126	
55.	Laura Lopez	2435 N Dittman Ave	stella11572@gmail	
56.	Isela Silva	5318 Ithaca Ave Los Angeles CA 90032	(323)	
57.	Samantha Bernal	4640 Catalpa St 9158231	(323)	
58.	Consuelo Guerrero	4614 Catalpa St	830-4444	
59.	Hugo Camacho	333 S. Av. 2. LA CA		

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
60. Arthur Fierro	1319 N. Ave 56 La, Ca, 90042	(323) 849 9340	
61. Fernando Hernandez	4465 GATE 316	323 222 3371	bettianne76@aatt.net
62. Bettina Molano	4229 Abner St #3 LA CA 90032		
63. MARY JAQUEZ	5145 DARTMOUTH AVE L.A. 90032	JAQUEZMSSPARKLE@AOL.COM 323) 222-2948 323) 376-1461	(323) 629-2186
64. Irma Elizondo	2445 Sasper st LA 90032		
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The Petition

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Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
1. <i>[Signature]</i>	2507 Richelieu Ave	626-840-1480	
2. <i>José Jacanyillo</i>	5507 Norwood Ave	(323) 222-4623	
3. <i>ARTHUR SANCHEZ</i>	4627 Richelieu TERN	323-2223826	
4. <i>Ruganda Simba</i>	4627 Richelieu Ave	573.2222826	
5. <i>HIROSHI ISAGO</i>	2485 N. PITMAN AV.	323-221-9640	
6. <i>Isabel Contreras</i>	4632 Richelieu Pl	323222-8471	
7. <i>Magdalena Jauris</i>	4616 Richelieu	323-221472	
8. <i>Humberto Rodriguez</i>	4606 Richelieu	323-479-0034	

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

Not
↓

9. WILBER SEVILLA 4601 RICHELIEU PLACE (323) 445-1542 MIKRIH20@YAHOO
APT 1810

10. Genoveva Recio 3711 Baldwin St 90031 213-503-1973

JR.

11. RAUL RECIO 3711 BALDWIN ST 90031 213-503-1973

12. Raul Recio Sr. 3711 Baldwin St 90031

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La Petición

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Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

1. Rosa Hernandez 2371 Belleglade NVA 90032 (323) 900-32 (223) 9-86
2. Adela Castellanos 5109 Dartmouth Ave. L.A. Ca. 323-224-8759
3. Hilario Anguiano 5109 Dartmouth Ave Los Angeles Ca. 323-718-4958
4. Merzli Anguiano 5109 Dartmouth Ave LA, CA (323) 224-8759

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
5. Kelly Narvaez	5138 Dartmouth Ave 90032	(323) 681-3120	Kellynarvaez@yahoo.com
6. Sergio Balandra	5157 Dartmouth av LA 90032	323 2238172	
7. Isabel Balandra	5157 Dartmouth Ave	(323) 223-8172	
8. Miguel Lora	5171 Dartmouth ave.	358. 1077 323. 767	
9. Eddie Duran	4495 Bateside Dr	323-225-2724	
10. Yolanda Duran	4495 Bateside Dr	323-225-2728	
11. Amanda McNeil	4560 Catalpa st	323 2223122	
12. Elvira Salcido	3 Tova Oregon		
13. Baudelia Gallegos	E - R - U - N		
14. Rosendo Caballero	Nieve F - l - o - r - e		
15. Maria Astorga	5074 Stacia Ave	(323) 225-2365	

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

- 2433 N. Eastern
16. Patricia Sotoz Ave #409 La Ca 90032
- 5501 Templeton St
17. Louraine Rys L.A. Calif 90032
18. Josephine Orona 4490 Gateside Dr L.A. Ca 90032
19. Esperanza Montenegro 2024 Vineburn Ave L.A. Cal 90032
20. Placido Montenegro 2024 Vineburn Ave L.A. Cal 90032
21. ~~Chia Garcia~~ 3317 Edw Ave LA 90032
22. Justin Polanco 538 Dartmouth Ave, LA CA 90032 323 470-1050
justin-polanco@yahoo.com
23. Lebra Gomez 4369 Sardonxx 323 822-4481
24. Debra Lenda 4509 Mercang Ave 323) 2254203
25. Van M. Ayeu 5475 Templeton Ave 323-333-6465
LA 90035
26. Norma Gonzale 4161 N Portola Ave 90032 323 580 B59

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
27. Charlie Melgoza	3320 N. Eastern Ave LA, CA, 90032	323 382-2235	Tamaleman90032@gmail.com
28. David Gomez	4925 Axtell St. A LA, CA 90032		davidgomez0581@gmail.com
29. Anna Gonzalez	4925 Axtell St Apt D		
30. Raymond Gomez	4925 Axtell St Apt D		
31.			
32.			
33.			
34.			
35.			
36.			
37.			

La Petición

Nosotros, los abajo firmantes participantes en El Sereno llamamiento a la Comisión de Planificación de Los Angeles y el Consejo de la ciudad de Los Ángeles a:

1. Propuestas de para crear una comunidad planificada maestra (o desarrollo descomunal similar) sobre la propiedad en 2520-2668 North Eastern Avenue and 2647 Lombardy.
2. Trabaje en colaboración con las oficinas locales, del condado, estatales y federales, fideicomisos de tierras organizadas, organizaciones de conservación, los encargados de la subvencion, los filántropos y la comunidad circundante para preservar esta parcela de tierra y dedicarla a uso público.
3. 3. Participar directamente con la comunidad para desarrollar un activo relevante y duradero para las generaciones presentes y futuras.

The Petition

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3. Engage directly with the community to develop a relevant and lasting asset for present and future generations.

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
-----------------	-----------------------------	-------	-------

1. Maria Lopez	2520-2668 North Eastern Avenue and 2647 Lombardy	323 722 0430	
----------------	--	--------------	--

2. 2476 Richelieu AV.	LA. CA. 90032		
-----------------------	---------------	--	--

3. Delia Cordero	323-222-8821	2417 Richelieu ave LA. 90032	
------------------	--------------	---------------------------------	--

4. Humberto Cordero	323-221-7716	4856 Edison ST LA, CA. 90032	
---------------------	--------------	---------------------------------	--

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
5. MANUEL CUEVAS	2070 PRIMROSE AVE	90032	
6. JOSE CUEVAS	2003 ROSADEL AVE	90032	JRIVERA2404@YAHOO.COM
7. JUAN A. RIVERA	2404 RICHELIEU AV.	90032	CSISNEROS3@YAHOO.COM
8. George Sisneros III	2310 Richelieu Ave	90032	
9. Lorraine Sisneros	2310 Richelieu Ave	90032	lorrainehrn@yahoo.com
10. Bill Saxelby	2306 Richelieu Ave L.A.	90032	
11. RAUL CASTILLO	2236 RICHELIEU AVE	LOS ANGELES, CA	90032
12. Flora Flores	2230 Richelieu Av		
* 13. Claudia Martinez	2210 Richelieu Ave. L.A	CA 90032	Carmachomartinez1@yahoo.com
14. Patrick Yang	2237 Richelieu Ave L.A,	CA 90032	
15. Jimmy Mirero D	4614 Edella A	LA CA	90032

Signatures

	Name (Required)	Address with ZIP (Required)	Phone	Email
16.	Joshua Valle	2210 Orange Grove Ave 91803		JB91SV@aol.com
17.	TANIA HOLGOW	4611 Edelle Place		tholgow274@yahoo
	Narduna Solano			
18.	JOE SOLANO	4647 Edelle Place 90032		
19.	Margie Morales	2240 Edelle Pl. 90032		
20.	JOE SOLANO	4647 Edelle Pl 90032		
21.	Stephanie Lew	4627 Norelle st. LA, CA 90032.		
22.	Evelyn Gould	3127 1/2 Glendale Blvd #D, LA CA 90039		
				MOYVALENTE@MAIL.COM
23.	MOISES VALENTIN	2527 MALLORY ST. L.A. CA 90032		
24.	Mariana Valente	2527 Mallory St. L.A. CA 90032		
25.	Moises A. Valentin	2527 Mallory St., Los Angeles CA 90032		
26.	Mundo Ramos	2523 Mallory St. LA 90032		

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

27. Elisa Manly 2533 Mallory St (323) 707-2069

28. Rebecca Sim 2705 Ballard St. (323) 828-0871

29. Justin 2723 Ballard St (323) 855-2449

30. Paula M 2731 Ballard St (323) 224-9762

31. Jeremy Lewis 2724 Ballard Street, LA 562-204-3898

32. H. BRADFORD DAVIES 3847 HELLMAN AVE 90032 (323) 363 3331 BRAD.DAVIES@EARTHLINK.NET

33. ANSLEY DAVIES 2753 BALLARD ST 626 590-4159 ansleyblew@yahoo.com
LA CA 90032

34. Victor Gonzalez 2807 Budan Avenue 1420 (800) 456

35. Gabriella Marquez 2807 Budan Avenue 1420 (800) 456 gabriellamarquez90@yahoo.com
LA CA 90032
4819 GAMBIER ST

36. DANIEL SANCHEZ 101 CA 90032

37. Joseph P. Servato 2827 CHADWICK CIR.
L.A. CA. 90032

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

38. Sandra Mauro 2827 Chadwick Cir 245-B117 323-
CHRIS323LA@GMAIL.COM

39.  CHRIS OCHOA 2834 CHADWICK CIRCLE

40. Lurdes Henrich 2841 CHADWICK CIR

41. Karla NUNEZ 2913 Ballard Ct. KMN90032

42. Angel Seriano 2909 Ballard Ct 90032

43.

44.

45.

46.

47.

48.

La Petición

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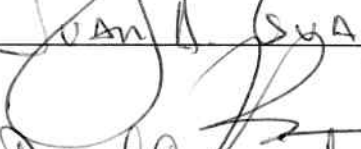
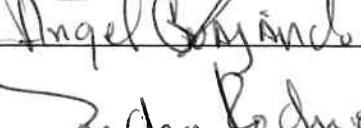
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Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
1.  TOM OKAMOTO	2416 RICHÉLIEU ^{LA 90032}	323-225-0250	
2.  Anthony Tran	2437 Richelieu Ave. 90032	323-219-6905	
3.  Juan A. Sosa	2412 Richelieu A ⁹⁰⁰²⁻	(323) 574-8139	
4.  Angel Sosa	2412 Richelieu A ⁹⁰⁰³⁻		
5.  Sandra Rodner	2412 Richelieu A 90032	323 422-8172	

La Petición

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Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

1. TERESITA YU 4220 ARNER ST, L.A., CA 32 (323) 2271772

2. ARMINDA CHAVEZ - 2433 N. Eastern av. Los Angeles CA 90032 (323) 2298631

3. Teresa Cardenas 4458 Gaterado Dr 90032³²³ 2233943

4. Patricia [Signature] 2448 N Eastern Dr. 323 2255604

Signatures

- | Name (Required) | Address with ZIP (Required) | Phone | Email |
|----------------------|-------------------------------|-------------------|-------|
| 5. Lucia Morales | 3161 Blanchard ST LA CA 90063 | Tel. 323-264-7854 | |
| 6. Ros Rosas | 4754 Divid ST LA CA 90032 | 323 224 1201 | |
| 7. Micaela Pinon | 3726 Mc Kenzie Ave. | (323) 223 7716 | |
| 8. Socorro Hernandez | 5022 Lynnfield St | 323 222 4759 | |
| 9. Socorro Hernandez | 5022 Lynnfield St. | 323 222 4759 | |
| 10. Oscar Vasquez | 5239 Edna St LA 90032 | (323) 223 1614 | |
| 11. Malucia Aguilar | 5239 Edna St LA 90032 | (323) 245 416 | |
| 12. Blanca Vega | 3142 Vaguerro Ave, LA 90032 | (323) 227 4760 | |
| 13. JOSE VIZCARRA | 3142 VAGUERO AV LA 90032 | (323) 227-4760 | |
| 14. Annie Malin | 2514 Vaguerro Ave | 221-0921 | |
| 15. Clara S Barner | 323-222 8892 4508 JADE ST | | |

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
16. Ramon Martinez Jr	P.O. box 32196 LA CA 90032	(323) 378-4420	Raymtz1961@gmail.com
17. Ramon Martinez Sr	4722 Klamath St Los Angeles CA 90032	(323) 222-9254	
18. Maria Preciado	4706 Klamath St Los Angeles Ca 90032	(323) 223-9318	
19. Ramona Beas	11	11	
20. Maria S Beas	2576 Richelieu Ave	11	
21. Tang chu	4651 Richelieu Terrace LA 90032	(213) 361-3532	
22. Larry Anaya	4651 Richelieu Terrace LA 90032	(323) 810-9116	larryanaya74@gmail.com
23. Laura Gutierrez	4651 Richelieu Terrace LA 90032		lgutierrez@sbcglobal.net
24. Adriana Maldonado	4640 Richelieu Terr. LA 90032		a.maldonado17@gmail.com
25. BALBINA FISHER	4421 Portola Ave	323-225-5301	
26. Julia Fowler	4636 Richelieu terrace		

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

27. Mary Mianh 4626 Richelieu Terr

28. Lee Kim Chong 4616 Richelieu Terr

grunt11600@hotmail.com

29. Alcaraz, Gil 4611 Richelieu Terrace (310) 703-9645

diana1@ucla.edu

30. Diana Alcaraz 4611 Richelieu Terr (310) 462-9631

31. Brian Escalante 4608 Richelieu Terr (323) 453-3059

32. Helen Lee 4631 Klamath St 323 221 0440*

TDONG@ATTN.COM

33. TED DONG 4625 KLAMATH ST. 323-343-0523

34. LINDA SANCHEZ 2483 RICHELIEU AVE 213-361-9963

35. BERNARDA SANCHEZ 2483 Richelieu Ave (213)-361-4442
(323)

36. Dolores Valdivia 2437 Richelieu 327-4080

37. Rachel & Luis Valdivia
valdivia dec@joh.com

Name (Required)

Phone

Email

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Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
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1. Demetria Contreras	5015 Ladd Ave	323-222-6467	Demetriaco@aol.com
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2. Mariade Jesus Contreras	2927 Okell Dr. LA 90032	323-227-8577	-
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3. Veronica Contreras	2927 Okell Dr.	323-227-8577	
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4. Jose Contreras	2927 Okell Dr.	323-227-8577	
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Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
5. Maria A. Escobar	4640 Bowman Blvd. (323)	629-5340	
6. Andrea Escobar	4640 Bowman Blvd. (323)	318-4065	
7. Trinidad Escobar	4640 Bowman Blvd. (323)	919-4857	
8. Terri Campos	2515 Mallory	323-384-5902	
9. Manny Campos	2515 Mallory		
10. Cory Campos	2515 Mallory		
11. Bianca Campos	2515 Mallory		
12. Rene Ramirez	2515 Mallory		
13. Steven Campos	2515 Mallory		
14. Vanessa Campos	2515 Mallory		
15. Cindy Roca	2519 Mallory St. 90032	(323) 221-3067	

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
16. Roxxanne Urquiza	2504 Mallory Street LA CA 90032	323 225-5568	roxxu-starfish@yahoo.com
17. Greg Macias	4911 Ladd Ave	323/972-1162	Koolgreg-2000@yahoo.com
18. [Signature]	4906 LADD AVE (626) 376-8567	323/221-3270	
19. DULCE BALAM	2523 MALLORY ST		
20. HILBERTO BALAM	2523 MALLORY (626) 376-8567		ST.
21. GUSTAVO CORREA	2523 MALLORY (626) 376-8567		ST.
22. Gladys Correa	2523 Mallory		ST. GUSTACORREA@ATT.NET
23. Virginia Hernandez	2537 Mallory St L.A.		
24. [Signature]	2537 Mallory St LA		
25. [Signature]	2553 MALLORY ST.		
26. Wendy [Signature]	2561 Mallory St.		

Signatures

	Name (Required)	Address with ZIP (Required)	Phone	Email
27.	Noé Murillo	2715 Budau Ave	(323) 896-8363	NoeMurillo26@hotmail.com
28.	Andrea Elizondo	2759 Budau Ave	626 247-0583	elizondoandrea@yahoo.com
29.	Nancy Gonzalez Marquez	2766 Budau Ave	(213) 220 7493	
30.	Carlos Beltran	2766 Budau Ave	(213) 220 7493	
31.	Reyna Insland	2835 Ballard Street Avenue	LA CA 90632	
32.	Heri Leena	2835 Ballard Street	LA CA 90032	
33.	CARLOS GONZALEZ JR	2758 Budau Ave	90032 (323) 490-2894	SKELETO-INK@yahoo.com
34.	CARLOS GONZALEZ	2758 Budau Ave	90032 (323) 221-6827	deadClownChuck@yahoo.com
35.	Rodriguez Obie	2754 Budau Ave	90032 (323) 915-1402	JaredRodriguez20813@yahoo.com
36.	Oscar Villanueva	2750 Budau Ave	90032	strictly213@yahoo.com
37.	Rene Camacho	2734 Budau Ave	LA 90032	rcamacho1974@gmail.com

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

323-743-3813

38. Season Rivas 2714 Budau Ave, 90032 Season4039@yahoo.com

39. Sherry Tuller 2710 Budau Ave Los Angeles

40. Liz Gonzalez 4223 Garden Home Ave LA 90032

41. Lisa Wells 4396 Gettysburg Ave 90032

42. Victor LLamas 5582 W Hambrn Ave 90032

43. Jose Rios 2518 N. Eastern Ave 90032

44. Jose M. Zendeja 2304 N Eastern Ave 90032

45. Luis Cardona 4831 Klamath St. L.A. 90032
Cardonas3502@yahoo.com

46. Jean Takamiya 4837 Klamath St LA 90032

47. Patricia M. Juarez 4843 Klamath 90032
(213) 304-2984

48. Jeffery Steph 4843 Klamath St. LA 90032
@jefeqvo@yahoo.com

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

49. Jeimi Tejada 4853 Klamath St karinatejada91@yahoo.com

50. Gustavo Tejada 4853 Klamath St tejada2008@yahoo.com

51. Nicolás Quintanilla 4847 Klamath St nic760313@gmail.com

52. Ervin Bac 2448 Klamath St

53. Liliana Roque 2448 Klamath St

54. Byron Eduha 41373 Huntington DR

55. Edgar Barrionto 2448 Klamath St

56. CHUN WING LEE 2579 LOMBARDY BLD.

57. Joseph Cruz 2567 Lombardy Blvd

58. Rosa Ledesma 2555 Lombardy Bl

59. Rogelio Ledesma 2555 Lombardy Bl

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

60. Pomela Keshishyan 2541 Lombardy Blvd Los Angeles 818-859-2107 kathymin1015@gmail
61. Celaya Hector 2525 Lombardy Blvd Los Angeles
62. Luiz Inez Celaya 2525 Lombardy Blvd L.A.
63. David & Molly Orozco 2533 Lombardy Blvd. 90032
64. Jesus & Norcepor 2546 Lombardy 90032
65. Elmin Ramirez 2518 Lombardy BL. 90032
66. Jesus Gutierrez 2526 Lombardy Blvd. 90032
67. Austin Miller 2505 Lombardy Blvd 90032
68. Dani Bradley 5167 FRANK Ave. 90032
69. Heidi Shyu 5167 Frank Ave 90032
70. Jamie L. Harrison


Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

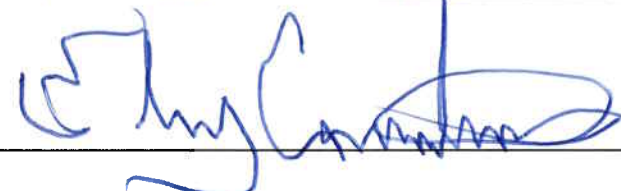
71.  Mckayla@Sarah@gmail.com

72. EMUANEL ARGUETA 5048 LA CALANDRIA DR. EARGUETA11@GMAIL.COM

73. Maria Gómez 5014 LA Calandria Dr

74. Luis Gomez 5014 La Calandria Dr. - Pz. pkp2339@lausd.net 323793-7395

75. Pamela Perkins 5101 La Calandria Way, LA,

76.  4951 La Calandria Drive 90032

77. Jose Uribe 4922 LA CALANDRIA DR 90032

78. HAONG THI G 5041 LA CALANDRIA DR LA CA 90032

79. Alex Mercado 2536 Lombardy Blvd (213) 434-6730 LXM_73@hotmail.com

80. Veronica Mercado 2536 Lombardy Blvd (213) 434-6729

81. Sergio Perez 2712 N. Eastern Ave. B13-559-4611 323-559-4611

Signatures

	Name (Required)	Address with ZIP (Required)	Phone	Email
82.	Cesar	3330 Fithian Av	323 915/64/03	
83.	Alex CORVERA	4632 Richelieu Pl	323/222-8470	
84.	Patty Saavedra	5071 Bohlig Rd 90032	(323)376-2780	
85.	Luis Navarro	3507 Farnsworth	(323) 742-6045	
86.	Guadalupe Rangel	3507 Farnsworth Av.	(323)742-6203	
87.	Isabel Hernandez	5233 1/2 Alhambra Ave	(323) 2250043	
88.	Rafael Peña	5233 1/2 Alhambra Ave	(323) 225-0043	
89.	Anita Hernandez	5233 1/2 Alhambra Ave	(323) 2250043	
90.	Clementina Garnica	2459 N Eastern Ave	(323) 223-0983	
91.	Javier Garnica	2459 N Eastern Ave	(323) 4-93-4581	
92.	Marisela Contreras	4767 Gambier St	323.412-3406	

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

93. Hilarro Contreras 4767 Gambier St 323 379-0156

94. Leticia Salazar 2425 Endicott St. 90032

95. Martha Jimenez 2425 Endicott St 90032

96. ~~Martha Jimenez~~ 2425 Endicott St 90032

97. RICHARD SALAZAR 2425 ENDICOTT 90032

98. Loretta Dena 4925 Lynnfield St. 323 274 6778 Tutti head 1979 @aol.com

99. Sylvia G. Jimenez 3320 1/2 Linda Vista Ter, Los Angeles CA 90032

100. ~~Emilia Vasquez~~ 2372 Eastern Av Los Angeles CA 90032

101. Emilia Vasquez 2372 Eastern Av. Los Angeles CA 90032

102. Regina Bass 3441 City Terrace #3 Los Angeles 90032

103. Brian Montano 3525 W. Via Corona St. 90640 Montebello CA 90640

- | NAME | ADDRESS | PHONE | EMAIL |
|------------------------|-----------------------------------|--------------------|---------------------------|
| 104. Tania Amarillas | 4679 Grey Dr Apt. 2 90032 | (213) 400-3521 | taniamarilla@gmail.com |
| 105. Olivia Gonzalez | 3308 Elloft Ave LA. CA | 323 (323) 276-7851 | |
| 106. PEDRO CORDA | 3917 COLLIS AVE | 323 225-4678 | |
| KRISTINE + | | | |
| 107. KSN CARBAJAL | 4612 NOELLE ST. | 323/276-0213 | KCSTRAWBERY@EARTHINK.NET. |
| 108. Mayra Gonzalez | 4369 1/2 Esmeralda st 90032 | (323) 276-8171 | |
| 109. Santos Ruiz | 4964 Humbler St | 323) 225-2585 | |
| 110. Rachel Valdivia | 2437 Richelieu Ave | 323 287 4080 | |
| 111. Luisa Espino | 2920 1/2 N DAFERN AVE | | |
| 112. Patricia Gomez | 4929 Lynnfield St #3 LA. CA 90032 | | |
| 113. Robert Sanchez | " " " | | |
| 114. Susana Sanchez | " " " | #151 A. CA 90032 | |
| 115. Leonardo Santiago | 4344 Winchester AV. #40 | (323) 315-8162 | |
| Iron works | | | |

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Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
1. Erin Herbe	15914 Mirinda Ct Whittier CA 90603	310-902-5929	erinpatria.hughes@yahoo.com
2. Jenny Recio	2222 Parkside Ave 90031	323-222-7996	recio@cei.net
3. Raul Recio Jr	"	"	_____
4. Jessica Leggett	2171 W. 240th St #103 Lomita, CA 90717	310-200-9961	lioness727@aol.com

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
5. Juan Manuel Contreras	4520 (90032) Lowell Ave	323-945-7782	
6. Mario A Northe	2929 Okell Dr.	90032	
7. Christopher Northe	2929 Okell Dr.	90032	
8. Ashantas Martin	421 N. Curtis Ave	91801	
9. Cassandra Correa	4648 Bowman Bl	90032	
10. Robert Correa	4648 Bowman Bl	90032	
11. Adrian Gonzalez	2037 Haven St	90032	
12. Raul Recio Sr.	2222 Parkside Ave	90031	
13. Robert Candelario	4609 Zane St. L.A.	90032	
14. RAINBEAU THOMP	5010 LADD AVE.	90032	
15. Dolores Garcia	2516 Mallery St. L.A.	90032	

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

16. Angelica Cervantes Los Angeles 90032

17. Jose Perez " "

18. Elizabeth Morales 4911 Templeton St 323-223-0740

19. Victor Perez 2924 Chadwick 323-223-1577
90032

20. RUDY HURTADA 3234 CASTALIA AVE 90032

21. Jacob Rios 5151 CASTLEMAN AVE 90032

22. Eva M Reyes 2226 Eastern Ave 90032

23. ALMA LUCERO 2927 O'Kell DR 90032 323-227-8577

24. Ricardo Chavez 5022 LADD AVE 323-448-8406

25. Maria "Lucy" Chavez 5022 Ladd Ave. 323-493-2618

26. Jose Chavez 5022 Ladd Ave. 213-507-4579

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
27. <u>Jackeline Macias</u>	<u>4911 Ladd Ave.</u>	<u>(323) 385-2434</u>	<u>jackie-macias54@yahoo.com</u>
28. <u>Ashley Armijo-Moura</u>	<u>5014 Ladd Ave. LA, CA 90032</u>	<u>(818) 554-2668</u>	<u>aarmijo@hotmail.com</u>
29. <u>Erica Armijo-Moura</u>	<u>5014 Ladd Ave. LA, CA 90032</u>	<u>(909) 841-3958</u>	<u>foxdemoura@aol.com</u>
30. <u>Linda Macias</u>	<u>4911 Ladd Ave. L.A, CA 90032</u>	<u>(323) 221-9824</u>	<u>girlfriend233@yahoo.com</u>
31. _____	_____	_____	_____
32. _____	_____	_____	_____
33. _____	_____	_____	_____
34. _____	_____	_____	_____
35. _____	_____	_____	_____
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37. _____	_____	_____	_____

La Petición

Nosotros, los abajo firmantes participantes en El Sereno llamamiento a la Comisión de Planificación de Los Angeles y el Consejo de la ciudad de Los Ángeles a:

1. Propuestas de para crear una comunidad planificada maestra (o desarrollo descomunal similar) sobre la propiedad en 2520-2668 North Eastern Avenue.
2. Trabaje en colaboración con las oficinas locales, del condado, estatales y federales, fideicomisos de tierras organizadas, organizaciones de conservación, los encargados de la subvención, los filántropos y la comunidad circundante para preservar esta parcela de tierra y dedicarla a uso público.
3. 3. Participar directamente con la comunidad para desarrollar un activo relevante y duradero para las generaciones presentes y futuras.

The Petition

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Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
1. Astrid Hernandez	2502 N. eastern ave 90032	(323) 7413 0088	
2. Josefina Parra	2506 N Eastern Ave. 90032	323) 343-9258	
3. Andrés Juárez	4849 Klamath St.	323-221-0132	
4. Deborah Vargas	4849 Klamath St.	213-210-6375	deborahvargas@yahoo.com

Signatures

- | Name (Required) | Address with ZIP (Required) | Phone | Email |
|-----------------------|--|----------------|------------------------------|
| 5. Laura Parra | 2506 N. Eastern Ave. LA CA 90032 | 323-216-8124 | pawag0485a@att.net |
| 6. Jose Arrizon | 4827 Klamath Pl LA CA 90032 | (323) 226 9071 | |
| 7. Jose' Grubel | 4827 Klamath Pl. LA CA 90032 | | solano solano 60 @ gmail.com |
| 8. Jesse Solano | 5204 Calandria RD. LA CA | | magarcia92@yahoo.com. |
| 9. Martha Garcia | 2511 Lombardy Blvd | (323) 788-1240 | |
| 10. Ignacio Flores | 2511 Lombardy bl. | (323) 788-1179 | |
| 11. Sandro Salcedo | 5103 La Calandria Dr. L.A. CA 90032 | (323) 595-7656 | |
| 12. David Lopez | 5103 La Calandria Dr. L.A. CA 90032 | | Same # |
| 13. Martin Tirtasana | 5002 La Calandria Los Angeles, CA 90032 | | gr8martin@gmail.com |
| 14. Thomas Marti | 4936 La Calandria dr L-A 90032 | | |
| 15. Bryan W. Richards | 4913 La Calandria Dr. Los Angeles, CA. 90032 | | |

Signatures

- | | Name (Required) | Address with ZIP (Required) | Phone | Email |
|-----|------------------|----------------------------------|----------------|-----------------------|
| 16. | Lidia CAVALES | 4908 1/2 LA CALANDRIA WAY | | |
| 17. | Rose Lugo | L.A. 90032 4908 La Calandria Way | | |
| 18. | Karina Cruz | 5110 La Calandria Dr LA 90032 | 323) 223 2806 | CRUZKARINA620@AOL.CO. |
| 19. | Ruby Marek | 4926 La Calandria 90032 | (562) 866-8404 | rubymarek@gmail.com |
| 20. | Steve Marek | 4926 La Calandria Way 90032 | 362 866 8404 | Stevmarek@gmail.com |
| 21. | DON JOHNSON | 4939 LA CALANDRIA WAY 90032 | 310 499-6331 | |
| 22. | DOROTHY JOHNSON | 4939 LA CALANDRIA WAY 90032 | 310-499-6331 | |
| 23. | Christine Acosta | 4947 La Calandria Wy 90032 | 323-224-8357 | |
| 24. | Arnold V. Acosta | 4947 La Calandria Wy 90032 | (323) 919-6631 | |
| 25. | Susan Luna | 4955 La Calandria Wy 90032 | (323) 929-0436 | |
| 26. | Mike Luna | 4955 La Calandria Wy 90032 | | |

Signatures

Name (Required)

Address with ZIP (Required)

Phone

Email

27. MANUEL RIOS 4959 LA CALANDRIA WAY (213) 223-7396

28. Robert Hernandez 4977 LA CALANDRIA WAY (323) 547-5748

29. William Hernandez 4977 LA CALANDRIA WAY (323) 547-5708

30. Eladio Chavez 4983 La Calandria Way 323-399-0228

31. Eli S 4983 La Calandria Way 323-795-3866

32. ERIC GREGORY 5030 La Calandria Way 626-429-3103

33. Melin Kelly 5015 Ladd Ave. 90032 323-222-6467

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La Petición

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3. Engage directly with the community to develop a relevant and lasting asset for present and future generations.

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
1. Rosy De Sancto	522 Alhambra CA, CA 90025	424-543-9787	RdeSancto@nextdoor.com
2. Juan Espinoza	West Lind Vista 90025		espinozanone@notmail
3. Xanni Valentín Chavira	5055 Williams Pl. Los Angeles CA 90015	(213) 924-8577	Valentin chavira3@gmail.com
4. DANA KOTUSKI	446 N PASEO DE LA PAZ SUR - MONTRO PASADENA 91101		DANA@DANAKOTUSKI.COM

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
5. Jason Curtis	5306 Alhambra Ave Los Angeles, CA 90032	585 727 3752	epostmark@gmail.com
6. Fernando Galvez	2341 Coral St. Los Angeles, CA 90031		galvez-photo@yahoo.com
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Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
1. CHRISTOPHER DOLAN	2500 N. EASTERN AVE.		marthaelisa38@yahoo.com
2. GARCIA, MARTHA E.	4824 Klamath St. L.A CA 90032		floreserc@gmail.com 323-497-3385
3. Christwa R. Flores	4852 Klamath St. L.A. 90032		
4. Drew Barnett-Hamilton	4844 Klamath St. (512) 810-9844 LA, CA 90032		drewbh@mac.com
5. Brandy Diaz	2474 1/2 N. Eastern Ave Los Angeles CA 90032		
6. Peggy Diaz	2474 1/2 N. Eastern Ave Los Angeles CA 90032		
7. Elton Tinoco	48247 Klamath Pl. 323 559 6221		
8. Larry Wong	4854 Klamath Pl. 323 691-8294		

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
9. Stella S. Garcia		1 323 222-2374	None
10. Gilbert G. Garcia		1 323 868-2388	None
11. Donna Valentino		818 530-6131	dvalentino@antioch.edu
12. Belen Camacho	Lombardy	626 234-7625	baalmose@yahoo.com
13. Veronica Calderon			ronii99@yahoo.com
14. Benito Camacho	5118 LA Caceranga 90038		
15. Jose L. Ontiveros	5051 La Calandria Dr. LA CA 90032		
16. Oscar Martinez	5051 La Calandria Dr. LA CA 90032		
17. Anel Martiny	5051 La Calandria Dr. LA CA 90032		
18. Maria Ontiveros	5051 La Calandria Dr. LA CA 90032		
19. Teresa M. Llamas	5008 la Calandria Dr. LA 90032 CA.		

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
20. David Botello	4952 La Calandria Dr. LA 90032	(323) 225-7595	artedavid@sbcglobal.net
21. Lupo Botello	4952 La Calandria Dr. LA 90032	323-225-7595	
22. Juan P Arcoraz	4614 Zane st Los Angeles LA 90032		
23. Maria D. Moreno	4812 Converse ST. LA 90032		
24. ALFREDO VELEZ	2736 N. EASTERN AVE.		
25. Jose S Chavez	5022 Ladd Ave LA 90032		
26. Cindy Renteria	5022 Ladd Ave LA 90032		labeautyxx@901.com
27. Isabel Corvera	4632 Richfield Pl L.A 90032		
28. Amorela Ontiveros	4916 Ithaca LA 90032		
29. Salvador Velasco	2716 Chadwick Circle 90032	323-343-1622	lasocsalev@yahoo.com
30. Soraya Merlo	2761 Chadwick Circle 90032		Sorayamerlo@yahoo.com

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
31. Frances Cortez	5216 Edna St ²¹³	494 5758	creepymouse@att.net
32. Rick Butz	5216 Edna St ²¹³	494 5758	//
33. Robin Estrada	5216 Edna St ²¹³	494-5758	//
34. Terry Hernandez	2820 Phelps Ave ⁽³²³⁾	707-4074	teresanaranjo@roadrunner.com
35. Isaac Hernandez	2820 Phelps Ave	—	—
36. EMILY SANCHEZ	5356 Templeton St		exs4048@lausd.net
37. Angelen Sanchez	5356 Templeton St		jan.nov86@yahoo.com
38. Emilia Sanchez	5356 Templeton St		mellita57@yahoo.com
39. Leonardo Rodriguez	4866 Lynnfield St.		
40. Juan Rodriguez	4866 Lynnfield St.		
41. Richard Rung	5112 BORLAND RD.		
42. Dena Norrera	3406 Farnsworth Ave S.E.		

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
43. Duke	2904 N. Eastern Ave	(323) 276-6706	
44. Abraham Romero	2904 N. Eastern Ave.		
45. John Wille	2910 N. Eastern Ave		
46. Della Ramos	2910 N. Eastern Ave	(323) 222-0190	
47. Tony Linares	4856 Gambier ST Apt #2	(323) 798-6483	
48. Carolina Cruz	4856 Gambier ST Apt #2	(323) 798-6683	
49. Jeff Purdy	4873 Templeton	(323) 361-5003	
50. Frank Saliz	4714 Stanton	(323) 227-9813	
51. Daisy Ibarra	2812 N. Eastern AVE		
52. Felipe Ibarra	2812 N. Eastern AVE	LA 90032	
53. Edward Sando	4803 Catalpa St	LA CA 90032	
54.			

Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
79. MOISES RAMOS	2407 RICHELIEU AVE 90032	(323) 226-0129	SOLDOE885@aol.com
80.			
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The Petition

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Signatures

Name (Required)	Address with ZIP (Required)	Phone	Email
1. LUIS SANITIZ	4900 LA CALANDRIA WAY	323 272-7184	luisysanchez402@gmail.com
2. George Cabrera	1912 Boca Ave, L.A. CA (323)	245-9408	georgecabrerap52@gmail.com
3. Penelope Uribe-Abea	5131 La Calandria Way 90032	323 529 4306	323-791-2537
4. Iris De Anda	4311 1/2 Van Horne Ave	329 791 2507	
5. Lynda De Anda	4341 Van Horne Ave. LA CA 90032	(323) 365-4164	LVGIRL7@yahoo
6. CATHERINE URIBE-ABEE	5131 LACALANDRIA WAY LA CA 90032	332-7928	7ESTREAS@gmail.com
7. Steve Abea	5131 La Calandria Way LA CA 90032	323-332-7938	steveabea@roadrunner.com
8. Arturo Minchaca	1052 Kendall Dr. San Gabriel Ca	1-626-224-7711	

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Name *

ex. John Q. Public, Jr.

Address *

ex. 1234 Main Street, Los Angeles, CA 90032

Phone Number

ex. 123-555-1234

Email Address

ex. [yourname@gmail.com](#)

Would you like to be added to our mailing list? *

Yes

No

Submit

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[Report Abuse](#) [Terms of Service](#) [Additional Terms](#)

17 signatures.

The following people signed the petition online at 90032.org.

Timestamp	Name	Address	Phone Number
6/6/2014 15:44:15	Annamarie Reza	2644 Lombardy Blvd, Los Angeles, CA 90032	323-222-3690
6/8/2014 18:12:52	Yolanda	2486 Endicott St, Los Angeles, CA 90032	
6/9/2014 16:59:58	Jorge Garcia	2486 Endicott St	
6/23/2014 20:04:13	larry wong	4859 klamath place	323-691-8294
6/23/2014 22:39:03	Andrea Garcia	4569 Zane street, Los Angeles, CA, 90032	323-362-3701
6/30/2014 15:16:41	Gabriel Fernandez	5340 Allan St Los Angeles CA 90032	
7/2/2014 14:36:41	Donna Tucker	5040 Kimball Street, Los Angeles, CA 90032	323-222-7007
8/27/2014 14:32:51	Mary Jaquez	5145 Dartmouth Avenue	3232222948
8/27/2014 15:09:44	David R. O'Dell	2231 Thelma Av. LA 90032	
8/27/2014 15:11:17	Elizabeth Berru O'Dell	2231 Thelma Ave, Los Angeles, CA 90032	
8/27/2014 20:51:01	Christopher Le	2828 Vaquero Avenue, Los Angeles, CA 90032	714-287-8799
8/27/2014 20:52:00	Victoria Shubert	2828 Vaquero Avenue, Los Angeles, CA 90032	909-573-2889
9/5/2014 8:08:49	William R Chrisley Jr	5060 Kimball Street, LA CA 90032	323-252-8615
9/5/2014 8:09:33	Lucio Aguirre Jr	5060 Kimball Street, LA CA 90032	323-893-5193
9/12/2014 15:05:28	Rainbeau	5010 Ladd Ave	3236387246
11/15/2014 16:36:00	Gil Alcaraz	4611 Richelieu terrace	310-703-9645
11/15/2014 16:36:47	Diana G. Alcaraz	4611 Richelieu terrace	310-462-9631



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Trees ENV-2015-1918-MND and etc.

2 messages

Melissa Kellogg <emailmkellogg@gmail.com>

Wed, Jan 15, 2020 at 3:11 PM

To: May Sirinopwongsagon <MAY.SIRINOPWONGSAGON@lacity.org>

Thank you for the conversation today, May. Attached is the photo of qualifying protected native "special value habitat" trees (*Juglans californica*) that were not included in the 2015 tree maps for the project in El Sereno. I am writing to advise that the planning dept ensure they have been included on any tree report supporting the proposed MND or other relevant project document.

Please let me know if you have any questions.

Melissa Kellogg
ph: 510.759.3125



Eastern Lombardy qualifying trees at corner.pdf

496K

May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Tue, Jan 21, 2020 at 7:39 AM

To: Melissa Kellogg <emailmkellogg@gmail.com>

Hi Melissa, thank you for the information.

Sincerely,

May Sirinopwongsagon
(213)978-1372
Department of City Planning
Central Project Planning Bureau
200 N. Spring Street, Room 621
Los Angeles, CA 90012

[Quoted text hidden]

The image below, from May 2019, shows (3) protected *Juglans Californica* (California Black Walnut) trees that may not appear on the protected tree report submitted as part of **ENV-2015-1918-MND** and/or other reports relevant to the proposed project.

These naturally-occurring trees, despite being frequently cut back to ground level and/or heavily pruned without relevant permits, do qualify for protection given their collective trunk size. They were not included in either of the (2) 2015 tree reports. Please ensure the tree report currently under consideration for this project includes these, and all other qualifying protected trees on site.



June 26, 2016

Mr. Gregory J. Shoop
Los Angeles Department of City Planning
200 N. Spring Street, 6th Floor
Los Angeles, CA 90012

Re: **Letter Opposing**

Case Nos.: VTT-73531, APCE-2015-2048-ZC-ZAD, ENV-2015-1918-MND

Dear Mr. Shoop:

I strongly **object** to the planned construction of 42 homes for the location of 2520 N. Eastern Avenue located at the intersection of Lombardy Blvd. in the community of El Sereno for the following reasons:

The Northeast Hillside Zone Change Ordinance was enacted specifically to protect the small communities of Northeast LA from developments such as this one. By rezoning you are essentially changing the ordinance that was designed as a fail-safe against projects like this.

This hill and surrounding parcel of land is the major defining feature of this area and this project will substantially degrade the existing visual character of the site and surroundings. There will be permanent disfigurement to the landscape and its hillside village appeal. It will damage current scenic resources and scenic vistas. Open spaces, peace and quiet, and quality of life will be degraded to a point where it will not be a livable environment for families and residents.

42 homes with 3 to 4 bedrooms are too many homes for this site: This site is a hillside with 4 lots and now the request is to subdivide into 42 lots to build 42 homes, which would saturate the hillside in the Northeast Los Angeles Area which values open space. This plan is not based on appropriation and intelligent design but rather saturation to extract maximum financial profit with disregard to all negative impacts.

Small Lot Subdivision Ordinance Should Not be approved: The homes will be too tall and close together and on this hillside will be out of place and incompatible with the existing neighborhood of mainly single-story homes. Due to the height and massing of these homes on a hillside, they will present as **out-of-scale** development that will loom large over the surrounding single-family, single-story homes.

Re-zoning Should Not be Allowed if it Increases Density: The developer asks for re-zoning of a lot that is (Q)R1-1D to (T)(Q)RD5-1D. Immediately to the south of this lot are single-family, single-story homes zoned (Q)R1-1D. A (Q)R1-1D lot should not be allowed to be re-zoned to a higher density zone especially if the adjacent developed lots are (Q)R1-1D. **The Small Lot Subdivision is NOT allowed in single-family residential zones and a request to re-zone voids this protective tenant of the Small lot Subdivision Ordinance.**

Hillside Soil Removal – Air, Noise & Light Pollution: Dust and other pollutants caused by excavating and removing 28,500 cubic yards of soil will be harmful to everybody and especially those that have respiratory health sensitivities/illnesses. Children from the schools and the El Sereno Recreation fields across the street on Eastern Avenue will suffer when they breathe in so much dust and other pollutants while at recess and after-school sports. Construction noise will be distracting to the children trying to learn at Farndale Elementary School and El Sereno Middle School. Current residents will have increased long-term noise & light pollution from such a development that will diminish their quality of life.

The project will create new sources of substantial light and glare which will adversely affect day and night time views. The impact from light pollution from hundreds of light sources where there are currently none is a very large impact.

The acoustics in this area carry sound and amplify it unlike other flat areas. Since hills surround these parcels and the homes within a relatively small area, sound carries extremely well, which create noise disturbances to hillside homes.

Traffic Congestion / Street Safety: Eastern Avenue, Lombardy Blvd. and neighboring streets already experience a high volume of traffic. Drivers drive faster than safe around the curve on Eastern Avenue where the entrance is planned and this will be a hazard because cars will slow down to enter the site and other cars will be accelerating around the curve. Increased traffic congestion with residents exiting on Lombardy Blvd. and attempting to access Eastern Ave. during school/rush-hours.

Inadequate Parking: 2 car parking garage for each house is required. With 42 homes with 3 to 4 bedrooms, there is not enough parking if there are 3 adults in a household, each with a car. No other on-site parking except for 20 guest parking spaces. Instead of building so many homes, there should be more parking for the new homeowners and any visitors they may have.

Ground Stability and Drainage: The property is located in a liquefaction and landslide area and with the proposed stresses of so many homes with square feet from 1,729 to 2,279 may cause instability for the proposed homes and damage to the current homes in the surrounding area.

Loss of Protected California Black Walnut Trees & Habitat for Wildlife: These trees are protected for a reason and their removal should not be

allowed. Digging too close to the trees during grading can cause root damage and disruption of their subsurface environment jeopardizing the remaining trees. Wildlife that makes their homes there will be displaced.

The Mitigated Negative Declaration [ENV-2015-1918-MND, VTT-73531, APCE-2015-2048-ZC-ZAD] inaccurately downplays major impacts to less than significant. These impacts cannot be lessened or "mitigated" to any degree of satisfaction. The Mitigated Negative Declaration that has been submitted does not remotely mitigate the impacts in correlation to the damage caused. These are irreversible damaging impacts.

I would like to express my **OBJECTION** to the proposed development utilizing the Small Lot Subdivision Ordinance, zone change, variance requests and Adoption of the Mitigated Negative Declaration so that it is recorded as part of the official record at the Department of City Planning's Hearing. I do not agree that the Mitigated Negative Declaration adequately shows the significant negative impacts of the planned project and I **request that a Full Environmental Impact Report (EIR) be prepared.**

Sincerely,



Signature

BURR HILSABECK

Printed Name

4906 Ladd Avenue
Los Angeles, CA 90032

Additional comments: We have lived all our adult lives in apartments in very crowded areas, dealing with parking issues, not feeling part of a neighborhood community but rather a number. We were finally able to afford to buy our first home earlier this year and chose this neighborhood specifically for the hillside views which are amazing and allow us to breath and relieve stress from the work week by getting away from the hustle & bustle of downtown where we work. This project would take away the very reason we moved here: to get away from overcrowded living spaces and be part of a small neighborhood.

Channel Law Group, LLP

8200 Wilshire Blvd.
Suite 300
Beverly Hills, CA 90211

Phone: (310) 347-0050
Fax: (323) 723-3960
www.channellawgroup.com

JULIAN K. QUATTLEBAUM, III *
JAMIE T. HALL **
CHARLES J. McLURKIN

Writer's Direct Line: (310) 982-1760
jamie.hall@channellawgroup.com

*ALSO Admitted in Colorado
**ALSO Admitted in Texas

May 24, 2017

VIA ELECTRONIC MAIL

Gregory Shoop
City Planner
City of Los Angeles
200 N. Sprint Street, 6th Floor
Los Angeles, CA 90012
greg.shoop@lacity.org

**Re: Proposed Mitigated Negative Declaration (ENV-2015-1918-MND) for the
Proposed Project located at 2520, 2532, 2608, 2668 North Eastern Avenue and
2647, 2649, 2651 Lombardy Boulevard is inadequate under CEQA;**

Dear Mr. Shoop:

This firm represents Melissa Kellogg and Demetria Contreras with respect to the City of Los Angeles' ("City") consideration of various discretionary approvals sought to build a project located at 2520, 2532, 2608, 2668 North Eastern Avenue and 2647, 2649, 2651 Lombardy Boulevard, in the neighborhood of El Sereno (the "Project"). This letter is intended to inform the Board of Public Works that the Project is not eligible for a Mitigated Negative Declaration ("MND") under the California Environmental Quality Act ("CEQA"); that the MND prepared for the Project proposes inadequate mitigation and monitoring measures; that more adequate mitigation and enforcement measures are available; that feasible alternatives to the project with less severe adverse environmental impacts are available; and that going forward with the Project without requiring a full Environmental Impact Report ("EIR") and is prohibited under CEQA. The City should prepare an EIR, rather than attempt to rely on the excessive length of the Mitigated Negative Declaration as an excuse for blinding itself to the unmitigable significant environmental impacts associated with the Project.

I. The Project

The Project site is a hilltop in the El Sereno neighborhood of the City of Los Angeles. It is directly across the street from El Sereno Recreation Center and Farmdale Elementary. El Sereno

Middle School is just one block to the north of the property. The due to the adjacent middle school and elementary school, and its location in an area of the city with few park spaces, the sports field, pool, and recreation center building are extremely popular. Indeed, when over 200 residents were surveyed by Melissa Kellogg, local residents overwhelmingly indicated that they used public parks **daily (42%) or weekly (37%)** and thought that the hilltop on the Project site should be used for public space. An excerpt of the survey results is attached as Exhibit A.

In the middle of this recreational hub, the developer seeks to level one of the few unspoiled hilltops in East Los Angeles. The developer seeks to build forty-two three-to-four bedroom houses with two-car garages on this approximately five-acre hillside. To accomplish this the applicant has requested the City grant **five** distinct discretionary approvals: a Vesting Zone Change and Vesting Tentative Tract Map (to subdivide **three lots** into **forty-two lots** with an average size of **5,200 sqft** each and build houses on all of them), a Tree Removal Permit (to remove **thirty-nine** Protected Trees), a Zoning Administrator's Determination (to build **fifty-four retaining walls** in lieu of the allowed **two**), and a Haul Route Approval (to remove **85,000 cubic yards of earth** in lieu of the allowed **3,000** cubic yards).

This development is excessive. These permits are not required to be issued for the reasonable development of the property, and feasible alternatives to the Project with less significant adverse environmental impacts exist. The property is a stellar candidate for mixed public uses, and public and private and funding sources may be willing to pay for the parks development into such a use. By turning the hilltop into an inviting point of connection to the adjacent recreational hub, such a project could unite the community. The properties could be turned into community garden space, walking trails, picnic areas, playground and exercise space, or outdoor education facilities. Many of these options are feasible, low cost, would provide a substantial benefit to the community, and would not have unmitigable environmental impacts. A public project on the hilltop could help strengthen and unite the El Sereno community; provide space for residents to exercise, play, and enjoy nature; and save the local native woodland.

The developer did not attempt to concede anything in terms of the scope or scale of the Project when redesigning it to take into consideration Protected Trees. Both before and after revision, the developer sought to build forty-two single family homes on only five acres in a

II. The California Environmental Quality Act

a. Purpose of California's Environmental Protection Statute

The California Environmental Quality Act is California's broadest environmental law. CEQA helps to guide public agencies such as the City during issuance of permits and approval of projects. Courts have interpreted CEQA to afford the fullest protection of the environment within the reasonable scope of the statutes. CEQA applies to all discretionary projects proposed to be conducted or approved by a City, including private projects requiring discretionary government approval. *See* California Public Resources Code, sections 21000 - 21178, and Title 14 Cal. Code Regs., section 753, and Chapter 3, sections 15000 - 15387.

b. CEQA's Broad Definition of a "Project" Includes *All Phases* of a Development

"CEQA broadly defines a 'project' as 'an activity which may cause either a direct physical change in the environment, or a reasonably foreseeable indirect physical change in the

environment, and ... that involves the issuance to a person of a lease, permit, license, certificate, or other entitlement for use by one or more public agencies.' [Citation.] The statutory definition is augmented by the [CEQA] Guidelines [Cal.Code Regs., tit. 14, § 15000 et seq.], which define a 'project' as '*the whole of an action*, which has a potential for resulting in either a direct physical change in the environment, or a reasonably foreseeable indirect physical change in the environment....'" *Tuolumne County Citizens for Responsible Growth, Inc. v. City of Sonora* (2007) 155 Cal.App.4th 1214, 1222, 66 Cal.Rptr.3d 645 (*Tuolumne County*). This includes all phases of a project that are reasonably foreseeable, and all related projects that are directly linked to the project. (CEQA Guidelines section 15378).

c. CEQA Has a Strong Presumption in Favor of EIR Preparation

A strong presumption in favor of requiring preparation of an Environmental Impact Report ("EIR") is built into CEQA which is reflected in what is known as the "fair argument" standard, under which an agency must prepare an EIR whenever substantial evidence in the record supports a fair argument that a project may have a significant effect on the environment. *No Oil, Inc. v. City of Los Angeles* (1974) 13 Cal.3d 68, 75, 82; *Friends of "B" St. v. City of Haywood* (1980) 106 Cal.App.3d 988, 1002.

"The EIR is the primary means of achieving the Legislature's considered declaration that it is the policy of this state to 'take all action necessary to protect, rehabilitate, and enhance the environmental quality of the state.' [Citation.] The EIR is therefore 'the heart of CEQA.' [Citations.] An EIR is an 'environmental "alarm bell" whose purpose it is to alert the public and its responsible officials to environmental changes before they have reached ecological points of no return.'" *Laurel Heights Improvement Assn. v. Regents of University of California* (1988) 47 Cal.3d 376, 392.

Under CEQA and the CEQA Guidelines, if a project is not exempt and *may* cause a significant effect on the environment, the agency *must* prepare an EIR. PRC §§ 21100, 21151; 14 Cal. Code Regs. §15064(a)(1), (f)(1). "Significant effect upon the environment" is defined as "a substantial or potentially substantial adverse change in the environment." PRC §21068; 14 Cal Code Regs §15382. A project "may" have a significant effect on the environment if there is a "reasonable probability" that it will result in a significant impact. *No Oil, Inc. v. City of Los Angeles* (1974) 13 Cal.3d 68, 83 n.16; *Sundstrom v. County of Mendocino* (1988) 202 Cal.App.3d 296, 309, 248 CR 352. This standard sets a "low threshold" for preparation of an EIR. *Pocket Protectors v. City of Sacramento* (2004) 124 Cal.App. 4th 903, 928; *Bowman v. City of Berkeley* (2004) 122 CA4th 572, 580; *Citizen Action to Serve All Students v. Thornley* (1990) 222 CA3d 748, 754; *Sundstrom v. County of Mendocino* (1988) 202 CA3d 296, 310.

III. The Mitigated Declaration is Deficient and an EIR is Required

This Project not only may have a significant effect on the environment, it *will* have a significant effect on the environment. The developer proposes to remove 96-98 trees and 85,000 cubic yards of soil—equivalent to a cube of soil with an 18,000 sqft footprint measuring 135 feet in height. The developer proposes to build forty-two houses, each with its own two-car garage on a previously undeveloped hillside. The destruction of an entire unspoiled hilltop in the City of Los Angeles, increased traffic, increased greenhouse gas emissions, and other potential growth related impacts constitute "substantial or potentially substantial adverse change[s] in the environment[.]" and an EIR is required. PRC § 21068, 14 Cal Code Regs 15382.

a. Zoning and [Q] Conditions

The Project conflicts with existing zoning code in that the proposed grading and export quantities are significantly higher than those permitted under current permanent [Q] conditions. The Project site is located in a Hillsides Zone, subject to a permanent [Q] classification under ordinance No. 180,403 regarding development in hillsides areas. The ordinance sets limits for size, height, retaining walls and grading.

Under the existing [Q] classification, grading is restricted to a maximum of 1,000 cubic yards for any one property. The requested zone change lifts the [Q] classification and does not reinstate it. The MND describes temporary (Q) conditions, but the permanent [Q] conditions including grading limits would be removed. While the site remains a hillside, located in the hillsides zone, it would not be subject to the established grading and development limitations for this zone.

The impacts of this action are severe and unmitigated and set an undesirable precedent for future hillside development throughout Los Angeles.

b. Grading

Under the existing Hillsides Ordinance and [Q] classification, a maximum of 1000 cubic yards of soil may be removed from a site in the hillsides zone – for, a total of 3000 cubic yards from the three-lot subject site.

As discussed above, the Project seeks a zone change to RD5, the zone in which small lot development is allowed. The zone change will lift the grading limits of the Hillsides ordinance and they will not be reinstated. Instead of 1000 or 3000 cubic yards, the Project would cut 82,000 cubic yards of soil from the sites.

The proposed level of grading is not necessary to reasonably develop the subject site. Site characteristics do not make strict adherence to the existing zoning regulations impractical or infeasible. The Project creates its own hardship. This level of grading and export would not be necessary for a smaller project that is more in scale with the surrounding land uses. The proposed **grading is not remedial, is not required for reasonable development of the site, it is in excess of limits set by the Hillsides Ordinance, and it cannot be mitigated.**

The Project's grading of 85,000 cubic yards of soil reduces approximately 1/3 of the existing hilltop by 30', leaving the remaining third standing in a pyramidal form, with exposed slopes cut to a grade of 2:1. It is not made clear how or whether these slopes will be replanted. No study is provided to demonstrate the impacts of this activity and mitigation measures do not address the potential impacts nor demonstrate that they can be mitigated below a level of significance. In addition to impacting the existing visual character and quality of the site, this action has the potential to alter the remaining native soil and natural drainage patterns, which in turn affects the remaining native trees on site and, in turn, the habitat they comprise.

c. Construction Activity and Hauling Route

Section IV *Environmental Impact Analysis*, IV-18 states that 7,800 truck trips are expected to remove up to 78,000 cubic yards of cut material from the Project site and that haul activity would average four haul trips per hour onto local streets. It is further states that the Project would generate an average of 87 haul trips per day over a three-month grading period.

Four haul trips per hour does not equate to 87 haul trips per day. 87 haul trips per day. It is unclear how mitigation measures, including 16-3 *Schools Affected by Haul Route* can be successfully accomplished given an average of 87 haul trips per day over a three-month grading period.

Section IV *Environmental Impact Analysis*, IV-116 indicates that Project activity, including demolition, construction, tree removal and clearing, grading and other noise-generating activities will occur daily between 7 a.m. and 9 p.m. over a period of 2 years. The construction phase schedule set forth in the emissions section (IV-14) indicates as 4-year timeline.

The Project site is located at the center of a natural amphitheater, formed by surrounding hills to the N, NE and SE sides. Notices regarding construction schedule do not serve to mitigate the impacts of sound to adjacent residences and schools over this lengthy period. Temporary sound barriers are insufficient to mitigate to a level of insignificance the impacts of sound in this terrain. Any haul route assigned will necessarily pass schools located directly across from the Project site and the single family residential area lining the streets between the Project site and freeway access points. Mitigation of the impacts of 87 haul trips per day over a three-month grading period will not be mitigated by the measures described.

d. Unsafe Access Points

The potential for entrance and exit points for this Project are limited. Those proposed are awkwardly located and present several safety concerns including:

- potential for auto queuing on a four-lane secondary highway
- the expectation that drivers cross two lanes of oncoming traffic approaching from around a bend approximately 200 feet away to access the Eastern Avenue entrance of the development from the north. This change in existing traffic pattern poses substantial safety risks on a wide street where speeding is prevalent and where curves prevent drivers from seeing oncoming traffic.
- concerns about existing speeding traffic on the curves of Eastern Avenue, where fatal accidents have occurred in the past two years.
- the expectation that drivers exiting the Lombardy Blvd. side, intending to access Eastern Avenue, cross oncoming traffic to enter a center left turn lane just 60 feet from where west bound Lombardy traffic merges into the same lane to turn left onto Eastern Avenue. Should drivers exiting this area intend to turn right onto Eastern Avenue, further safety issues arise. This intersection currently routinely sees auto queuing during moderate and peak commute hours. The presence of a painted traffic triangle and a three-way stop at Lombardy, Phelps and Budau further complicates the issue. Safety concerns at this location are not limited to drivers. During peak commute hours both sides of Lombardy

and Eastern Avenue see high pedestrian traffic as area school children walk to and from the two large schools within two blocks of the intersection.

To add to the potential risks to safety, construction vehicles and large trucks hauling soil will be added to this mix for 24 months.

The proposed Project entrance/exit points present more problems than solutions, and with increasing density in and around the Project area over time, the problems will only increase. The applicant's assessment of the traffic impacts and associated safety impacts is not sufficient to demonstrate minimal or adequately mitigated impact. Satisfactory mitigation of the traffic issues associated with the proposed scale of development is infeasible given the location and contours of the site.

e. Aesthetics

The proposed Project's level of development exceeds the existing adjacent residential densities and heights, and will leave a scarred, denuded pyramid in place of a natural hill.

Plans indicate severe grading will remove two thirds of the hilltop, leaving a 35' tall pyramid of native soil. Between 100 and 200 established homes in surrounding hillsides have views to this hilltop and the variety of trees on site. The reduction of the natural land form to an earthen pyramid, with two manufactured slopes at a 2:1 grade and abutted by two stacked 6-foot high retaining walls will clearly impact both the scenic vista from surrounding homes, and the existing visual character and quality of the site and its surroundings. The revised MND does not acknowledge these impacts. Mitigation of this type of large scale destruction of a natural vista is infeasible.

The Project requires 57 retaining walls up to 6 feet high, including two stacked 120-foot long, 6-foot high retaining walls sited prominently mid-slope on the northernmost side of the hill, plainly visible to southbound travelers on Eastern Avenue and to existing homes on Lombardy, Budau, Phelps, Ballard and Lynnfield. The count is in addition to an approximately 450-foot long wall in the public right of way along Eastern Avenue.

The City of Los Angeles has adopted ordinances limiting the height and quantity of retaining walls in hillside areas. These limitations are intended to improve and maintain the quality and character of the City's hillside neighborhoods. The quantity and location of the Project's retaining walls would alter and impact the visual character of the site. The great number of walls required for this Project (54, vs 2 as permitted) are a self-imposed hardship and indicate development plans inappropriately suited to the subject site.

The severity of grading and retaining required for this Project points to the unsuitability of the site for the proposed level of development. Alternative analysis is needed.

The introduction of lighting, including up lighting on building facades, roof top deck lighting, lighting for driveways and streets, lighting for guest parking areas, dusk to dawn lights, and path and landscape lighting, to a currently unlit hilltop amounts to a significant visual and aesthetic impact on the surrounding residential area. The MND states that the Project would not include sources of nighttime illumination that would adversely affect nighttime views in the area. No more detail is given. It is unclear how the light sources listed above would not impact the

surrounding residential area. In some cases, new homes would sit 20' back and just uphill from the property line of existing homes, bolstered by stacked retaining walls. In these situations, light trespass or spillover is difficult to mitigate. No indication is made that the retained hillside would allow for successful growth of dense tree barrier. Shrubs would be insufficient to mitigate light cast from this height. The MND does not provide sufficient information to determine how or whether mitigation would be feasible or successful.

f. Geology

There is substantial evidence in the geologic literature of a known fault crossing the north edge of the proposed development (Yerkes and Campbell, 2005). Even though the fault is not within an Alquist-Priolo Earthquake Fault Zone, it is associated with the active Elysian Park Anticline (Bullard and Lettis, 1993; Oskin and others, 2000) which has been demonstrated to cause ground deformation during past earthquakes. The MND contains no comment on the potential for future movement on the fault through the site should a large magnitude earthquake occur on the Elysian Park Blind Thrust.

Neither the consultants' reports nor the City comment letters mention the active Elysian Park Anticline as a seismic ground failure concern for the development, even though the axis of the anticline lies just several hundred feet south of the site (Yerkes and Campbell, 2005). The active Elysian Park Anticline (Bullard and Lettis, 1993; Oskin and others, 2000) has been demonstrated to cause ground deformation during past earthquakes. There is no comment on the potential for ground deformation (e.g., tilting, folding) should a large magnitude earthquake occur on the Elysian Park Blind Thrust.

g. Sensitive Species

The revised MND includes a discussion of sensitive species identified as occurring or having the potential to occur onsite.

***Lasiurus cinereus* - Hoary Bat** - Table IV-6 lists the Hoary Bat as a sensitive species with a moderate potential to occur at the Project site. The table is inaccurate in stating that the Arroyo Seco wash, 2 miles to the northwest, is the closest substantial semi-natural water source to the Project site. Approximately 3000 feet to the west a natural water source, used by a variety of wildlife species, runs through Ascot Hills.

***Juglans Californica* – Southern California Black Walnut** - The Southern California Black Walnut is among three species protected in the City of Los Angeles. It is included in the California Native Plant Society Inventory of Rare and Endangered Plants with a ranking of 4.2 marking it as a watchlisted species with limited range fairly threatened in California. The tree has an extremely limited natural range restricted to the Southern California foothills and is not found elsewhere in the state or in the nation. The species grows primarily on hillsides and its population has been reduced drastically by hillside development just like the proposed Project.

The actively propagating stand of **Southern California Black Walnut (*Juglans californica*)** existing on the Project site is part of a natural ecosystem. The number of such natural ecosystems left in our city continues to dwindle as the rewarding housing market inspires developers to build in formerly infeasible areas.

The Project has been reconfigured in order to bring it into compliance with Fire Department access codes with the coincidental benefit of tree preservation. No additional alternatives have been presented. This prompts concern that the developer does not take seriously his responsibility for preservation and careful management of the remaining stand of protected trees on this hill. Building the same 42-lot subdivision and incidentally taking down fewer trees is not a concession on the part of the developer.

Given the rapidly dwindling population of *Juglans California*, the removal of over 60% of an existing stand and the hillside on which it grows must be recognized as having a substantially adverse impact. Mitigation cannot reduce the impact wholly to a level of insignificance given the irrevocable interruption of an ecosystem which has evolved over many years.

Replanted trees would require years to reach the maturity of those being removed. The site, if developed, would see significant changes in grade and soil compaction not conducive to native tree growth. Page IV-40 states that during its operational phase, "most of the Project site would be developed with impervious surface, and all storm water flows would be directed to storm drainage features and would not come into contact with bare soil surfaces." How, then, would the remaining native trees function in such a manipulated and unnatural habitat? Fire prevention efforts would further reduce the area available for the evolution of a functional natural habitat. The reconstruction of native habitat requires far beyond replanting saplings. It is not evident, given the documents submitted, that the developer has a clear understanding of, or respect for, this responsibility.

We question whether the need to mitigate the impacts of this Project have been taken seriously, and whether all potential impacts of this Project have been accurately presented and thoughtfully and thoroughly considered by all parties involved.

Although the tree report states that an accounting of trees was done in the past 6 - 8 months, the tree count and map does not appear to record several multitrunked Southern California Black Walnut trees located near the public right of way at the corner of Eastern Avenue and Lombardy Blvd. It is unclear why they do not appear on the tree map, but they appear to qualify for protection under the city's protected tree ordinance.

h. Wildlife

As provided in the biologist's report, the Project site serves as a refuge for area wildlife. Refuges such as this are important elements of the urban landscape and their value cannot be dismissed. Intensive development like the proposed Project displaces 'urban wildlife' into residential backyards and city streets, and habitat disruption can result in an imbalance on food chain populations and lead wildlife to seek both food and shelter in residential areas and to stray into city streets and public areas.

i. Tree Removal Mitigation Measures are not Sufficient

The revised MND provides mitigation measures for the illegal removal of 8 protected trees from the subject site.

Verbal reports from the Urban Forestry Division have identified the illegal destruction of 13 protected trees. No report by a city office is included that documents the identification of the protected trees or how the number of trees removed was achieved.

The City's protected tree ordinance provides for mitigation by replanting only in the case of permitted protected tree removal. In the case of illegal removal, the ordinance provides for a hearing and withholding of building permits. Mitigation by replanting is not in accordance with the Protected Tree Ordinance.

A permit for the removal of 39 protected trees (over 60 % of protected trees on site) is requested. The mitigation measures provided are insufficient for several reasons, including:

- Mitigation does not specify that the replacement trees will be planted on site. Planting replacement trees in areas other than the subject site will not mitigate the effects of tree removal, permitted or otherwise, on the immediate community in which site exists.
- The time frame in which replacement trees will be planted is not stated.
- Mitigation does not include a requirement for professional inspection and monitoring for the survival of the trees. There is no stated length of time for which an Arborist must monitor both retained and newly planted trees. We suggest a minimum of 10 years with the arborists report to be submitted to the Department of Urban Forestry annually and, during the monitoring period, any retained or newly planted tree determined to be dead or dying will be replaced as outlined in the mitigation measures.
- There is no mitigation stated for trees damaged during construction. Reporting of damaged trees is the responsibility of the developer. It is unclear at what intervals, if any, during this four-year Project, city staff will inspect the protected trees.

There is no provision for the transfer of responsibilities for the ongoing monitoring and survival of the trees to a homeowner's association or property owner in the event the property is sold.

j. Feasible Alternatives Exist which would cause Fewer Adverse Environmental

We are disheartened by the property owners' apparent desire to maximize development on this site with little regard for the obvious impacts to the community in which it is proposed, both during construction and into the future.

The Project revision is not an improvement and the MND does little to explore alternatives to the extensive protected tree removal and heavy grading. The site was developed at one time in accordance with existing zoning, and could again be developed at a level that would not impact native tree populations or existing terrain. No alternatives are presented to reduce the number of trees removed.

Public comment at community meetings has made it clear that many members of the community value shared public space and find the proposed number of lots in this Project excessive.

Alternatives to the proposed Project include the following:

- Reconfigure the Project for fewer houses and to reduce grading to the amount allowable under the existing permanent Q conditions.
- Instead of encroaching on the public right of way with private property, dedicate space at the north side of the site for public play and exercise area along Lombardy Boulevard
- Rather than removing existing mature trees at the southwest corner of the site, dedicate a public dog park in that area.

- Reconfigure the Project to a scale compatible with existing retaining wall and grading limits.
- Dedicate the property for use as public trails, park space, or community garden space. There are various organizations that provide funding for land dedicated to public use and the property is a prime location for recreational space.

IV. The City Has Unlawfully Deferred Application of Mitigation Measures to Another Date

Many of the conditions for approval prepared by the City for the Project are designed to mitigate the environmental impacts of the Project. Unfortunately, the City has simply deferred environmental analysis to another date in many of these conditions of approval. This does not comply with CEQA.

Conditioning a project on another agency's future review of environmental impacts, without evidence of the likelihood of effective mitigation by the other agency, is insufficient to support a determination by the lead agency that potentially significant impacts will be mitigated. *Sundstrom v. Cnty. of Mendocino* (1988) 202 Cal.App.3d 296. Further, requiring formulation of mitigation measures at a future time violates the rule that members of the public and other agencies must be given an opportunity to review mitigation measures before a project is approved. PRC § 21080, subd. (c)(2)). See *League for Protection of Oakland Architectural & Historic Resources v. City of Oakland* (1997) 52 Cal.App.4th 896; *Gentry v. City of Murrieta* (1995) 36 Cal.App.4th 1359, 1396; *Quall Botanical Ganlens Found., Inc. v. City of Encinitas* (1994) 29 Cal.App.4th 1597, 1605, fn. 4; *Oro Fino Gold Mining Corp. v. Cnty. of El Dorado* (1990) 225 Cal.App.3d 872, 884; *Sundstrom v. Cnty. of Mendocino*, supra, 202 Cal.App.3d at p. 306, (condition requiring that mitigation measures recommended by future study to be conducted by civil engineer evaluating possible soil stability, erosion, sediment, and flooding impacts was improper). Moreover, a condition that requires implementation of mitigation measures to be recommended in a future study may conflict the requirement that project plans incorporate mitigation measures before a proposed negative declaration is released for public review. PRC § 21080, subd. (c)(2); 14 Cal Code Regs § 15070(b)(1). Studies conducted after a project's approval do not guarantee an adequate inquiry into environmental effects. Such a mitigation measure would effectively be exempt from public and governmental scrutiny.

V. At a Minimum, Recirculation of the MND is Required

Once a negative declaration has been circulated, it may need to be recirculated for another round of review and comment if it is "substantially revised" after the public notice of the first circulation period has been given. 14 Cal Code Regs § 15073.5(a). A substantial revision includes two situations (14 Cal Code Regs § 15073.5(b)):

1. *A new, avoidable significant effect is identified, and to reduce that effect to a level of insignificance, mitigation measures or project revisions must be added.*
2. *The lead agency finds that the mitigation measures or project revisions originally included in the negative declaration will not reduce potentially significant impacts to a level of insignificance, and new mitigation measures or project revisions are required.*

New information will require recirculation when it amounts to a substantial revision of the negative declaration, which is defined to mean the identification of new significant environmental impacts or the addition of new mitigation that is required to avoid a significant environmental impact. 14 Cal Code Regs §15073.S(b).

In this case, numerous deficiencies with the MND have been identified. Further, “new information” has been provided to the City, which requires that the MND be corrected and recirculated. We request that the City provide 60 days for public review of any Recirculated MND. This will allow for careful review of the MND and the opportunity to work cooperatively with the City and the applicant to resolve any issues that may arise.

As stated above, however, a full EIR is required—recirculation of the MND would not be sufficient.

VI. Conclusion

For the reasons listed above, Melissa Kellogg and Demetria Contreras respectfully contend that the City cannot approve the Project in light of the deficient MND. I may be contacted at 310-982-1760 or at jamie.hall@channellawgroup.com if you have any questions, comments or concerns.

Sincerely,

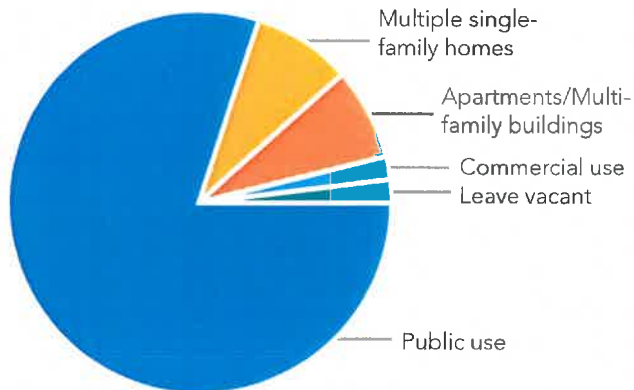
Jamie T. Hall

Exhibit A



Eastern Lombardy Survey Results Summary

What would you like to see the Eastern and Lombardy lots used for?



88%

Amount of respondents who said they would use a new park/open space that was within walking distance of their home.

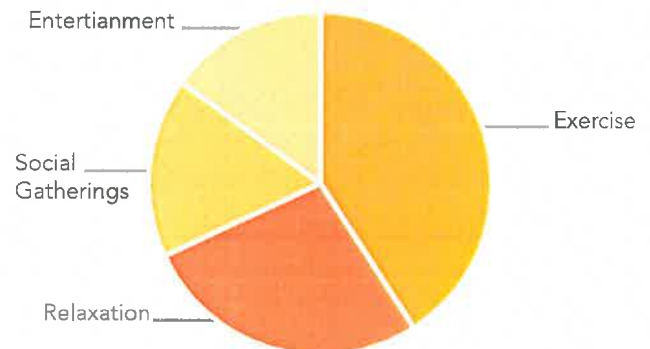
If you selected Public Use, what would you like to use there?¹



How often do you use local parks?²



How do you use your local parks?



90032.org



info@90032.org



@ES90032



facebook.com/preserveelsereno

Note: Survey was conducted in person and online, in English and Spanish, between June 1st and November 30th, 2014, with over 200 respondents from the immediate area.

¹ Respondents were allowed to select multiple answers
² 6% of respondents said they do not use their local parks



Greg Shoop <greg.shoop@lacity.org>

ENV-2015-19182 messages

Melissa Kellogg <emailmkellogg@gmail.com>

Thu, May 26, 2016 at 10:23 AM

To: Blake Lamb <blake.lamb@lacity.org>

Cc: greg.shoop@lacity.org, Kevin Ocubillo <kevin.ocubillo@lacity.org>

Dear Blake,

Thank you for directing me to the on-line location of the environmental documents published on May 12, 2016 for ENV-2015-1918: http://planning.lacity.org/eir/publication/Pub_051216.htm#1918

It is my understanding that these documents published by the planning department are the entirety of the documents the department puts forth to substantiate the determination that *although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions on the project have been made by or agreed to by the project proponent* and to outline the sufficiency of the mitigations.

The proposed MND determines that the project presents no impacts or less than significant impacts in the areas of Geology and Soils. However, reports referenced in the proponent's response for this finding are not present with the published documents.

In IV. Environmental Impact Analysis, Section 6. Geology and Soils, a Geologic and Geotechnical Engineering Review prepared by GeoSoils Consultants Inc. is referenced and notes refer the reader to Appendix D for this document. This report is not found in the planning department's physical file, nor among the published documents.

Appendix D as published contains only reports prepared by Hu Associates in January of 2008 and Ray Eastman in May of 2007, both for projects previously proposed and determined by the CPC to be unsuitable for this site. No other reports are included.

Additionally, the July 24, 2015 Geology and Soils Approval Letter (log# 86839-03) issued by the Department of City Planning, lists 14 total reference reports and letters relevant to this project, including (2) reports prepared by GSS Engineering in 2008 and 2011, and (4) reports prepared by GeoSoils Consultants in 2015.

Each of these reports and the associated Department correction and approval letters are essential to understanding the Geology and Soils. It is not possible to make an accurate determination of the environmental impacts of this project without them.

It concerns me deeply that the omission of these significant documents, related to such a complex area of impact, has been overlooked. This leads me to question the department's determination and whether all potential impacts of this project have been accurately presented by the project proponent, and thoughtfully and thoroughly reviewed by the lead agency.

I respectfully request that the department publish these essential documents and announce a 60 day extension to the MND comment period beginning on the date the documents are published, to provide sufficient time for interested parties to comment on the department's proposed adoption of these documents.

Many thanks for your attention to this issue.

Cordially,

6/7/2016

City of Los Angeles Mail - ENV-2015-1918

Melissa Kellogg
ph: 323.222.6467
emailmkellogg@gmail.com

Blake Lamb <blake.lamb@lacity.org>
To: "Shoop, Greg" <greg.shoop@lacity.org>

Thu, May 26, 2016 at 10:32 AM

hi greg, can you make sure Melissa can find the docs she needs and respond to her? Also can you let her know about the extension?

Blake

[Quoted text hidden]

Mr. Gregory Shoop
City Planner
Los Angeles Department of Planning
200 N. Spring Street, Room 621
Los Angeles, CA 90012

July 14, 2016

RE: ENV-2015-1918-MND, APCE-2015-2048-ZC-ZAD, VTT-73531-SL

Dear Mr. Shoop;

Please accept this response to the Proposed Mitigated Negative Declaration filed for ENV-2015-1918-MND, a small lot subdivision project proposed for the hillsides and hilltop of lots located at the corner of Eastern Avenue and Lombardy Boulevard in El Sereno.

We strongly oppose the proposed project and find for the following reasons that the Mitigated Negative Declaration (MND) is deficient in its analysis of the significant environmental impacts the project would bring.

1. AESTHETICS:

The subject site is a 4.8 acre undeveloped hill located in Northeast Los Angeles, an area of the city recognized for the hills that define its natural skyline and the general character of the area. Whether actively or passively managed, each remaining undeveloped hill contributes to the enduring identity of this area of Los Angeles, just as the beaches and canyons are intrinsic to the identity of other areas of the city.

If blanketed with multi-story homes standing taller than any tree on site, the natural skyline of the subject site would be substantially changed. The natural contours of the existing hilltop would also be substantially modified by removing and exporting 28,500 cubic yards of soil to access bedrock for a portion of the lots proposed. This level of modification, lowering the hilltop by 20 feet, is unnecessary and is simply a hardship brought upon itself by the proposed size of the project.

The proposed project's level of development exceeds the existing adjacent residential densities and heights and is further exacerbated by the location of the largest components of the project (33 of 42 structures with optional roof decks), elevated and overlooking adjacent uses.

In its undeveloped state the existing hill provides, for people traveling along Eastern Avenue and enjoying the El Sereno Recreation Center and Park, visual relief from the residential development that fronts the length of this secondary highway in both directions. The addition of houses to the site would diminish this effect and **would introduce hard materials to reflect and amplify traffic sound** instead of capturing and absorbing the sounds as the existing trees, earth and grasses do.

The hill stands as a **visual and aural buffer** between Eastern Avenue and the developed streets hidden behind the hill. The proposed development would eliminate this beneficial natural element. These losses cannot be mitigated to a level of insignificance by the project as it is proposed.

The **introduction of lighting**, including up lighting on building facades, roof top deck lighting, lighting for driveways and streets, lighting for guest parking areas, and path and landscape lighting, to a currently unlit hilltop amounts to a **significant visual and aesthetic impact** on the surrounding residential area,

as well as a **biological impact on area wildlife that are naturally active in the dark**. The use of dark sky lighting in public areas would mitigate only a small portion of the impact and would not lessen the impacts on those residences located below the hilltop lots to a level of insignificance.

23 retaining walls of varying height are proposed for the project, but no plans specifically describing the various walls, their locations and specific heights, with scale sections and elevations are present among the project documents. Figure 11-7, a project site plan prepaid by CAJA Environmental Services, implies walls, but contains no level of detail. **The impacts on safety and aesthetics cannot be fully assessed without detailed retaining wall plans.** While all walls have the potential to significantly impact the safety and aesthetics of the environment, of particular concern is a wall located at the cusp of the hilltop's south side (lot 26 in the diagram).

3. AIR QUALITY:

The applicant's response to impacts of air quality acknowledges that measures and benchmarks are difficult to gauge and identify. It is not difficult, however, to recognize that the disturbance and removal of 20' of hilltop, along with other soil removal across the site, will necessarily impact daily air quality for visitors traveling to the nearby senior center and recreation center, children walking through the neighborhood to attend the two schools across the street, those same children at play during recess and lunch periods, workers at the nearby carwash and cafe, transit riders waiting at bus stops on Eastern Avenue and on Lombardy Boulevard, and residents of homes in the immediate area who remain at home during the day.

The January 5, 2015 report by GeoSoils Consultants notes: "The bedrock is dense, but rippable with heavy duty grading equipment." This is no light excavation project but, instead, a seriously impactful process. Add to this emissions from heavy duty equipment on site and trucks traveling to and fro along the haul route. **These impacts are an unavoidable byproduct of intensive hillside development such as is proposed for this project and, given the heavily occupied specific location for which it is proposed, they simply cannot be mitigated to a level of insignificance with soil stabilizers or by 'applying water to disturbed areas of the site three times a day.'**

4. BIOLOGICAL RESOURCE

Trees

The subject site hosts a **natural and actively propagating stand of California Black Walnut (*Juglans californica*)**. This species is protected in the City of Los Angeles and other Southern California municipalities, and is included in the California Native Plant Society Inventory of Rare and Endangered Plants. It has an extremely limited natural range restricted to the Southern California foothills and is not found elsewhere in the state or in the nation. The species grows primarily on hillsides and its population has been reduced drastically by hillside development like the proposed project.

The once broader population of *Juglans californica* has seen heavy losses in the Los Angeles area. Writing in the botanical journal *Fremontia* in 2002, E.N. Anderson calls the stands of walnuts in the Puente Hills and the hills of downtown Los Angeles "desperately endangered" and names these areas as the first priority for conservation.

Anderson notes that in addition to providing shade and being an essential part of a native habitat, the trees provide erosion control with an extensive root system and readily re-generate even after fire damage. This extensive root system, which can be sensitive to off-season water, would mean that **any**

tree intended to be retained on site would require protection above and beyond that proposed by the applicant.

The City of Los Angeles has confirmed that (13) mature protected California Black Walnut trees were illegally destroyed on the subject site in October 2014 while the site was under the joint governance of Mr. Alvarado Banegas/Bancomer Development and Clearwater Communities. Following the unpermitted removal of native trees, the required project tree preservation report was prepared which identifies 102 mature protected trees remaining on site. The report does not document the number of saplings or trees measuring a trunk diameter less than 4 inches. The project calls for the removal of 67% of those remaining mature native trees, with a proposal to replant on the project site. **No information is provided to ensure *Juglans californica* will be used for replanting and none is provided to ensure the site, once developed, will have the capacity to support these native trees.**

Given the rapidly dwindling population of *Juglans California* in its native range, the removal of over 60% of an existing stand and the soil in which it grows must be recognized as having a substantially adverse impact with the understanding that mitigation cannot reduce the impact wholly to a level of insignificance given the irrevocable interruption of an ecosystem which has evolved around this stand of native trees over many years.

Young trees planted on this site or any other would require many years to reach the level of maturity of those being removed. The site, if developed, would see significant changes in grade and soil compaction not conducive to *J. californica* growth, and houses would be built into the slopes the trees favor. Fire prevention efforts would further reduce the area available for the evolution of a functional natural habitat. **The reconstruction of native habitat goes far beyond replanting saplings. It is not evident, given the documents submitted, that the developer has a clear understanding of, or respect for, this responsibility.**

Regarding the **Tree Preservation Report** submitted by Clearwater Communities, the last line of page 12 states "No protected trees will be removed." **This statement clearly conflicts with statements about protected tree removal in other documents submitted for this project.**

Regarding **mitigation measures for replacement of protected trees**, specifically: "the canopy of replacement trees, at the time they are planted, shall be in proportion to the canopies of the protected tree(s) removed," the tree report documents the canopies of those trees as ranging from 12 feet (tag #54) to 45 feet (tag #7). **It is inconceivable that replacement trees can be found in proportion to these canopies, and simply impossible that trees of that size would successfully survive replanting on the subject site.** That this illogical statement is included in the project documents **calls into question whether the need to mitigate this impact has been taken seriously, and whether all potential impacts of this project have been accurately presented and thoughtfully and thoroughly considered by all parties involved.**

The project's **grading plan reduces soil levels to bedrock and returns only 5' of compacted fill, an amount insufficient to safely support deeply rooted, soil stabilizing native trees.** Even trees considered to have shallow root structures, such as the California Sycamore, include root systems reaching deeper than 5 feet. Roots will not extend into bedrock. **It is unclear, then, how the project can adequately or safely enact the mitigation measures outlined with respect to tree removal and the replanting of hundreds of replacement trees.**

Wildlife

Wildlife corridors or linkages are interconnected tracts of land through which native species can disperse. They are characterized by significant natural resource value and provide pathways for gene flow, seed dispersal, daily home range movements, seasonal or altitudinal migration, and dispersal habitat for juveniles. They allow for daily and seasonal movements as well as movements over many generations. Spatially, wildlife corridors function on a broader landscape/ecosystem scale with no absolute size, or at smaller spatial scales, such as home range.

Though natural landscapes have an inherent degree of connectivity, over the past 50 years habitat alteration has greatly reduced this connectivity. **Establishing connections between isolated or fragmented habitat patches is essential for sustaining natural ecological processes, population viability, and biological diversity.** The opportunity to protect and/or restore connectivity with regard to the subject site exists with restoration activity in Ascot Hills, remaining undeveloped fragmented habitat in the immediate area, and protected and unprotected lands to the north.

Loss of connectivity is by no means inevitable and development does not have to result in a proliferation of barriers to wildlife movement. **On April 22, 2016 the Los Angeles City Council directed the writing of an ordinance to create a wildlife corridor in its Hillside Ordinance Zone, demonstrating the value it places on providing for connectivity.** This ordinance will require easements and deed restrictions in perpetuity to protect wildlife habitat connectivity and will designate a Regional Wildlife Habitat Linkage Zone "in as much as just one single family residential project can cause adverse impacts to the existing and threatened wildlife population."

The MND claims no impact to the movement of any native resident or migratory wildlife species, and claims 'the area is not used as a significant wildlife corridor.' **No verification or studies are presented to document this claim.**

The MND is insufficient in that it claims no impacts and sets forth no mitigation. Any person observing from the street or neighboring properties during daylight hours will view hawks as well as many smaller native and migratory bird species perched among the trees on this site, hunting, feeding, breeding and nesting among the existing vegetation, and moving to and from the site. This undeveloped site is also valuable to nocturnal wildlife. **The proposed project would substantially degrade the environmental quality of this existing wildlife habitat. A full EIR is necessary to reveal the impacts to actual and potential local and migratory species living on and moving through the subject site.**

6. GEOLOGY AND SOILS

Fault Lines

There is substantial evidence in the geologic literature of a known fault crossing the north edge of the proposed development (Yerkes and Campbell, 2005). Even though the fault is not within an Alquist-Priolo Earthquake Fault Zone, it is associated with the active Elysian Park Anticline (Bullard and Lettis, 1993; Oskin and others, 2000) which has been demonstrated to cause ground deformation during past earthquakes. **The MND contains no comment on the potential for future movement on the fault through the site should a large magnitude earthquake occur on the Elysian Park Blind Thrust.**

Seismic Ground Shaking and/or Failure

Neither the consultants reports nor the City comment letters mention the active Elysian Park Anticline as a seismic ground failure concern for the development, even though the axis of the anticline lies just several hundred feet south of the site (Yerkes and Campbell, 2005). The active Elysian Park Anticline (Bullard and Lettis, 1993; Oskin and others, 2000) has been demonstrated to cause ground deformation during past earthquakes. **There is no comment on the potential for ground deformation (e.g., tilting, folding) should a large magnitude earthquake occur on the Elysian Park Blind Thrust.**

Landslides

The developer **does not present sufficient evidence** regarding adverse effects related to landslides. Previously approved development in hillside areas of El Sereno has spurred landslides and encountered sinkholes and land subsidence, **demonstrating the unpredictable nature of the area's geology and the potential for such threats to be overlooked or underestimated without the benefit of a full EIR.**

Soil that is Unstable

Large portions of the development would be constructed on **compacted fill, which poses risk when not adequately compacted under specific conditions with moisture content closely monitored.** At a minimum structures on improperly compacted fill will see severe wall cracks, foundational problems revealed long after the builder's warranty expires, or structural failures with larger issues such as platform settlement, wall collapse and failed slopes posing significant risk. **A greater risk is posed by improperly compacted fill in liquefaction zones, such as the project area.**

Should compaction efforts be inadequate, either due to miscalculation, inadequate oversight, non-compliance or other factors, people and structures, including but not limited to neighboring downhill properties, **would be exposed to potential substantial adverse effects including the risk of loss, injury or death, as well as creating a blight on the site and the community in which it occurs.**

The proposal to build 42 homes, with the majority sited on compacted soil and positioned above both existing and proposed new homes is not well-suited for the subject site. **The potential impacts to safety and welfare require a more extensive exploration.**

23 retaining walls of varying height are proposed for the project, but no plans specifically describing the various walls, their locations and specific heights, with scale sections and elevations are present among the project documents. Figure 11-7, a project site plan prepaid by CAJA Environmental Services, implies walls, but contains no such detail. While all proposed walls have the potential to significantly impact the safety and aesthetics of the environment, of particular concern is a wall located at the cusp of the hilltop's south side (lot 26 in the diagram). The safety of this wall, and others, with regard to site soil stability cannot be assessed without further information.

The project as proposed **promotes grading and limits area for potential landscaping, this contributes to decreased slope stability and the increased possibility of slope failure.** The removal of over 100 established trees with extensive and stabilizing root systems would significantly increase soil instability on the site. Given the number of structures, the space allotted between individual structures, the footprint of the project in relation to property lines, and fire clearance and defensible space best practices, **very limited space is allotted for vegetation and the incorporation of trees with stabilizing root systems.**

The **grading plan reduces soil levels to bedrock and returns only 5' of compacted fill, an amount insufficient to safely support deeply rooted, soil stabilizing native trees.** Even trees considered to have

shallow root systems, such as the California Sycamore, have root systems reaching deeper than 5 feet. Any tree planted in severely reduced soil levels poses a significant risk of uprooting, further exacerbated if located on a hilltop or near the cusp of a slope. Roots will not extend into bedrock. **Re-vegetation cannot be counted upon to aid in soil stabilization.**

No plans are presented for drainage or water infiltration. The impacts of drainage on soil stability cannot be assessed.

Expansive Soils

According to reports prepared by Ray A. Eastman and included in appendix D of the MND documents, **soils on the subject site have a high expansive potential.** The project applicant cites only the preparation of a Final Geotechnical Report to address building standards and pledges subsequent compliance with those standards. Whether this report and the recommended standards would alter the proposed project or bring about additional direct or indirect impacts cannot be known at this time. **The MND is inaccurate in stating the impacts are less than significant as an accurate assessment of the impacts related to expansive soils, and associated direct and indirect impacts cannot be made without further information.**

Excessive Grading

The applicant proposes the excavation and removal of 28,500 cubic yards of soil to remedy unstable geologic conditions. It is not readily discernable from the available documents how much additional soil would be graded and reused on site.

The applicant describes the excessive excavation and export of soil in order to build 42 homes as remedial, which would earn an exemption from the City's protective Hillside Ordinance. Under the Northeast Los Angeles Hillside Ordinance, a maximum of 1000 cubic yards of soil is permitted to be removed from a site in the hillside zone – for, a total of 3000 cubic yards from the three-lot subject site. Los Angeles City defines remedial grading as grading that is necessary to mitigate a geologic or geotechnical hazard on a site.

No geologic or geotechnical hazard is demonstrated to currently exist on the subject site, and none would be posed by a project sized to suit the site. The proposed level of grading is not necessary to develop the subject site. The extensive amount of grading and the excessive number of retaining walls is proposed for the purpose of constructing 42 houses on the project site. The project site is in a hillside area and there is nothing unique about the sizes or shape of the proposed lots. **Site characteristics or existing improvements do not make strict adherence to the zoning regulations impractical or infeasible.**

The quantity of buildings specified for this project would force intensive grading of areas which would not otherwise be considered for the proposed use. The project creates its own hardship. This level of grading and export would not be necessary for a smaller project that is more in scale with the surrounding land uses. **Therefore, the grading is not remedial and is in excess of limits set by the Hillside Ordinance.**

Any attempt by the applicant to justify the amount of grading and export by citing the subdivision of 3 parcels into 42 parcels would ridicule the City's protective Hillside Ordinance. The administrative process of subdivision should not become the loophole that allows for the carting away of over 28 *thousand* cubic yards of real-life, physical hillside – more ten times the amount permitted for that same hillside defined as 3 lots instead of the desired 42.

Undue Influence

Grading maps indicate **the project is not fully contained to the project site, but neighboring properties must be contoured to support the project.** Permissions from the owners of the affected adjacent properties are not included among the project documents. The July 24, 2015, Geology and Soils Approval Letter finds the tentative tract map and the referenced reports acceptable provided certain stated conditions are met. Those conditions include "Secure the notarized written consent from all owners upon whose property proposed grading/construction (such as retaining walls) access is to extend." To approve a project pending approval by these homeowners, would place undue pressure and **would unduly influence these homeowners' decisions** on whether to agree to allow grading or construction to extend to and directly impact their properties.

7. GREENHOUSE GAS EMISSIONS

The Impact of Tree Removal

Trees reduce carbon dioxide in the air, thereby reducing the warming 'greenhouse' effect of the gas in two main ways (McPherson, 2007, Urban Tree Planting and Greenhouse Gas Reductions), taking carbon dioxide out of the air as they grow, providing shade, and transpiring water which lowers air temperature and in turn cuts energy use, reducing emissions at power plants. Over the lifetime of a single tree, several tons of carbon dioxide are taken up (McPherson and Simpson, 1999, Guidelines for Calculating Carbon Dioxide Reductions through Urban Forestry Programs).

The project applicant declares Greenhouse Gas Emissions through the combustion of fossil fuels by heavy-duty construction equipment and through vehicle trips generated by workers, owners, inspectors and other individuals traveling to and from the project site. The applicant outlines its intention to adhere to various established mandatory requirements for efficiency and Green building.

The applicant's calculations for greenhouse gas **impacts do not include the removal of over 100 trees and the destruction of unnumbered young and emerging trees on the subject site which, left standing, would remove several hundred tons of carbon dioxide from the air over their lifetimes.** This action significantly offsets the gains claimed by the project applicant's proposed 'reduction from business as usual' amortized over a 30-year period at the subject site. **Further consideration is required before the project's impacts related to Greenhouse Gas Emissions can be fully understood.**

Solid Waste Generation

The project applicant outlines construction waste reduction, source reduction, recycling and composting requirements to which the project would be subject, but **does not clearly state how the required reductions would be achieved.** In Table IV-8 Project Consistency with AB32 Scoping Plan GHG Emissions Reduction Strategies, the applicant states the project would have minimal impact on solid waste facilities, **but does not provide supporting documentation or demonstrate how this is achieved.** Without this information the MND is incomplete and impacts cannot be accurately assessed.

8. HAZARDS AND HAZARDOUS MATERIALS

Interference with Emergency Response Plan or Emergency Evacuation Plan

The project **applicant claims no impact** regarding physical interference with an adopted emergency response plan or emergency evacuation plan.

Per the 2001 Los Angeles Operational Area Mass Evacuation Process Guide, prepared as part of the Regional Catastrophic Preparedness Planning for Los Angeles and neighboring counties, primary evacuation routes in the Los Angeles Operational Area consist of the major interstates, highways, and prime arterials. Area Senior Centers and schools are recommended evacuation points. **The primary access point during the estimated 12 month construction period is a single lane access point off of Eastern Avenue, a four lane secondary highway, within 500 feet of the primary access points for the El Sereno Recreation Center, Farmdale Elementary School and the El Sereno Senior Center.** The movement of numerous trucks daily along this route, and the movement of large trucks in and out of a single access point situated on a four lane secondary highway, only 200 feet and around a bend from a signaled intersection, will necessarily even impact daily traffic. **It is inconceivable that it would have no impact on emergency evacuation efforts, should the need arise.**

Further, Eastern Avenue is a primary route used by emergency vehicles in area emergency response. **The presence of construction vehicles on this route introduces an obstacle to emergency response that would not exist in this area were it not for the proposed project and the excessive grading that requires 28,500 cubic yards of soil to be trucked away from the site.**

The project applicant makes no statement regarding mitigation of these impacts. The MND is insufficient to substantiate the finding of no impact.

9. HYDROLOGY AND WATER QUALITY

Interference with Groundwater Recharge

The applicant's statement "during stormwater events, most of the stormwater flows from the Project site to the local streets" is **not substantiated with any hydrology report or other verifiable report.** Streets border only two sides of the site. The path of water captured on generally flat and even minimally swaled areas of the site, along with water moving from the site to lower elevations at or beyond the property lines during a storm water event, cannot be adequately verified by the limited number of borings cited (1 per 21,275 square feet). **There is not enough information provided** to support the assertion that the site is not an area of or instrumental in groundwater recharge.

10. LAND USE AND PLANNING

Physical Division of an Existing Community.

The project presents as an urban community under the name Vista El Sereno. The project site is located in the Los Angeles neighborhood of El Sereno and is situated between the El Sereno Recreation Center and commercial businesses 2000 feet to the east along Alhambra Avenue, and between the El Sereno Senior Center and the commercial business areas approximately 4000 feet to the north along Eastern Avenue and Huntington Drive. These businesses characterize, in part, the El Sereno neighborhood.

A planned and named community, particularly when constructed in a style that is inconsistent with the majority of the surrounding residences, necessarily divides the established community by which it is surrounded, whether it be gated or not. The introduction of a planned hilltop community now known as Monterey Hills, in an area once known as El Sereno, re-identified that area as a separate community. There is reason to believe that the proposed project situated not at the edge of but within a long-established community, would unnecessarily divide the existing community. The project applicant claims no impact for this environmental factor. **Further consideration is warranted.**

Conflicts with Land Use Plans, Policies or Regulations

The applicant's claim of Less Than Significant Impact is **not adequately supported**. In several cases the **information provided to support this claim is irrelevant or is inaccurately stated**.

Conflicts with the General Plan Framework Element

The proposed project conflicts with the Los Angeles City general plan framework element in several ways.

The **policy for housing** in the Framework Elements demonstrates **conservation of the scale and character of existing stable residential neighborhoods** is a priority of the plan. While the proposed project is comprised of single family homes, the **scale and density differ widely** and **presents a jarring transition** from the immediately surrounding neighborhood which consists of single story cottage style, Spanish style and traditional homes dispersed at a lower density than proposed for the project site.

The Framework Element's **Open Space and Conservation objectives** include establishing a linear open space and greenway system that connects open spaces and is linked to communities and neighborhoods...Bikeways, hiking trails, parks and passive open space are among the improvements that may be considered." The second objective is "Consider open space as an integral ingredient of neighborhood character." Other objectives include: "Consider appropriate methods to protect significant remaining major open spaces, including hillsides and undeveloped areas" and "Promote the joint use of open space with public facilities (schools, transit, and other)." **The site's potential as part of linear open space system** providing the immediate community with the various benefits of open space, serving as an integral ingredient of the neighborhood's character, and offering the possibility of joint use by schools that exist in the immediate area of the site is **eliminated by the full-scale development of the site**.

The Framework Element's **Transportation objectives** include "Protect residential neighborhood from the intrusion of additional traffic generated by new regional or local development." The proposed project introduces 42 homes, each with two car garages, in a residential area distant from busways or rail transit, active city centers and areas of business and commerce. Its proximity to transit lines is insubstantial. It can be expected that two or more residents of each house will travel separately to and from work and/or school each weekday contributing to existing commute-hour traffic. The burden on the existing residential neighborhood would be intensive due to location of the project and the complicated entrance/exit points on Eastern Avenue and on Lombardy Blvd. **The project documents do not clearly demonstrate how the problematic entrance/exit points will be ameliorated**.

The proposed project fronts on Lombardy Boulevard and on Eastern Avenue, neither of which are identified in the Framework Element as a pedestrian or transit related priority, nor as a Busway Route, Pedestrian-Priority Street and District, Transit-priority Highway, or Future Transit Priority Highway. **The applicant's claim to compliance with the Northeast Los Angeles Community Plan and the Compass Blueprint Report, or other plans based on proximity to transit lines is insubstantial**.

The project area is not identified in the Framework or the associated long range plan diagram as a neighborhood district, community center, or other type of center and is not recommended as an appropriate location for increased density. The subdivision of three lots into 42 lots and the construction of multi-story homes with roof decks among an established area of primarily single story homes necessarily increases density and is not compatible with existing concentrations of use. The project site is not near existing or potential public transit corridors and stations identified in Figure 8-2, the Framework Element's Citywide Pedestrian & Transit Related Priorities Linked to Urban Form map.

Per the Framework Element in those stable neighborhoods characterized by a mix of densities and dwelling types, permitted densities may be reduced to levels consistent with the character of the entire area in order to minimize impacts on infrastructure, services, and/or maintain or enhance the residents' quality of life. The loss of potential units in these locations can be offset by the provision of new housing opportunities in mixed-use districts, centers, and boulevards. Even through the lens of a mixed density neighborhood, the proposed project's level of density is inconsistent with the character of the entire area.

Regarding policy 3.2.4 *Provide for the siting and design of new development that maintains the prevailing scale and character of the City's stable residential neighborhoods and enhance the character of commercial and industrial districts:*

Note that the applicant **omits pertinent language** "...of new development that maintains the prevailing scale and character..." in the restatement of this policy found in the El Sereno Project IV. Environmental Impact Analysis page IV-74. This error **calls into question the accuracy of the documents submitted by the applicant with regard to policy and code compliance.**

Conflicts with the Northeast Community Plan

The Northeast Community Plan Map designates the subject site for Low Residential General Plan Land Uses. The plan includes the following relevant objective and policies relevant to housing in residential hillside areas:

- a. Minimize grading commensurate with the densities shown on this Plan; and
- b. Limit land use intensities and population densities to those which can be accommodated by the transportation system, public service facilities, utilities and topography.

The project is in a hillside area. There is nothing unique about the lot size or shape of the proposed lots. As stated in Policy b, land use should be limited to that which can be accommodated by the topography.

The proposed project would add to, rather than avoid, the problematic issues faced by Northeast area communities and outlined in the Northeast Community Plan, including:

- Incompatibilities among types of land uses and scale of development that detract from established neighborhood or community character.
- Destruction and deterioration of community resources, including scenic views and view sheds, open space, open space corridor and historic structures.

It would bypass or eliminate opportunities outlined in the plan, including:

- Preservation of open space and open space corridors, and enhancement of greenways amenities and public access, particularly in hillside areas and near flood control channels.
- Development of measures to require that hillside development be of low density, appropriate in scale, and minimally disruptive of the natural terrain.
- Potential for specific plans or other zoning actions to limit density and intensity of development, encourage compatible uses and services, and enhance visual character near light rail station stops and major development sites.
- Developing guidelines and schedules for upgrading or removing unsightly infrastructure facilities, e.g. utility poles.

It would conflict with policies of the Northeast Los Angeles Community Plan including:

Policy 1-5.3 "Consider the steepness of the topography and the geologic stability in any proposal for development within the Plan Area." The project's scale demands a substantial amount of grading on the site. Section (7) Environmental of the Zone Change entitlement filing for the project would allow for a maximum 81,569 cubic yards of soil cut, 23,522 filled, and the export of 58,038 cubic yards of soil from the site. Rather than scaling the project to suit the site, this proposal struggles against the existing topography to advance its own agenda.

In its supporting documents, the applicant describes the project as an extension of the existing residential neighborhood. However, the project's architectural style, its density and its heights and massing are not visually compatible with the surrounding neighborhood.

No data is provided to demonstrate that the project would include adequate driveway access to prevent auto queuing. Entrance to the driveway on Eastern Avenue, for cars approaching from the North, requires crossing two lanes of southbound traffic. With no signal present, and with southbound traffic approaching from around a bend approximately 200 feet away, significant potential for auto queuing exists, along with potential safety risks.

Conflicts with the Compass Blueprint Report

The applicant's demonstration of project consistency with the Compass Blueprint report is weak and, at times, irrelevant. Where the policy is stated as "Locate new housing near existing jobs and new jobs near existing housing" the applicant's response of "The project is infill development of housing within the Northeast Los Angeles Community Plan area of the City with within proximity to transit." **This does not demonstrate the project to be located near existing jobs.**

Similarly, where the policy is stated "Provide a variety of housing types in each community to meet the housing needs of all income levels" the applicant's response "The project includes development of 42 homes, each with 3 bedrooms" **does not demonstrate variety or housing types and does not demonstrate how this serves to meet the needs of all income levels.**

Where the policy is stated "Focus development in urban centers and existing cities" the applicant's response does not demonstrate consistency with the policy. The project area is not currently identified as an urban center, nor is it identified in the Los Angeles General Plan Framework Element or in the Northeast Community Plan as an area targeted to become an urban center. It is located in the City's Hillside Zone.

In general, the **project is inconsistently represented throughout the document.** The subdivider's statement describes "Subdivision of a 3 parcel vacant lot totaling 5.01 acres" The parcel profile report lists a Lot/Parcel area (calculated) of 86,915.1 (sq ft) -- this equals 1.995 acres. The Master Land Use Permit Application describes a 217,312 sq ft.(4.988 acres) "undeveloped lot" . The CEQA Initial Study and Checklist Environmental Setting summary includes this project description: "...the Project site includes three contiguous lots totaling approximately 212,750 square feet..." Table IV-10 Project Consistency with the 2008 RCP, under consistency discussion states "The project is located on a developed site surrounded by a dense urban environment in the City." Page IV-98 of the IV. Environmental Impact Analysis describes Existing Project Site Conditions as "currently not developed with any structures."

Conflicts with Zoning Code

The proposed project requests a zone change to allow for Small Lot Development. Small Lot Subdivisions are permitted only in areas zoned for multi-family housing or commercial uses. The City's Small Lot Guidelines state the **Small Lot Ordinance** "was envisioned to allow the subdivision of underutilized land in multi-family and commercial areas for the creation of up to 15 lots with detached single family homes. **It was not intended to generate a request for a General Plan amendment and zone change to permit the development.**"

The project also requests a Zoning Administrator's Determination to allow retaining walls outside of the limits set by the Northeast Hillside Ordinance. **The need for retaining walls in excess of the allowable length and height is self-imposed by the sheer scale of the project and not in keeping with the existing Zoning Code and qualified conditions.**

The significance of the project's impacts on and conflicts with the existing zoning code is not adequately addressed in the MND and requires further consideration.

The project will not enhance the built environment in the surrounding neighborhood or perform a function or provide a service that is essential or beneficial to the community, city, or region. The applicant wishes to build 42 homes on the hillside lot, which would require a Zoning Administrator's Determination (ZAD), request to allow the construction of 23 retaining walls varying in height from 3.5 feet to 7.5 feet, in lieu of the maximum of two 10-foot high retaining walls required. Additionally, the applicant proposes grading of more than 1,000 cubic yards, and export of more than 750 cubic yards. The amount of export is 28,500 cubic yards and the amount of grading is not readily discernable based on the documents published for this review period.

As outlined above, this project would be out of scale with the development on the surrounding properties. The requested ZAD for the additional retaining walls would not be necessary for a smaller project that is more in scale with the surrounding land uses. Overall, the project will not enhance the built environment in the surrounding neighborhood or perform a function or provide a service that is essential to the community.

The applicant made no compelling argument for why he should be permitted extensive retaining walls in lieu of the amount required. **Site characteristics do not make strict adherence to the zoning regulations impractical or infeasible for a project appropriately sized for the project area.**

The requested zone change and the proposed project is not consistent with public necessity, convenience, general welfare and good zoning practice

13. NOISE LEVELS

Noise levels are not necessarily single event, or "peak" levels, but an average of acoustic energy over periods of time such as 8 hours or 24 hours, and over long periods of time such as years.

The EPA notes noise levels of 45 decibels are associated with indoor residential areas, hospitals and schools. Per the project documents, noise levels of up to 77.6dBA are projected at Farmdale Elementary School and up to 81.6 at adjacent residences. Sustained over a period of two years, these increased levels are indeed significant.

Mitigation measures proposed are not sufficient. Specifically:

12-2: two weeks' notice of the construction schedule is an inadequate amount of time for local schools to notify families that pedestrian routes will be impacted, provide for resources to guide children past the construction site, attempt to prepare for and mitigate the distractions that the construction noise and activity will cause in classrooms.

12-3 and 12-5: the hillsides surrounding the project site function as a natural amphitheater, amplifying and distributing sound throughout the area. **Construction noise on the hilltop and hillsides will impact homes in the surrounding hillsides. The stated mitigated measures do not address this impact.**

The applicant acknowledges the significant noise impacts brought on by the proposed project. Data provided is not sufficient in confirming that the mitigation measures proposed will reduce these impacts, repeated over a period of two years, to a level of insignificance. Additionally, impacts to homes in the surrounding hillsides is not addressed. **Not enough information is provided to substantiate claims.**

13. POPULATION AND HOUSING

Inducing Substantial Population Growth in an Area, Either Directly or Indirectly

Specific restrictions are placed on development in Hillside Zones throughout Los Angeles and in the Northeast Los Angeles Hillsides. **The Small Lot Ordinance employed for the proposed project was not developed for use in the Los Angeles Hillsides Zones, which are typically zoned R1.** As stated in the Small Lot Guidelines, **the Ordinance was not intended to generate zone changes** or general plan amendments. The proposed project, entitlements including a zone change, retaining walls exceeding the limits set by the Hillsides Ordinance, and the self-imposed excessive grading which is not entirely remedial, **presents a precedent for the use of Small Lot Development in hillside zones** and would quickly encourage other developments of this type in hillside areas. Small Lot Development is a planning tool successfully used in commercial and multifamily zones where existing infrastructure, access to commercial amenities and business areas supports its density. **The proposed project makes improper use of an important planning tool and would only encourage others to follow suit. This potential city-wide impact is not at all addressed in the MND.**

14. PUBLIC SERVICES

Fire Protection:

The applicant outlines measures the project would be required to incorporate but **does not demonstrate how those measures will be incorporated.**

Schools and Parks

Estimated project student generation is based on calculation rates from 2008. The Los Angeles area population growth rate has increased significantly since that time, necessarily affecting the calculation rate. Three-bedroom homes situated within two blocks of elementary and middle schools and less than ten blocks from a high school, with active charter schools also in the community, have a higher potential to attract buyers with children.

Similarly, the location of the proposed homes directly across the street from a recreation center and near to hillside open space has a higher potential to attract buyers intending to make use of these amenities. Therefore, the project has the potential to impact those parklands across the street and within .5 miles much more heavily than those in other parts of the community.

The calculation of impacts on school population and on area parks cannot be accurately represented in the data presented.

16. TRANSPORTATION AND TRAFFIC

Project documents related to traffic impacts were prepared in 2012 for a different project, with different uses and different entrance/exit points and cannot accurately represent the impacts of this proposed project.

The potential for entrance and exit points for this project are limited. Those proposed are awkwardly located and present several safety concerns including:

- potential for auto queuing on a four-lane secondary highway
- the expectation that drivers cross two lanes of oncoming traffic approaching from around a bend approximately 200 feet away in order to access the Eastern Avenue entrance of the development from the north. **This change in existing traffic pattern poses substantial safety risks** on a wide street where speeding is prevalent and where curves prevent drivers from seeing oncoming traffic.
- concerns about existing speeding traffic on the curves of Eastern Avenue, where **fatal accidents have occurred in the past two years.**
- the expectation that drivers exiting the Lombardy Blvd side, intending to access Eastern Avenue, cross oncoming traffic to enter a center left turn lane just 60 feet from where west bound Lombardy traffic merges into the same lane to turn left onto Eastern Avenue. Should drivers exiting this area intend to turn right onto Eastern Avenue, further safety issues arise. **This intersection currently routinely sees auto queuing during moderate and peak commute hours.** The presence of a painted traffic triangle and a three-way stop at Lombardy, Phelps and Budau further complicates the issue. Safety concerns at this location are not limited to drivers. **During peak commute hours both sides of Lombardy and Eastern Avenue see high pedestrian traffic as area school children walk to and from the two large schools within two blocks of the intersection.**
- To add to the potential risks to safety, construction vehicles and large trucks hauling soil will be added to this mix for 12 months or more.

The proposed project entrance/exit points present more problems than solutions, and with increasing density in and around the project area over time, the problems will only increase. **The applicant's assessment of the traffic impacts and associated safety impacts is not sufficient to demonstrate minimal or adequately mitigated impact. Satisfactory mitigation of the traffic issues associated with the proposed scale of development is infeasible given the location and contours of the site.**

The proposed **Haul Route is not clearly stated or identified in the project documents.** Therefore, the potential impacts of the route cannot be determined.

18. MANDATORY FINDINGS OF SIGNIFICANCE

For the above stated reasons, **the proposed project has the potential to degrade the quality of the environment, and substantially reduce the habitat of the protected tree *Juglans californica*.** The project documents state that **this numbers of this species will be reduced.**

Additionally, in light of the project as a whole including any mitigation measures imposed, the **project's location and size will not be compatible with and could adversely affect or further degrade adjacent properties, and the surrounding neighborhood.**

The project's many individual impacts as outlined above are considerable and considered as a whole represent substantial risk to the community's safety, to its character and to its environment. **The MND presented does not adequately demonstrate mitigation measures that would reduce these impacts to a level of insignificance.**

It is clear that the proposed project is simply not suited to the subject site and by its own design encounters hardships which are self-imposed and, therefore, do not merit the permissions requested.

Finally, the verified and documented unpermitted removal of 13 mature protected trees from the subject site while under the joint care of both parties is a grave concern. It is an understatement to say that the proposal to remove an additional 68 mature protected trees from the site adds insult to injury. In light of these conscious actions, mitigation is insufficient and, instead, the city's protective ordinance 177404 must be invoked to withhold building permits for the subject site.

In total the collective environmental effects of this project are cumulatively substantial when viewed in the context of past, current and reasonably anticipated future projects.

We have found nothing in Clearwater's record of evidence that shows that compared to its natural state the new development will advance the interest of the community safety and welfare.

Sincerely, and with deepest thanks for your consideration of these points,



Melissa Kellogg and Demetria Contreras

5015 Ladd Avenue
Los Angeles, CA 90032
Ph: 323.222.6467

cc: Councilman Jose Huizar, CD 14
Darlene Navarrete, City Planning Department
Harold Arrivilaga, East Los Angeles Area Planning Commission



Greg Shoop <greg.shoop@lacity.org>

Comments on ENV-2015-1918- MND Eastern/Lombardy - Vista - "El Sereno Project" and CEQA/Mitigated Negative Declaration)

1 message

Tom Williams <ctwilliams2012@yahoo.com>

Wed, May 31, 2017 at 2:48 PM

Reply-To: Tom Williams <ctwilliams2012@yahoo.com>

To: Greg Shoop <greg.shoop@lacity.org>

Cc: Miguel Vargas <mvargas41035@gmail.com>, ANTHONY MANZANO <rosehillsreview@yahoo.com>, Melissa Kellogg <emailmkellogg@gmail.com>, Nichole Sanchez <nichole.sanchez@lacity.org>, "blake.lamb@lacity.org" <blake.lamb@lacity.org>, Jose Huizar <councilmemberhuizar@lacity.org>, Shawn Kuk <shawn.kuk@lacity.org>, Kevin Oculillo <kevin.ocubillo@lacity.org>, Zenay Loera <zenay.loera@lacity.org>

DATE: May 31, 2017

TO: Gregg Shoop
City of Los Angeles, City Planning Department
greg.shoop@lacity.org ph: 213.978.1243

CC:

FROM: Dr. Tom Williams, Senior Technical Advisor
Citizens Coalition for A Safe Community 4117 Barrett Rd, LA, CA 90032-1712
323-528-9682 ctwilliams2012@yahoo.com

Subject: Eastern/Lombardy - Vista - "El Sereno Project" and CEQA/Mitigated Negative Declaration (MND)
ENV-2015-1918- MND ENV-2015-1918-Appendices;
Project: NG-17-054-PL VTT-73531, APCE-2015-2048-ZC-ZAD

RE: Comments on Mitigated Negative Declaration and CEQA Considerations
April and March 2017 MND/Initial Study (IS)
Inadequate/Incomplete and Request for Recirculation as EIR

SEE ATTACHED FILE

After lengthy review of the various documents related and referenced in the two circulated MNDs, I request that the Department of City Planning withdraw all current documents, review comments, revise/quantify, and prepare a complete and adequate Environmental Impact Report with a full range of residential alternatives and draft mitigation monitoring and report program. This request is based on the many inadequacies and incompleteness of the MND. Continuation with the current documents will lead to prolonged reviews and further actions.

I have reviewed and requested replacement of earlier MNDs all of which are now invalid and current MND which is incomplete and inadequate based on summary remarks below.

Current documents are totally unsuitable for public review and meaningful comments. All documents are without State Clearinghouse enumeration and apparently have not been official submitted to State of California.

Three different titles have been used for this project along with different project description (e.g., previous and current). Major variations have been mentioned but have not been illustrated, documented, quantified, and reviewed/assessed.

The MND includes "measures" of draft plans and future submittals which are not available for review and comment in the MND and must not be adopted because proposed measures are not valid MMs and cannot be reviewed as presented or in the future. Therefore the significance of their mitigative effects is not open to review/comment, is unquantified, and is inadequate, incomplete in substance.

For a Small Lot Subdivision project, the Project Description is inadequate and incomplete, and a single add-on Figure II-2 supplement does not delineate each structure/lot covering the entire site and indicates large "open areas" without assignment.

Similarly, a change from earlier project descriptions refers to "54 retaining walls" without specific well demarcated locations and without grading plans and cross-sections/profiles. Such absences renders the current CEQA document totally useless, inadequate, and incomplete.

Given various SLS issues above/below, the project as shown requires both an operating and a maintenance association for all facilities and services, including security and fire suppression for the major open space and un-demarcated areas in the central and westerly "open area".

Noise, vibration, light, and visual scale of the project has been transferred from the westerly Eastern Av. corridor to the southerly and easterly boundaries and the existing Mallory and Harmony residents who will be significantly and massively impacted by/during grading/hauling, concrete retaining wall construction, and then unit construction and then again from the occupied units overlooking and affecting their backyard uses forever. The benefits for Eastern Av. and ES-Recreation Center are bought by the severe penalties for the 20+ residents along the easterly and southerly boundaries.

CEQA documents provided and their supporting appendices were Adobe/pdf files which were largely secured and contents could not be readily searched as key words and could not be copied by the public for more readily review and commenting, unlike many other CEQA reports from the Department of City Planning.

The Project Description is incomplete and inadequate and useable for review and comment with **Absences** of:

- Site finished grading plan and no site profiles/sections - N-S/E-W;

- Construction, staging, loading, and/or entry/exit plans for material supply, hauling out/removal, and site storage;

- Haul Plan/Controls - Two-mile hauling route for 7800 round-trips to/from site to I-710, entry/exit for turning long wheel based vehicles with 6 traffic signals, important right turn into and left turn from site, right turn onto I-710 on-ramp;

- Retaining-wall plan and profiles and comparisons with "previous lines";

- Landscaping and irrigation plans for retained and replacement trees and ground cover;

- Pedestrian/Active transport system on site and on Eastern and Lombardy margins, especially related to public transit and school-safe routes;

- LID-Stormwater collection, storage, and reuse systems for all units and impervious surfaces;

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After days of reviewing and cross-checking I finally give up adding more comments. The MND is incomplete and inadequate, and the MND must be replaced with a full, adequate, and complete EIR with several residential alternatives, including those compliant with existing zoning.\

Thanks for the opportunities for review of this project(s).

Dr. Tom Williams 4117 Barrett Rd, LA-CA, 90032-1712



EastLombardy053117.doc

165K

DATE: May 31, 2017

TO: Gregg Shoop
City of Los Angeles, City Planning Department
greg.shoop@lacity.org ph: 213.978.1243

CC: Nichole Sanchez; nichole.sanchez@lacity.org
Blake Lamb, Senior City Planner; blake.lamb@lacity.org ph: 213.978.1167
Councilmember Jose Huizar; councilmemberhuizar@lacity.org
Shawn Kuk, Planning Director; shawn.kuk@lacity.org
Kevin Ocubillo, Planning Deputy, kevin.ocubillo@lacity.org
Zenay Loera, District Director, zenay.loera@lacity.org
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Ladd Avenue

FROM: Dr. Tom Williams, Senior Technical Advisor
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Thanks for the opportunities for review of this project(s).

Dr. Tom Williams 4117 Barrett Rd, LA-CA, 90032-1712

MORE SPECIFIC COMMENTS:

The Figure II-1 is poorly positioned and titled as "Previous" when the public can easily assume that the first figure for the Project is the "Project" not its ancestor. This is the first indication of a "confusion" approach which may be aimed at frustrating the reviewers and fostering a "give up" attitude. The initial missing II-2 Current Project Site was not provided until 50% through the review period for the previous plan.

pdf 39 p.i Table of Contents Section 5 Preparers **No Section 5 provided, MND/IS is incomplete.**

"SLS" requires allocation of a fair share of the parcel open ground to each plot rather than a larger "in-common parcel"

Road - Private Responsible Party - Cleaning/Painting/Services Access/ReDo's

Traffic congestion - Tripling at Kerwin/Round Top

Haul Route and Balanced Cut/Fill on Site - No Haul Route Undocumented by Geotechnical Report.

p.51 XV. TRANSPORTATION CIRCULATION a. Less than significant impact

The development of proposed single family dwellings be similar to adjacent single family structures in the area and **is not anticipated to result in an increase in traffic**, therefore **should have** a less than significant impact.

Single page of Mitigation Negative Declaration, pdf-8

No ToC for Appendices

No visitor parking indicated any where

Single point of access for 38 units; and for 39-42

No pedestrian Access to west-, north-, & NE-side slopes

p.1 4) 54 retaining walls vs two; **no plan and profiles provided** p.1 5) Haul route, **None provided.**

p.1 "Finding: ...mitigation measure(s) outlined on the attached page(s) will reduce any potential significant adverse effects to a level of insignificance. (Reference to page 2)

No p.2. No headings or explanation for p.3-8 are provided. Pg.3-8 appear to be "mitigation measures" either unique to this project or generally for any City project construction.

[same repeated on pdf.8, March 2017]

p.3 1. Aesthetics - Trees

1-1 Non-protected tree needs a Plot Plan for all trees; draft of existing and landscaping plan(s) is(are) required.

1-2 A "Tree Report" is required; the report must be included as part of

4:1 replacement ratio

"required landscape plan"

1-3 4 planted for 1 removed protected trees. No penalty for illegal removal of protected trees; 1-2/3 same 4:1 replacement for permitted removals

1-4 Tree Preservation Report (Apdx. A, Tree Survey, TPR Apdx. E small scale and F large scale)

No plot plan provided; No construction area shown on Apdx. A / Apdx.E small scale, maybe at larger scale Apdx.F; "on the line shown on the map" – No line and no map.

p.4\1-4/5,6,7 "Shall" > "Should" **Provide only "shall's" or "must's"**

p.4 3. Air Quality **3-1, 3-2, & 3-3 need online realtime logging of all equipment and daily verified.**

p.5/3-4 **"Encourage" and SOON measures are not enforceable.**

Change in format, "Encourage" and lack of definitive statement "shall" Provide only "shall's" only Rule 403 Measures are not documented even in a draft report/compliance manual and must be monitorable via online/realtime web page for monitoring and reporting of mitigation measures.

p.5 4. Biological Resources **4-1 "A" option is unenforceable without monitoring and reporting. Eliminate**

...or...4-1 "B" option includes "should(s)" and therefore not enforceable. Replace all shoulds with shall and include online/realtime and video camera web-based coverage of all conditions for monitoring/reporting by qualified biologist.

p.5\3-5/7 Change in format. **Provide only "shall's"**

p.6\4-1/2 **Add "neighborhood councils" to City and County**

p.6 12. Noise p.6\12-1/ "Readily visible" **is undefined and unquantified**

12-1 All project info must be posted and updated on site and provided online-realtime through a project webpage.

12-2/ Residences and school notification, ADD "Recreation Center" within 500ft.

12-2/ "Construction Schedule" Provide current draft

- 12-2 **Provide and regularly update online/realtime project schedules and activities related to noise and air emissions through a project webpage.**
- 12-3/ "Feasible" **Define and quantify**
- 12-3 **Noise controls and barriers must include noise and vibration monitoring for specific equipment and activities and include criteria, monitoring, and reporting for instantaneous, 1-, 10-, and 60-minute intervals online/realtime through a project webpage.**
- 12-4/ "Properly" **Define and quantify**
- 12-4 **Noise control must include noise and vibration monitoring along southern and eastern boundaries and include criteria, monitoring, and reporting for instantaneous, 1-, 10-, and 60-minute intervals on line/realtime through a project webpage.**
- 12-5 "As far as possible" **Define and quantify**
- 12-5 **"as far as possible" which would be south of the Eastern/Lombardy intersection; revise and develop specific plan for parking, waiting/idling, and staging.**
- 12-6 "Provided where feasible" **Define and quantify**
- 12-6 **Remove and specify "feasible"; provide criteria for instantaneous, 1-, 10-, and 60-minute intervals and enclosures to reduce noise emissions below the criteria.**
- 12-7 **Grading, loading/unloading, and transport must be documented in a "Haul Route" Plan. Specific conditions must be provided Left turn out-bound from the site to south-bound Eastern; similarly north-bound Eastern truck traffic must be provided sufficient clearance to make the right-turn into the site.**
- Eliminate "should minimize" and replace with the plan/report and described specifically as to 4000ft of residential arterial impacts.**
- 12-4 Noise control **must include noise and vibration monitoring along southern and eastern boundaries and include criteria, monitoring, and reporting for instantaneous, 1-, 10-, and 60-minute intervals on line/realtime through a project webpage.**
- 12-5 **"as far as possible" which would be south of the Eastern/Lombardy intersection; revise and develop specific plan for parking, waiting/idling, and staging.**
- 12-6 **Remove and specify "feasible"; provide criteria for instantaneous, 1-, 10-, and 60-minute intervals and enclosures to reduce noise emissions below the criteria.**
- p.6 16. Transportation **No travel, delivery, or haul route, provide draft traffic/travel/haul routes and access to/from site and Construction Staging, Storage, and Workers Parking Plan**
- 16-1 "Construction Staging and Parking Plan", "Prior to the issuance of...permit" **Provide current draft, NOW**
- p.6-7 16-1 Schedule off peak M-F only 9am-2pm (school/student travel hours)
- 16-2 "...Schools" **Add ES Recreation Center and ES-Senior Center to schools and provide draft plan**
- 16-3 **Provide LACSD and ESRC/LAR&P assigned haul hours based on school operations.**
- p.8 16-3 **Define and quantify "sequenced..." "minimize"**
- p.8\16-3/2 "to minimize conflicts" and "especially" **Define and quantify**
- 16-3 "Schools...Haul Route" **Be specific only one or more routes, prohibit ALL construction related traffic other than to Eastern, south of Lombardy, and none on Klamath/K.Place, Swigett and Lombardy**
- p.8 16-4 "should be parked on site...streets" when blocking...flagger
street closures not during peak traffic hours construction noise kept to a minimum...
storage in public rights-of-way construction hours
- Provide plan, manual, schedule, and maps**
- 16-5 **Staging along Eastern and Lombardy must be provided in a plan/report and placed on/updated online realtime web page.**
- p.8 Reference to "SEE ATTACHED SHEET(S) FOR ANY MITIGATION MEASURE IMPOSED."
No sheet(s) was identified and/or provided.

Initial Study pdf-9

- IS I-1/3-4-5 **Textual comparisons of differences between Current and Previous plans do not provide any graphic comparisons.**
- I-3 "...a detailed description of...setting and the Project...**Provide details, demarcations, and quantifications.**
- 1- "Finding...mitigation measure(s) outlined on the following pages...to a level of insignificance." **No demonstration of mitigative values as shown in comments. For example: I-1/4 Increased haul trips 7800 ROTs for II-1/3 78,000 cu yd hauling**

Fig. II-2, Provided after original MND circulation. Major Plots associated with open space plots #1 S&W, 20 >N>E, 21 >N, 22 >N, 23 >N, 24 >N>W, 25 >W--29 >W, 39 >+1W, 42 >+2E&S, Plots without open space for others 32-38, 30/31 > 29

Plots with lines that may be Retaining Walls: 1 S&W, 2-11 S, 12 S&E, 13-19 E, 20-24/25-29/30-31 Bldg2Bldg, 32-36, 39-42:14 plots

2-1/1 Project site is hilly, **site is a single HILL**

2-1/2 **Surrounding Recreation Center is not a land use but a service; Farmdale School is not a land use.**

2-1/3 one home per plot, **NOT SO common lands right of passage - legal trespass**

2-1/3 Cistern system, **Mentioned but inadequate - nothing provided as to collection, storage, or reuse.**

2-3 Views only show northernmost 1/4 of site **Inadequate.**

II(=2)-27/1 **Common facilities, grounds and access require formation of Homeowners Association/Special Assessment District for funding, bonding, operations, and maintenance/replacement for "private" security, infrastructure, lighting, water-supply, sewerage, and fuel modifications/clearing.**

2-19 "Park" **is a recreational center**

IV(=4)-2/1 with...mitigation measures...not result in any significant impacts related to trees..." **Without a full landscaping and irrigation plan and grading and staging plans impacts cannot be adequately or completely assessed.**

4-2/2 "similar to those adjacent to the Project site and within the area..." **Comparisons of project and surrounding structures are totally incomplete and inadequate without any visual much less quantified comparisons:** size height mass, accommodate topography, conform to visual character of sum, no....impacts...to visual character should occur

4-3/1 ...impacts related to light and glare would be less than significant. **Does not reflect impacts to the Mallory/Harmony residents**

4-3/2 - 4-5/1 mitigation related to trees. **Mitigative value depends on conditional requirements without any meaningful enforcements.**

4-5/2 Cumulative

Aesthetics - Crown-Roof height and Obstruction of existing surrounding/adjacent viewsheds

Aesthetic - Visual - Viewsheds of Project from surrounding residences and public roads

Outdoor lighting/reflective surfaces Shielding - light source cannot be seen from adjacent residential properties. Building Orientation and Existing Views Construction Noise - Peak/Short-term vs 10-60minute averages Gratings

Aesthetics impacts of "previous project" have been traded from those to the general public, recreational center users, and travelers along Eastern to those of the 20+ and other easterly, hillside residents to the east and south (Mallory and Harmony and beyond). No information has been provided and the entire Aesthetics section is inadequate and incomplete with regard to the trade offs made by the proponents.

4-9/6 - 13/1 2012 AQMP and 13/2 2016 SCAG RTP

4-13/3 Project **After many pages of general air quality issues (suitable for appendices) for confusion factors, the MND returns to the site impacts without adequate and complete discussion of grading/construction mitigation for the southerly and easterly existing residents. Similarly no discussion is provided for the air emission of the 7800 empty/load haul truck trips along Eastern Av. and their frequency and duration (e.g., 10 trucks/20 truck trips per day for 780 days or 20/40 for 390 days or for 78 weeks OR 90 calendar days - 13 weeks x 5 =65 haul days with 120 truck ROTs).**

4-18/2 Biol.Res. 4/21 T4-5 **include Toyon which has been nominated for protection by Department of City Planning without discussion herein. Need re-survey for locations and for new protection and for Elderberry #s and sites.**

Similarly, MND does not discuss current and Project compliance with Brush Fire Prevention/Fuel Modification requirements: 4in - 6ft (1/3) clearance - 200ft off/from structures all sites.

36/3 Cultural Resources **does not review and compare 1923, 1928, and other Aerial Photos (EDR) for Historic Structures and Uses of the site and potential historic and archaeological site. Unpermitted ground work on southerly and SEerly exposed slope would be of particular concerns and may have already adversely affected resources other than protected trees**

37/2 Unique paleontological resources "...fossils...found in the sedimentary...Project area and at the Project site\18 in the sedimentary deposits within the site... **Without the retaining wall, grading and filling plans, no impacts can be assessed; prospective mitigation measures are not enforceable and are not monitor-able and therefore cannot be judged as to mitigative values.**

4-39/1 Geology and Soils

No discussions are provided with maps/drawings for Fault Zone and Geological Section; similarly, no description/review of seismic information - nine measured earthquakes within two miles

4-39/3 Upper Elysian Park fault is reported to be 0.8mi/4200ft from Project Site, although the Appendix p.261 "Seismic Conditions" states 0.2 mi/1100ft South of site with Rich.Mag. of 6.4 (>0.5G).

As the mentioned fault is a "blind fault" without known surface expression both statement could be true but distance may be BELOW the site.

A surface fault is known 5500ft WSW of the site and may be traceable to Soto St. and perhaps further easterly towards the Project.

Although recorded earthquakes are easily available no references have been made to the presence of earthquakes in the area, other than those mentioned above which were not present in a review in May 2017 of available regional sources.

39/4 "recommendations of a Final Geotechnical Report" to be provided at sometime in future *is meaningless and unenforceable.*

This entire section is totally inadequate and incomplete with regard to the currently proposed project and grading and retaining walls along the southerly and easterly boundaries.

40/2 SWPPP/BMP subject to review and approval by the City, Site-specific SWPPP would be prepared including BMP. **Although the construction phase is discussed, the project Description has mentioned the "Cistern system" which represents a common element in the City's LID requirements for operations. However, the current project description does not include any discussion of the stormwater collection, storage, and reuse system and thereby renders the entire hydrology and utilities/services sections and assessment as incomplete and totally inadequate for review.**

41/2 Mention of high expansive potential and preparation of the future "final" geotechnical report **renders the entire hydrology and utilities/services sections and assessment as incomplete and totally inadequate for review.** 4-41-79/2

65/5 **As indicated elsewhere without any grading/construction plan and schedule, a mention of a 26-month duration of construction activities is totally inadequate for assessment when 78,000 cu yd of excess dirt must be exported with 7800 ROT truck trips and no grading or landscape plans are provided.**

4-79/3 8. Hazards - /5 **Review of historic aerial and ground photographs must be included as they were in earlier CEQA documents.**

82/2-3 LAFD and 83/1 wildland fire are mentioned **but without information regarding current and project requirements for fire prevention/fuel modifications and suppression/control as to sprinkling inside/outside, fire hydrants, and pressure requirements for a hillside location. No mention of small lot development issues with regard to fire containment and spread-prevention.**

Hazards assessment is totally inadequate and incomplete without documentation and publicly accessible documents.

84/ 9 Hydrology and Water Quality 84/1 ...would not have any point-source discharges. 84/2 ...older alluvium, and bedrock...

As no water collection, supply, stormwater or wastewater systems are described or delineated within the Project, no references can be made regarding impacts or mitigation.

84/2 All water supplied by MWD **Water supplies are from Department of WATER and Power and from various sources, including MWD.**

85/1 drainage and mention of cistern and LID BMP but nothing else

85/3 "...most of the project site would be developed with impervious surface,...flows would be directed to storm drainage flows and would not come in contact with bare soil surface.

85/3 most...flows...to local streets...enters the City's storm drain system....not to exceed existing rates...would not increase the runoff...to control drainage from the site to not exceed exiting rates.

86/2 LID but for construction not operations 86/4 ...comply with the City's LID Ordinance...

Hydrology of the Project and its assessment is totally inadequate and incomplete without documentation and publicly accessible documents.

87/5 10. Land Use and Planning

No mentions are made as to the Mobility element of the general plan or community plan; no mention is made as to the specific requirements for Small Lot Divisions and how the Project complies with ALL. 88/1-91/6

...Compass Blueprint Growth Vision...SCAG Report and 89/3 SCAG 2008 RCP **are out-dated.**

Land Use Planning assessment is totally inadequate and incomplete without documentation and publicly accessible documents.

- 4-104/table 4-17 El Sereno neighborhood **is not a “neighborhood”, whatever that is but is a LA City recognized “district” within the LA-32 Neighborhood Council area of the Department of Neighborhood Empowerment.**
- 105/1 Small Lot Subdivision Ord. 176354 IV-106-109/1-2 Q-Conditions, and 107 5) d. Homeowners Association **No documentation of full compliance and support for these provisions are documented in these CEQA documents. Mention is made of a Maintenance Association but because of various variations from the SLS requirements a Home Owner Association and bonding must be included.**
- 108 7) c. Total grading 82000 fill 5000 export 78000 **NO documentation, calculations or other materials are available for these numbers and therefore must be considered as inadequate if not totally incomplete.**
- 108 7) d. LID collect runoff to cisterns **Nothing else is provided within the MND or its appendices and renders the document as incomplete.**
- 4-110/1 11. Mineral Resources **This section only considers gravel and sand not oil/gas, although wells are known in El Sereno and East LA.**
- 4-110/4 12. Noise **General discussion without specifics to the Project; MND is incomplete.**
- 4-116/4 Mention of “multi family bldg. surrounding the project” **Without consideration of the 20+ single-family units immediately adjacent to the project, statement renders the MND as inadequate.**
- Without a grading/hauling/staging/construction plan and schedule noise cannot be properly assessed or mitigated. Noise/Vibration and air emission cannot be assessed and mitigated, and is totally inadequate and incomplete.**
- 119/1 400 vt/wk-d 30-40/commuter **Brief comments regarding operational traffic noise impacts do not consider the existing receptors and issues related to the left turn in-/egresses onto Eastern Av. and their impacts on the immediately adjacent residents to the south.**
- 171 14. Public Services and 209 18. Utilities and Services
- Public services are restricted to public access points and all services on the Site must be provided by the project, and operated/maintained/replaced by the owners of the site. As the current Project Plan shows areas without direct inclusion in adjacent unit plots, a basic requirement for SLS project is not fulfill, there are no responsible parties designated although a Maintenance Association is mentioned without description or designation of powers.**
- The entire assessment and mitigation of impacts related to Public Services, Utilities, and Infrastructure must be considered as incomplete and inadequate as no one is designated as responsible and no information is given regarding what services and infrastructure will be provided and supported by whom.**
- APPENDICES are totally confused with regard to documents from the first and subsequent MNDs and must be considered as incomplete and inadequate.**

EXHIBIT F.1

Interested Parties

Support



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Please Approve the Vista El Sereno Project

2 messages

Alex Sanchez <sanchezlx7@gmail.com>
To: may.sirinopwongsagon@lacity.org

Wed, Jan 22

Dear May,

I am writing to you to express my support for the Vista El Sereno project. I am born and raised in El Sereno and am a current resident writing to ask that you please support the Vista El Sereno project is long overdue and that site has been a sore thumb for the community. That location has brought many homeless encampments and illegal activity. It is very difficult for the police to get because it's so big and very easy for the homeless to hide within the hill. Homelessness is a major issue right now in the City of LA, and bringing more housing to the community can help with major issues.

Additionally, the project is a major improvement for El Sereno. The homes are beautifully designed and the layout is going to preserve a large portion of the hillside as well as green space. I am sure the City recognizes the positive impacts of this project and approves it asap.

Best,
Alex Sanchez
El Sereno, CA

May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>
To: Alex Sanchez <sanchezlx7@gmail.com>

Thu, Jan 23, 2020 at 6:46 AM

Hello,

Your comments have been received and included into the administrative record.

May Sirinopwongsagon
(213)978-1372
Department of City Planning
Central Project Planning Bureau
200 N. Spring Street, Room 621
Los Angeles, CA 90012

[Quoted text hidden]



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Support for Vista El Sereno

1 message

Alyssa Crispin <alysscrisp@gmail.com>

Thu, Jan 23, 2020 at 9:48 AM

To: may.sirinopwongsagon@lacity.org

Cc: Vista El Sereno <hello@vistaatelsereno.com>

Hello May,

My name is Alyssa Crispin and I currently live in El Sereno off Huntington dr. I bought my house here 2 years ago after falling in love with the Eastside and the beauty and culture it has to offer.

I see everyday the real potential for our community, local businesses and reputation and when I heard about the Vista El Sereno project I was so excited to see something that would help rejuvenate this community. I live very close to Eastern and Lombardy and I would love to see an empty area turn into a thriving community and help fund all of the local businesses that I currently support. I pledge my full support to this project and their vision to help fund more parks and recreational areas here in El Sereno. I would love to see the people of this community profit from such a wonderful expansion and I feel lucky they have seen what I see in El Sereno.

Please let me know if you need anything further in support of this project.

Thanks so much

Alyssa Crispin, MHA
(760) 900-2332
alysscrisp@gmail.com
4797 Huntington Dr. N
Los Angeles, CA 90032

Sent from my iPad



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Vista El Sereno Project

1 message

Christina Flores <florescrc@gmail.com>

Thu, Jan 23, 2020 at 2:20 PM

To: may.sirinopwongsagon@lacity.org

My name is Christina Flores. I live at 4852 Klamath Street which is directly south of the proposed new construction. My fellow homeowners and I wrote a letter in support of the project last year. I am writing you to assure you of my continued support for this project.

The majority of El Sereno residents will not have direct contact as the homeowners on Klamath Street. There were two fires on the project hill last year. Something needs to be built on the hill in order to safeguard the resident surrounding the hill. I and my neighbors are also aware that this project is not entry level housing; but this will not "destroy the current living environment in this area" as some have said. In all honesty, we would prefer that only 10 houses should be built on that hill, but we also know this is unrealistic. To leave it as it has been for these many years is a fire hazard most especially to those of us who live on Klamath Street.

If you determine that modifications to "the existing permanent Qualified "Q" Conditions contained within Ordinance No. 180,403" are not in order; please allow the Vista El Sereno company to build something on the hill. Despite comments to the contrary, this company has been the most transparent, concerned and considerate about the community and residents surrounding the project and have listened to our concerns in a way no other prospective builder has done in the past. I feel they are deserving of being allowed to build something on that hill.

Thank you for listening.

Sincerely,

Christina R. Flores

[4852 Klamath Street](#)

[Los Angeles](#)



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Vista El Sereno Support Letters

1 message

Abraham Mercado <Abraham.Mercado@consensusinc.com>

Tue, Jan 21, 2020 at 5:45 PM

To: "may.sirinopwongsagon@lacity.org" <may.sirinopwongsagon@lacity.org>

Cc: Nashia Lalani <NLalani@sheppardmullin.com>, Rob Flitton <RFlitton@thetruelifecompanies.com>

Hi May,

Please see attached the supporter letters we have on file. Please include them for the City record.

Best,

Abraham



Abraham Mercado

Vice President

Consensus

d: 213.802.1139

c: 323.684.7731

consensusinc.com

The Tower


[3900 W. Alameda Avenue | Suite 2050](#)

Burbank, CA 91505



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2 attachments

 **whitman_letter_of_support.pdf**
74K

 **ChristinaFloresSupportLetter.pdf**
81K

April 10, 2019

The City of Los Angeles

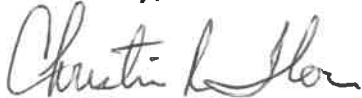
We the undersigned wish to indicate our strong support to allow the change in zoning from multi-family (apartment/condos) dwellings (RD6 and R1) to single family homes (RD5).

As the residents of Klamath Street/Harmony Lane the construction will directly affect us since our homes lie adjacent to the proposed construction site. We believe our opinion should be strongly considered above those more vocal opponents who do not live in the immediate vicinity. Our street has been directly affected by the current state of neglect of the property. There is currently a group of people unlawfully living on the property and there have already been two fires in the last year which could have caused great damage to our homes if not for the prompt efforts of our local firemen.

A change in zoning would allow for single family homes which would be a much better and needed option for the community than multifamily dwellings which would add considerably to the local traffic and congestion.

Thank you for your consideration.

Sincerely,



Print: Christina R. Flores

4852 Klamath Street, Los Angeles, CA 90032



Print: Edna Young

4832 Klamath Street, Los Angeles, CA 90032



Print: Jean Takamiya

4837 Klamath Street, Los Angeles, CA 90032

Steve Molina

Steve Molina 4838 Klamath ST

Print:

Scott Lee

4875 Klamath

Print:

Natasha Ngo

NATASHA NGO

4843 Klamath St.

Print:

Drew Lindstrom

Drew Lindstrom 4846 Klamath St.

Print:

Rodrigo Cardona

Rodrigo Cardona 4831 Klamath St.

Print:

Aaron Cole

Aaron Cole 4818 Klamath St

Print:

MARTHA E. Carrillo

MARTHA E. Carrillo 4824 Klamath ST.

Print:

Print:

Dear Neighborhood Council,

My name is Whitman Lam. My family moved to El Sereno back in the early 1980's, when I was 4 years old. Me, and my 2 younger siblings grew up in this neighborhood and attended the local LAUSD schools. I graduated from Roosevelt High School at the top of my class in 1998.

As a longtime resident of El Sereno, and now a Very Happy Father of a newborn baby daughter (She was born at midnight on Tuesday), I am proud to say that my intention is to raise my little girl in El Sereno, and go to the same schools that I went to. As a Refrigeration contractor and a small business owner, I will work very hard to support my child, and to make sure that she gets everything she needs to grow up into a successful adult.

And this is where I will need some help from YOU, the neighborhood council and my fellow El Sereno residents. We really need to make improvements to the quality of life in El Sereno. And this starts with building MORE HOUSING, making our streets safer, and encouraging businesses to open and grow along our busy streets.

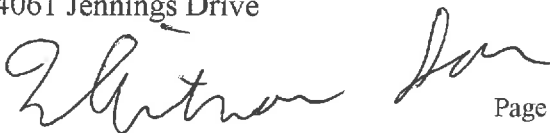
This is why I support the new Vista El Sereno housing development that is being proposed at the corner of Eastern Avenue and Lombardy Street. For far too long, that corner has been a dead zone, a magnet for illegal trash dumping, and a place that pedestrians generally avoid at night. That land should not be left empty, it is an opportunity to build more housing, which our community desperately needs right now. We simply do not have enough construction going on, for brand new For Sale houses and many families are being forced to move out of the neighborhood to far away places Even out of California.

New housing in the area will encourage more business activity, creating more jobs, and more tax revenue for our schools and roads. We still don't have a CVS or Walgreens pharmacy, and there are no Supermarkets for anyone living on the Southern side of El Sereno! This needs to change.

Also, I would like to see more street improvements to Eastern Avenue, especially the area next to the El Sereno Recreation Park. We need to install some protected Bike Lanes on both sides of the street, so that riding a bike or scooter is not like gambling with your life. I would also like to see more crosswalks, with the light up signals, so that people have safe places to cross at night.

I want to thank the El Sereno Neighborhood Council for all the work that you do, and listening to our concerns. I hope that my vision of a better future is not too far out of reality. I've seen many positive things happen in El Sereno for the past 30 years, but there are plenty of things that need to be fixed, and I worry that we are being left out of the loop, compared to more wealthy neighborhoods in Northeast LA.

Sincerely,
Whitman Lam (323) 578 2250
4061 Jennings Drive



03/20/19



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Letter of Support - 2520 N. Eastern Ave

1 message

Dennis Rodriguez <df.rodd@gmail.com>

Thu, Jan 9, 2020 at 7:14 PM

To: may.sirinopwongsagon@lacity.org

Dear Ms. Sirinopwongsagon,

Please find my attached letter of support for the proposed project at [2520 North Eastern Ave](#), in Los Angeles.

As an adjacent property owner, I strongly support the project for the reasons outlined in my letter.

Please let me know if you have any questions.

Thank you,
Dennis Rodriguez

[2722 Phelps Ave](#)
[LA CA 90032](#)



Ltr of Support - 2520 N Eastern Ave.pdf

265K

City of Los Angeles
Department of City Planning
May Sirinopwongsagon, City Planner
200 N. Spring Street, Room 621
Los Angeles, CA 90012

January 9, 2020

Re: 2520 North Eastern Avenue (2532, 2608, 2668 N. Eastern Ave and 2647-2651 N. Lombardy Blvd)
VTT-73531-SL; APCE-2015-2048-ZC-ZAD-ZAA

Dear Ms. Sirinopwongsagon,

As an adjacent homeowner to the proposed project, I am writing this letter in very strong support of the proposed project and support your approval of the requested actions by the development company.

The intersection of N. Eastern Avenue and N. Lombardy Blvd., generally speaking, is an attractive nuisance for unwanted activity. Across the street from a car wash / automotive sales, the sidewalk adjacent to the vacant land on which project will be built is frequently used as a pop-up, illegal food sales location, and for the purpose of sidewalk vending of junk items.

Both activities, which contribute unwanted blight into the neighborhood, will be strongly deterred or cease as a result of 42 homeowners taking residence on the now vacant land.

Additionally, adding these 42 small lot homes will have a net positive impact on the general livability of the community. In addition to removing blight and unwanted activities, the project will add 42 new housing units as well as 42 new property taxpayers to the tax rolls. Both should be considered welcomed additions to the City of Los Angeles by city officials.

For these reasons, I strongly support your approval of the Zone Change, the Q conditions modifications, the ZA Determination, and the ZA Adjustment, towards the overall approval of the project.

Thank you very much for consideration of my letter.

Best,



Dennis Rodriguez

2722 Phelps Ave.
Los Angeles, CA 90032



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

YES on Vista El Sereno

2 messages

Eric Halm <ehalm@bernards.com>

Wed, Jan 22, 2020 at 3:29 PM

To: "may.sirinopwongsagon@lacity.org" <may.sirinopwongsagon@lacity.org>

Cc: "hello@vistaatelsereno.com" <hello@vistaatelsereno.com>

Dear Ms. Sirinopwongsagon:

As a homeowner on Ladd Avenue, I believe that the Vista El Sereno project will positively impact the streetscape and property values in my neighborhood. However, there are a lot of anti-gentrification activists in the area and low-income housing advocates citywide. Therefore, in order to gain their support, I suggest that the developer designate at least 10% of the homes as affordable housing units, or build a multi-unit affordable housing building that can house 10% of the total occupants on the site, and offer these units as discount rentals to local, low-income families.

Very truly yours,

Eric Halm, AIA, LEED AP, Assoc. DBIA

Director, Business Development



An Employee Owned Company

555 First Street | San Fernando, CA 91340

T 818 898 1521 | D 818 336 3590 | C 818 263 5743

ehalm@bernards.com | www.bernards.com

May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Thu, Jan 23, 2020 at 6:47 AM

To: Eric Halm <ehalm@bernards.com>

Cc: "hello@vistaatelsereno.com" <hello@vistaatelsereno.com>

Hello,

Your comments have been received and included into the administrative record.

1/23/2020

City of Los Angeles Mail - YES on Vista El Sereno

May Sirinopwongsagon
(213)978-1372
Department of City Planning
Central Project Planning Bureau
200 N. Spring Street, Room 621
Los Angeles, CA 90012

[Quoted text hidden]



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

I urge the city of LA to Approve the Vista EL Sereno project

2 messages

GABRIEL VALENCIA <gvalenc1a@aol.com>

Tue, Jan 21, 2020 at 5:41 PM

To: may.sirinopwongsagon@lacity.org

To May Sirinopwongsagon, City Planner

My name is Gabriel Valencia and I am writing in support of the Vista El Sereno project. As a lifelong El Sereno native and current resident, I have seen first-hand the vagrancy that occurs at the site of the proposed project. There are homeless encampments, illegal dumping and as it is today, the property is a nuisance for the community. I'm deeply embedded in the community and currently raising my children in El Sereno. This is why I support the project and want to see something positive on that hill.

Adding 42 homes in a time when the City itself has declared a housing crisis should be welcomed! With new homes joining the community, there will be additional taxes infused into the City that can be reinvested into City services such as local parks, police and fire. Small businesses will benefit from the additional customers that will likely shop at their stores. For these reasons and many more, I urge you to support the project!

Best,
Gabriel Valencia
El Sereno, CA

Gabriel Valencia
Mobile (323) 697-4713

May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Thu, Jan 23, 2020 at 6:52 AM

To: GABRIEL VALENCIA <gvalenc1a@aol.com>

Hello,

Your comments have been received and included into the administrative record.

May Sirinopwongsagon
(213)978-1372
Department of City Planning
Central Project Planning Bureau
200 N. Spring Street, Room 621
Los Angeles, CA 90012

[Quoted text hidden]

EXHIBIT F.2

Applicant Outreach



Community Outreach Report

May 4, 2020

Introduction

- Starting January 25, 2019, the Vista El Sereno outreach team conducted a consistent community outreach strategy designed to meaningfully engage project stakeholders in El Sereno. Stakeholders included elected officials and neighborhood groups along with adjacent residents, businesses and public entities. This strategy included a mixture of various digital and in-person engagement tactics with the following goals:
 - Inform stakeholders of proposed plans for the project site
 - Allow stakeholders the opportunity to ask questions and provide critical project feedback
 - Make participation easy by creating clearly-defined and accessible channels for stakeholder participation
 - Build positive, supportive and meaningful relationships with stakeholders

Summary of Stakeholder Support

- 160 supporters to date
 - 45 support cards (Appendix A)
 - 20 support letters (Appendix B)
 - 95 supporters online
- Mailed to 163 adjacent neighbors and businesses
 - Total Doors Knocked: 121
 - Total Number of Business interactions: 54
- Engaged 14 Community Organizations
- Created project information channels
 - Website
 - Hotline
 - Email

Summary of Outreach Activities

- **Community and Business Walks**

- The Vista El Sereno outreach team conducted community and business walks to notify adjacent neighbors of proposed plans for the site and gather feedback. Walk materials were distributed including a project fact sheet, a “Sorry We Missed You” letter and support cards were delivered to neighbors.

- **Walk Dates:**

- March 12, 2020
- January 21, 2020
- October 30, 2019
- September 12 and 13, 2019
- March 14 and 18, 2019

Summary of Outreach Activities

- **Community Walk Area (3 community walks)**
 - The community walk area included 121 addresses



Summary of Outreach Activities

- **Business Walk Area (4 business walks)**
 - Business walks were conducted on the major business corridors of Eastern Ave (from Lombardy Blvd to Huntington Dr) and Huntington Dr (from Paola Ave to Van Horne Ave)
 - The business walk area included approximately 161 addresses



Summary of Outreach Activities

- Community Presentations

- The Vista El Sereno outreach team gave a series of community presentations to various stakeholder groups to provide them with the opportunity to learn about the project history, details and outreach plan. Opportunities for questions and community input were given at the following presentations:

- LA-32 Neighborhood Council, Land Use and Development Committee
 - February 20, 2019
 - March 20, 2019
- LA-32 Neighborhood Council
 - April 3, 2019
 - Supporters in Attendance: 12
- Hillside Village Property Owners Association
 - March 14, 2019
- Coffee with the Principal at Farmdale Elementary School
 - September 20, 2019
- El Sereno Senior Citizen Center
 - December 12 and 19, 2019

Summary of Outreach Activities

- Key Stakeholder Briefings

- The Vista El Sereno team conducted a series of key stakeholder briefings throughout the course of the outreach process to notify key stakeholders of the project, allow for questions, collect feedback and detail planned outreach activities.
 - Office of Jose Huizar, Los Angeles City Council District 14
 - March 9, 2020
 - January 25, 2019
 - El Sereno Business Watch
 - February 12, 2019
 - April 15, 2019
 - Adjacent Neighbors
 - April 7, 2019
 - January 21, 2020

Summary of Outreach Activities

- Key Stakeholder Briefings
 - Continued series of key stakeholder briefings throughout the course of the outreach process to notify key stakeholders of the project, allow for questions, collect feedback and detail planned outreach activities.
 - Shelhamer Realty Group
 - March 4, 2020
 - Los Angeles Police Department
 - March 5, 2020
 - El Sereno Stallions
 - March 5, 2020

Summary of Outreach Activities

- **Additional Key Stakeholder Briefings**
 - In addition to the key stakeholders already briefed, the Vista El Sereno outreach team has reached out to brief the following list of key stakeholders:
 - El Sereno Middle School
 - August 6, 2019
 - El Sereno Recreation Center
 - August 13, 2019
 - Farmdale Elementary School
 - September 10, 2019



Summary of Outreach Activities

- El Sereno Tree Lighting & Winter Jubilee
 - The Vista El Sereno team conducted an interactive pop-up at the El Sereno Tree Lighting and Winter Jubilee on December 14, 2019 to inform attendees about the project and gather support:
 - 100 Supporter Sign-ups
 - 64 supporters were El Sereno residents



Summary of Outreach Activities

- **Community Outreach Channels**

- The following communications infrastructure was developed to allow project information and questions to be easily accessible to the public:

- Project website

- vistaatelsereno.com
- Established March 2019

- Email

- hello@vistaatelsereno.com
- Began supporter email campaign March 20, 2019

- Community Information Hotline:

- 323.850.1311
- Established March 2019



Supporter Summary

List of Supporters

Aaron Cole	Christina Berrios	Estela Nunez
Adalberto Castillo	Christina Flores	Francisca Gines
Adela Gonzales	Christopher Cox	Gabriel Valencia
Alex Sanchez	Claudia Escobar	Gabriel, Sr. Valencia
Alicia Vimcent	Claudia Puga	Glenn Shelhamer
Alvaro Herrera	Cuichang Liang	Grace Ramirz
Alyssa Crispin	Daniel Faina	Guadalupe Vega
Amada Alvarez	David Myers	Guillermina Larios
Amelia De Santiago	David Rendon	Hair Creations
AmiJo Moreno	Diana Velez	Happy Hill Properties
Ana Barragan	Dianne Garcia	Isabel Ramirez
Ana Martinez	Digna Bautista	Jackie Borrego
Ana Tejada	Dm Ruiz	Jazmine Gonzalez
Andrea Ramirez	Dora Lopez	Jennifer Arcadia
Anna Gonzales	Dorothy Completo	Jessica Manzo
Annette Sanchez	Drew Lindstrom	Jessie Pence
Araceli Delgado	Edith Donato	Joaquin Petra
Ashley Buttanda	Edith Reyna	Jocelyn Bocanegra
Atricia Lopez	Edna Young	Jonne Sanchez
Audey Duran	El Chalateco	Jorge Quevedo
Aura Morales	El Taquito Restaurant	Jose Ramirez
Ben Lara	Elena Valencia	Juan Ceron
Bianca Macias	Elia Leyva	Juan E Ceron
Blanca Deyeth	Elizabeth Rosales	Juan Guzman
Blanca Escareno	Emma Ibarra	Juan Rivas
Brian Larsen	Enrique Mendez	Karen Jones
Castillo's Herbs and Vitamins	Eric Halm	Kathleen Guerra

Supporter Summary

List of Supporters

Kimberly Garcia	Matthew Bucaro	Tania Martinez
Kristina Gutierrez	Mayra Maldonado	Teresa Gutierrez
Las Flores Market	Miguel Moreno	The Village Mart & Deli
Leah Castillo	Mirna Benitez	Toly Velasco
Levon Ghukasyan	Monique Valencia	Tom Margarita
Linda and Armando Mejia	Naomi Rodriguez	Tommy Milan
Lisa Ostrander	Natasha Ngo	Tracy Mckinley
Lydia Cadena	Ofelia Flore	Vanessa De Leon
Maira Gonzalez	Pedro Cardona	Vanessa Soto
Manny Hernandez	Pedro Ornelas	Veronica Navarro
Marek Slipski	Priscilla Herrera	Veronica Sandoval
Margarita Nicolas	Priscilla Soto	Victory Outreach
Margarita Soorzano	Ramiro Nieves	Whitman Lam
Maria Amaya	Raquel Garcia	William Morrison
Maria Elena Ibarra	Regina Todd	William Preciado
Maria Figueroa	Rosa Escobar	Wow! Store
Maria Garcia	Rosa Ledesma	Xochitl Valencia
Maria Garcia	Rosanna Alvarado	Yanira Cazun
Maria Garcia	Ruben Soriano	Yolanda Ayala
Maria Luisa Martinez	Ruth Manzo	Yunqiu Chen
Maria Mendez	Ryan Hass	Yvonne Henderson
Maria Quezada	Saira Fuentes	
Maria Rodriguez	Scott Lee	
Marixa Velasquez	Silvia Santos	
Marta Taylor	Sofia Figueroa	
Martha Carrillo	Sonia Guerra	
Martin Mendoza	Steve Molina	

Public Information Materials

- **Fact Sheet**
 - A bilingual project fact sheet was produced and handed out at community presentations, key stakeholder briefings, community walks and business walks.
- **Walk Notice**
 - A bilingual walk notice was delivered to homes and businesses to notify them of the upcoming community and business walks.
- **Sorry We Missed You**
 - A bilingual “Sorry We Missed You” letter was left at the doors of homes and businesses that were not present or available when the outreach team conducted community walks and business walks.

Copies can be found in Appendix C.

Appendix A



Yes! I support Vista El Sereno

Si! Yo apoyo Vista El Sereno

I will voice my support by (Expresaré mi apoyo por):

- ☐ Attending a public hearing (Atender en una reunión pública)
- ☐ Speaking at a public hearing (Hablar en una reunión pública)
- ☐ Emailing my support to elected officials (Enviar un correo electrónico de apoyo a funcionarios electos)
- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Teresa Gutierrez

Phone (Teléfono):

(323) 947-1598

Email (Correo electrónico):

tgutierrez64@gmail

Address (Domicilio):

4977 Huntington Dr.

City (Ciudad):

Los Angeles

State (Estado):

Ca

Zip (Código postal):

90032

Comments (Comentarios):

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Maria R Castillo

Phone (Teléfono):

323 6970138

Email (Correo electrónico):

Mariacastillo@att.net

Address (Domicilio):

4441 Lynnfield St

City (Ciudad):

LA

State (Estado):

CA

Zip (Código postal):

90032

Comments (Comentarios):

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Claudia Puga.

Phone (Teléfono):

(213) 519-7270

Email (Correo electrónico):

Cpuga33.Cp@gmail

Address (Domicilio):

3314 Thelma Ave.

City (Ciudad):

LA

State (Estado):

CA

Zip (Código postal):

90032

Comments (Comentarios):

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Edith Reyna

Phone (Teléfono):

323-252-2445

Email (Correo electrónico):

edithreyna88@gmail

Address (Domicilio):

4601 Edelle Pl.

City (Ciudad):

LA

State (Estado):

CA

Zip (Código postal):

90032

Comments (Comentarios):

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Jorge A @uevedo

Phone (Teléfono):

323.. 709-6031

Email (Correo electrónico):

j@uevedos83@gmail.com

Address (Domicilio):

2631 Lombardy Blv

City (Ciudad):

2, A

State (Estado):

CA

Zip (Código postal):

90032

Comments (Comentarios):

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Martin Mendoza

Phone (Teléfono):

323) 742-7708

Email (Correo electrónico):

mrdoza campos
45@gmail.com

Address (Domicilio):

3314 N. Easter Ave

City (Ciudad):

Los Angeles

State (Estado):

CA

Zip (Código postal):

90032

Comments (Comentarios):

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Phone (Teléfono):

Email (Correo electrónico):

Address (Domicilio):

City (Ciudad):

State (Estado):

Zip (Código postal):

Comments (Comentarios):

Show your support online at

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VISTA
EL SERENO



Yes! I support Vista El Sereno

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- ☐ Emailing my support to elected officials (Enviar un correo electrónico de apoyo a funcionarios electos)
- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Jose Lopez

Phone (Teléfono):

323-321-4155

Email (Correo electrónico):

Address (Domicilio):

10856 STAN FORD

City (Ciudad):

LYNWOOD

State (Estado):

CA

Zip (Código postal):

90262

Comments (Comentarios):

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- ☐ Attending a public hearing (Atender en una reunión pública)
- ☐ Speaking at a public hearing (Hablar en una reunión pública)
- ☒ Emailing my support to elected officials (Enviar un correo electrónico de apoyo a funcionarios electos)
- ☒ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Guillermina C. Larios

Phone (Teléfono):

323 225 4505

Email (Correo electrónico): *lgarcia@msn.com*

Address (Domicilio):

2607 Lombardy Blvd

City (Ciudad):

LA

State (Estado):

CA

Zip (Código postal):

90032

Comments (Comentarios):

*Es muy de acuerdo y
apoyo que finquemos.*

Show your support online at

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Yes! I support Vista El Sereno

Si! Yo apoyo Vista El Sereno

I will voice my support by (Expresaré mi apoyo por):

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- ☐ Speaking at a public hearing (Hablar en una reunión pública)
- ☐ Emailing my support to elected officials (Enviar un correo electrónico de apoyo a funcionarios electos)
- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Myke Cordero@hotmail-com

Phone (Teléfono):

323 475 8150

Email (Correo electrónico):

myke.cordero@

Address (Domicilio):

3333 1/2 N Eastern ave.

City (Ciudad):

Los Angeles

State (Estado):

CA

Zip (Código postal):

90032

Comments (Comentarios):

Se incrementara la economia para los pequeños negocios.

Show your support online at

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Yes! I support Vista El Sereno

Si! Yo apoyo Vista El Sereno

I will voice my support by (Expresaré mi apoyo por):

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- ☐ Speaking at a public hearing (Hablar en una reunión pública)
- ☐ Emailing my support to elected officials (Enviar un correo electrónico de apoyo a funcionarios electos)
- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Sofia Figueron

Phone (Teléfono):

(323) 441-9509

Email (Correo electrónico):

Address (Domicilio):

3113 N Eastern ave.

City (Ciudad):

LA CA.

State (Estado):

Zip (Código postal):

90032

Comments (Comentarios):

*Estoy de de acuerdo
con el Proyecto.*

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Yes! I support Vista El Sereno

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- ☐ Speaking at a public hearing (Hablar en una reunión pública)
- ☐ Emailing my support to elected officials (Enviar un correo electrónico de apoyo a funcionarios electos)
- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre): **WILLIAM MORRISON**

Phone (Teléfono): **323-907-8800** Email (Correo electrónico):

Address (Domicilio): **2728 CINCINNATI ST**

City (Ciudad): **LOS ANGELES** State (Estado): **CA** Zip (Código postal): **90033**

Comments (Comentarios):

**I AM A CANDIDATE FOR LA CITY
COUNCIL DIST 14 CALL ME**

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- ☒ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Amelia de Santiago

Phone (Teléfono):

(323) 695 7847

Email (Correo electrónico):

Address (Domicilio):

118 Workman St.

City (Ciudad):

LA

State (Estado):

CA

Zip (Código postal):

90031

Comments (Comentarios):

*Esperando contar con
ustedes en caso de calificar*

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

SU Yi

Phone (Teléfono):

323 316-5169

Email (Correo electrónico):

Address (Domicilio):

225N AVE 25 APT 304

City (Ciudad):

LA

State (Estado):

9003

Zip (Código postal):

Comments (Comentarios):

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Emelda mara

Phone (Teléfono):

(313) 6814298

Email (Correo electrónico):

Address (Domicilio):

422 W AU 38 # D LA CA 90065

City (Ciudad):

LA CA

State (Estado):

CA

Zip (Código postal):

90065

Comments (Comentarios):

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Leah Castillo

Phone (Teléfono):

(323) 424-1509

Email (Correo electrónico):

leahc321@gmail

Address (Domicilio):

4136 Abner St. UA CA 90032

City (Ciudad):

UA

State (Estado):

CA

Zip (Código postal):

90032

Comments (Comentarios):

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- ☐ Emailing my support to elected officials (Enviar un correo electrónico de apoyo a funcionarios electos)
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Name (Nombre):

Pasla Ramirez

Phone (Teléfono):

323-5069140

Email (Correo electrónico):

Address (Domicilio):

2217 Alhambra 90071

City (Ciudad):

State (Estado):

Zip (Código postal):

Comments (Comentarios):

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Cristino Bermios

Phone (Teléfono):

323 809 6543

Email (Correo electrónico):

Address (Domicilio):

4648 huntington dr S # 229

City (Ciudad):

LA

State (Estado):

CA

Zip (Código postal):

90032

Comments (Comentarios):

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Lonnie Roman

Phone (Teléfono):

323

Email (Correo electrónico):

Address (Domicilio):

244-9909

City (Ciudad):

State (Estado):

Zip (Código postal):

Comments (Comentarios):

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Ella Leyva

Phone (Teléfono):

323 240-8796

Email (Correo electrónico):

leyvaefia@yahoo

Address (Domicilio):

3630 Locke Ave

City (Ciudad):

LA

State (Estado):

CA

Zip (Código postal):

90032

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- ☒ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Naomi Rodriguez

Phone (Teléfono):

323-799-3615

Address (Domicilio):

LA

State (Estado):

CA

Zip (Código postal):

90032

Comments (Comentarios):

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre): *Mayra Maldonado*
Phone (Teléfono): *(323) 809-9533* Email (Correo electrónico): *mayramaldonado.mam@gmail.com*
Address (Domicilio): *335 1/2 N Eastern Ave*
City (Ciudad): *Los Angeles* State (Estado): *Ca* Zip (Código postal): *90032*
Comments (Comentarios):

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Georgina Avalos

Phone (Teléfono):

323 915 5181

Email (Correo electrónico):

Address (Domicilio):

4929 LINCOLN

City (Ciudad):

State (Estado):

Zip (Código postal):

Comments (Comentarios):

NO

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

carolina Sanchez

Phone (Teléfono):

323-395-8748

Email (Correo electrónico):

Address (Domicilio):

1908 Daisy St

CA 90031

City (Ciudad):

Los Angeles

State (Estado):

Zip (Código postal):

Comments (Comentarios):

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- ☒ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Margarita Nicolás

Phone (Teléfono):

(213) 985-9793

Email (Correo electrónico):

Address (Domicilio):

1304 W 7TH ST Apt 503

City (Ciudad):

Los Angeles

State (Estado):

CA

Zip (Código postal):

90007

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Amalia Flores

Phone (Teléfono):

(323) 534-2168

Email (Correo electrónico):

Address (Domicilio):

1817 1/2 Workman St

City (Ciudad):

Los Angeles

State (Estado):

CA

Zip (Código postal):

90031

Comments (Comentarios):

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- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Araceli Castillo

Phone (Teléfono):

(323) 477-7852

Email (Correo electrónico):

maria@vls.com

Address (Domicilio):

2751 Lancaster Ave.

City (Ciudad):

LA

State (Estado):

CA

Zip (Código postal):

90033

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- ☒ Emailing my support to elected officials (Enviar un correo electrónico de apoyo a funcionarios electos)
- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre): Veronica Sandoval

Phone (Teléfono): 3235012829

Email (Correo electrónico):

veronicavermyan
9214@gmail.com

Address (Domicilio): 3740 Pueblo Ave L

City (Ciudad): LA

State (Estado): CA

Zip (Código postal): 90032

Comments (Comentarios):

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- ☐ Emailing my support to elected officials (Enviar un correo electrónico de apoyo a funcionarios electos)
- ☐ Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Maira Gonzalez

Phone (Teléfono):

(323) 802-9576

Email (Correo electrónico):

maigonzalez42046@gmail.com

Address (Domicilio):

2151 Lancaster Ave

City (Ciudad):

LA

State (Estado):

CA

Zip (Código postal):

90033

Comments (Comentarios):

Show your support online at

www.VistaAtElSeren.com/Support-The-Vision



Yes! I support Vista El Sereno

Si! Yo apoyo Vista El Sereno

I will voice my support by (Expresaré mi apoyo por):



Attending a public hearing (Atender en una reunión pública)



Speaking at a public hearing (Hablar en una reunión pública)



Emailing my support to elected officials (Enviar un correo electrónico de apoyo a funcionarios electos)



Recording an audio/video message of support (Grabar un mensaje de apoyo por audio o video)

Name (Nombre):

Araceli Delgado

Phone (Teléfono):

(323) 631-1400

Email (Correo electrónico):

*Araceli-delgado
4662a gahao conu*

Address (Domicilio):

1304 E 115th

City (Ciudad):

Los Angeles

State (Estado):

CA

Zip (Código postal):

90059

Comments (Comentarios):

Show your support online at

www.VistaAtElSeren.com/Support-The-Vision



May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

TTLC - El Sereno - Updated Community Outreach Reports

1 message

Whitney Hodges <WHodges@sheppardmullin.com>

Thu, Sep 26, 2019 at 4:07 PM

To: May Sirinopwongsagon <may.sirinopwongsagon@lacity.org>

Cc: Nashia Lalani <NLalani@sheppardmullin.com>

Good Afternoon May,

I hope this e-mail finds you well.

Please find updated reports for the TTLC – El Sereno community outreach efforts. This includes additional information not previously provided to the City. We will also be delivering a hard copy to you but wanted to confirm a good time tomorrow or next week to deliver.

Please let us know and we can come by to submit accordingly.

Best,

Whitney

Whitney Hodges

+1 619-338-6542 | direct

WHodges@sheppardmullin.com | [Bio](#)

SheppardMullin

501 West Broadway, 19th Floor

San Diego, CA 92101-3598

+1 619-338-6500 | main

www.sheppardmullin.com | [LinkedIn](#) | [Twitter](#)

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2 attachments



TTLC - Vista El Sereno - Community Outreach Update.pdf

361K



TTLC - Vista El Sereno - Additional Outreach Summary.pdf

259K